

EARLIMART COMMUNITY PLAN 2017 UPDATE



Tulare County Resource Management Agency
Economic Development and Planning Branch



Funded by the State of California
Housing and Community Development Department
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EARLIMART COMMUNITY PLAN UPDATE

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Earlimart Community Plan 2017 Update

Adopted: October 17, 2017

Tulare County Board of Supervisors
Resolution No. 2017-0869
GPA 17-005

GPA 14-005 (Eden Produce Company)
GPA 16-003 (Delano Joint Union High School District)

PZC 17-033 – Mixed Use Overlay District
PZC 17-034 – By Right Zoning District
PZC 17-035 – Earlimart Zoning District



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EXECUTIVE SUMMARY

INTRODUCTION

Earlimart's current Community Plan (GPA 87-11), which was adopted in November 1988 is over 28 years old, with minor amendments adopted in 1989 (GPA 89-04 and GPA 89-06), 1999 (GPA 99-01), 2004 (GPA 04-002), and 2006 (GPA 06-001). The 1988 Earlimart Community Plan is a collection of goals, objectives, and policies for the physical development of the Community. The existing Earlimart Urban Development Boundary (UDB), consists of approximately 1,400 acres (**see Figure 3**). The plan (GPA 14-005) serves, as a general guide for both public and private decisions affecting the community, and provides for the overall direction, density, and type of growth consistent with the needs of the community.

The surrounding area is predominantly agricultural land (consisting of vineyards, orchards, grazing land, and other agriculturally productive lands) in all directions, with scattered rural residences. Deer Creek is the southern boundary of the northernmost parcel included in the proposed project. The unincorporated hamlet of Teviston lies to the north of Earlimart and the Pixley National Wildlife Refuge is located approximately one-half mile west of the existing UDB limit, west of SR 99.

The objective in the preparation of a Community Plan for Earlimart is to develop a plan which can accurately reflect the needs and priorities of the unincorporated community of Earlimart. In addition, the County has prepared an Environmental Impact Report. The Environmental Impact Report (EIR) is to assist in fostering future economic development, grants, and economic development opportunities, which can tier off the General Plan and Community Plan EIR's.

Earlimart is currently designated an Unincorporated Community in the 2030 Tulare County General Plan (2012). It has become apparent that a more precise plan is needed to increase the availability of infrastructure funding and economic development within the community.

As with any community plan, the contents of this document are not intended to be absolute. Planning is a continuous process and, to be effective, requires periodic re-evaluation and revision to reflect changing needs and priorities. This Plan, therefore, should be reviewed on a periodic basis with the assistance and participation of local citizens, groups, and agencies. By doing so, it is envisioned that the Earlimart Community Plan will continue to provide meaningful and necessary guidance toward the development of the community in the foreseeable future.

LOCATION

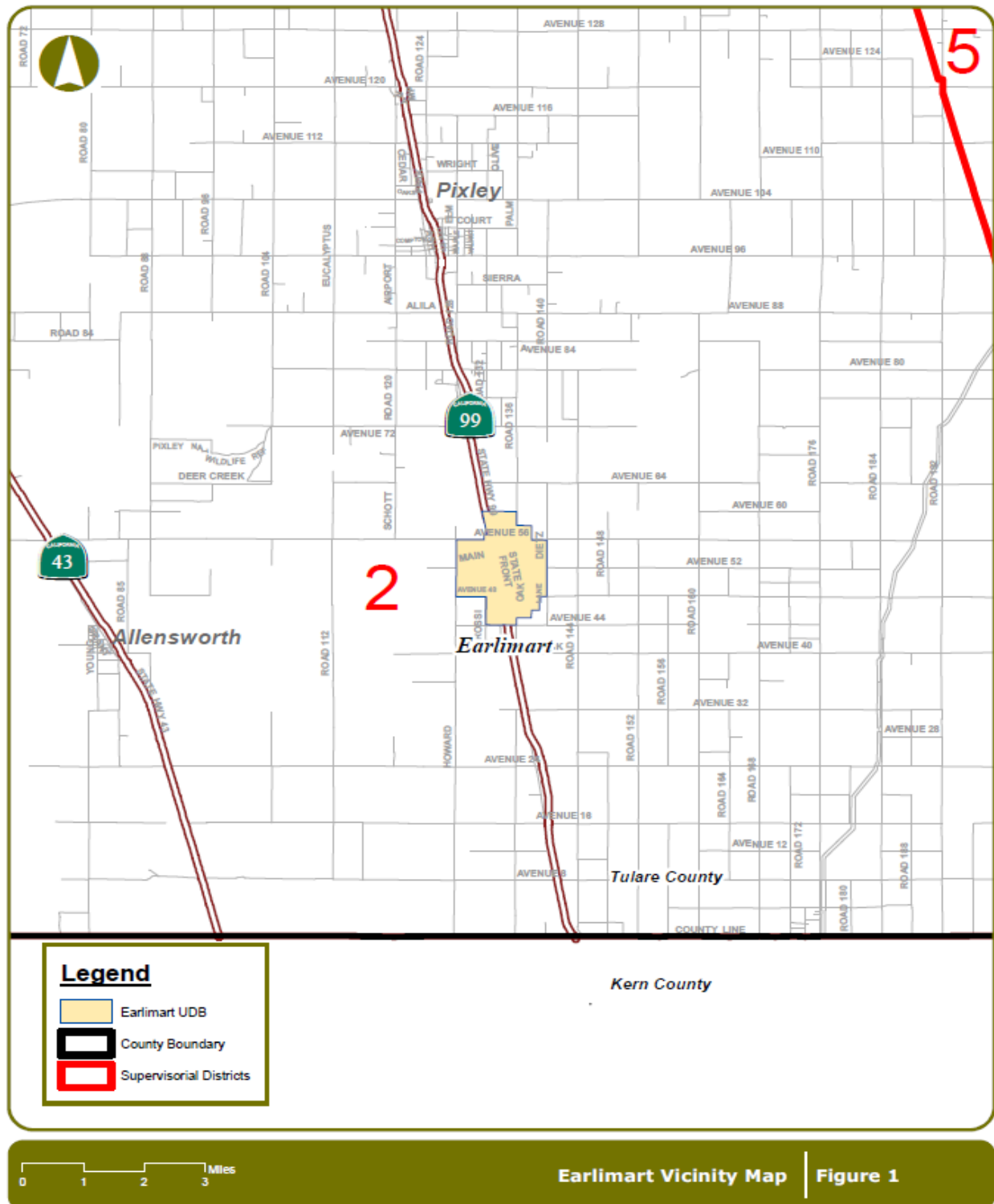
Earlimart is located in southern Tulare County along State Route (SR) 99 (**See Figure 1 – Vicinity Map**). Earlimart is, a rural unincorporated community of 8,537 persons located in southern Tulare County, approximately 25 miles south of Visalia and 40 miles north of Bakersfield. Earlimart is in Section 33 & 34, Township 23 South, Range 25 East; Section 3 & 4, Township 24 South, Range 25 East; and Section 27 & 28, Township 23 South, Range 25 East MDB&M, and can be found within the Pixley and Dinuba West Quads, United States Geological Survey 7.5 minute topographic quadrangle. Earlimart is located at an elevation of 283 feet above sea level. The coordinates of Earlimart are Latitude: 35° 52' 57" and Longitude: 119° 16' 12".

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State Route (SR) 99 and the railroad parallel each other and traverse Earlimart north to south. For the most part, the roadway is depressed as it passes through the community. Sierra Avenue (Avenue 56) connects Earlimart to State Route (SR) 43 and the community of Alpaugh to the west, and State Route (SR) 65 and the community of Ducor to the east. Earlimart is fortunate in that three streets, Armstrong Avenue, Washington Street and Sierra Avenue, cross SR 99 at grade. Packing houses and cold storage facilities are located along Front Road, which parallels SR 99. This roadway attracts substantial truck traffic because of the industrial uses on this road.

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Figure 1 - Vicinity Map



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HISTORICAL PERSPECTIVE

Earlimart was established in 1880, ten years after the construction of the San Joaquin Valley Railroad, now known as the Union Pacific Railroad. It was originally known as Alila, land of flowers. In 1910, Earlimart derived its present name from the fact that the crops in this region ripened early, and therefore could be taken "early to market". Thus, the name "earli-mart".

The community is predominantly a rural, agriculturally-related service center. It not only serves as an area where agriculturally oriented enterprises; such as packing houses and cold storage facilities are located, but it also is a bedroom community where many of the area's farmworkers reside. Earlimart is surrounded by agricultural lands (**see Figure 2 – Aerial Map**). Crops grown on these lands include cotton, alfalfa, grain and vineyards. Unlike many valley communities, there is little rural residential development surrounding the denser urbanized area. Earlimart's growth has been strongly influenced by SR 99 and the Union Pacific Railroad. The highway and railroad parallel each other in a north/south fashion and, in effect, have divided the community into three distinct areas.

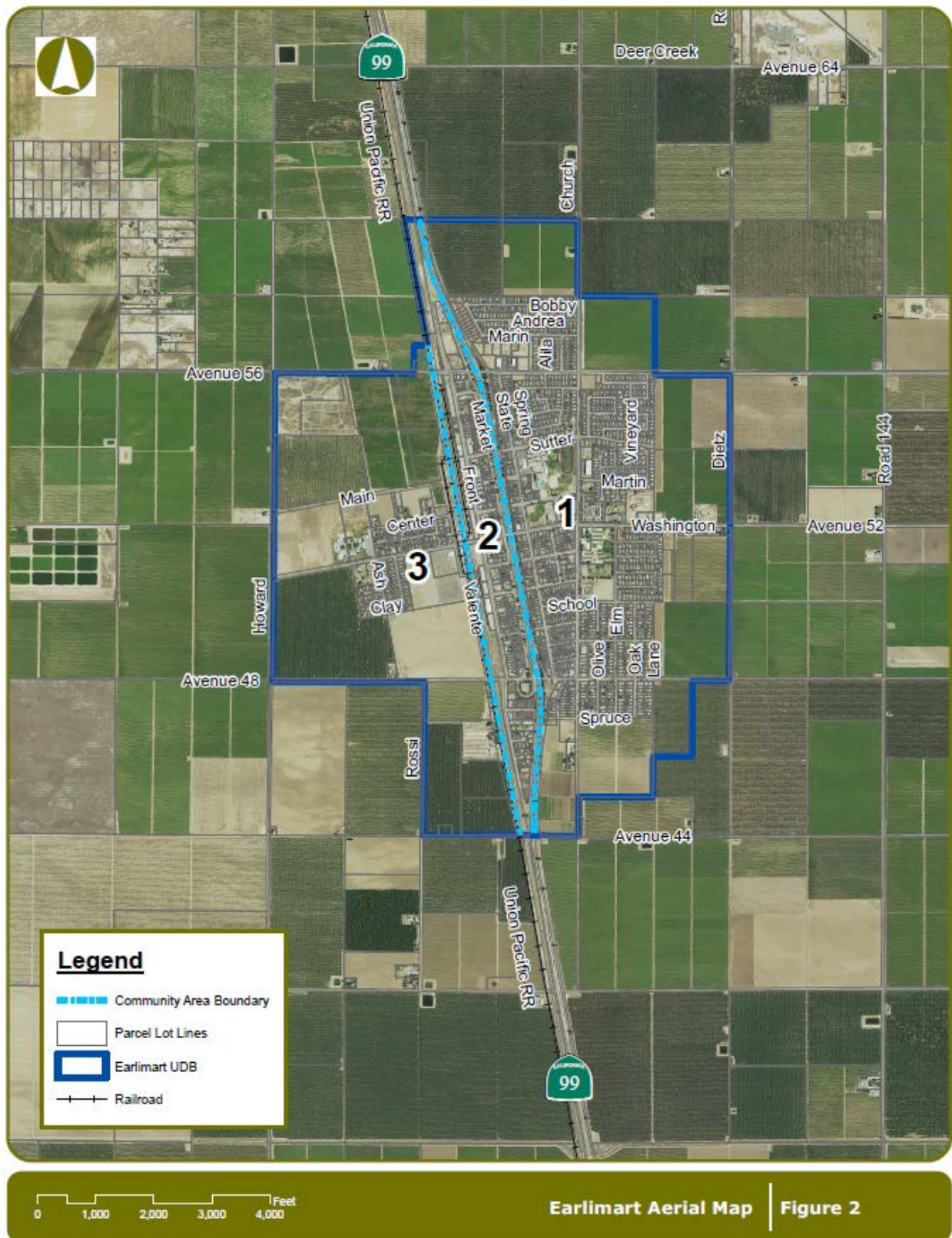
Area No. 1 is an older residential district east of SR 99. Except for new development to the north, early development in this area has predated present-day building and planning controls. It is not uncommon to observe two or three residential units located on the same lot. Mobile homes, travel trailers and small stick-built homes have been added to the primary residence. Although older and in need of some rehabilitation; this area is the "hub" of the community because of the public facilities located there, such as Earlimart's two schools, a park, churches, "Earlimart Memorial Building", and the fire station. The post office is located on N. Church Road and Center Avenue.

Area No. 2 is a mixture of commercial, industrial and older residential uses located between SR 99 and the Union Pacific Railroad. Dominating the area between the railroad and Front Street are packing houses and cold storage buildings. Small businesses in older commercial buildings and residential uses, many of which are deteriorated or dilapidated lie between Front Street and SR 99.

Area No. 3 is west of the Union Pacific Railroad. It is primarily undeveloped except for some newer residential development which occurred in the early 1980's.

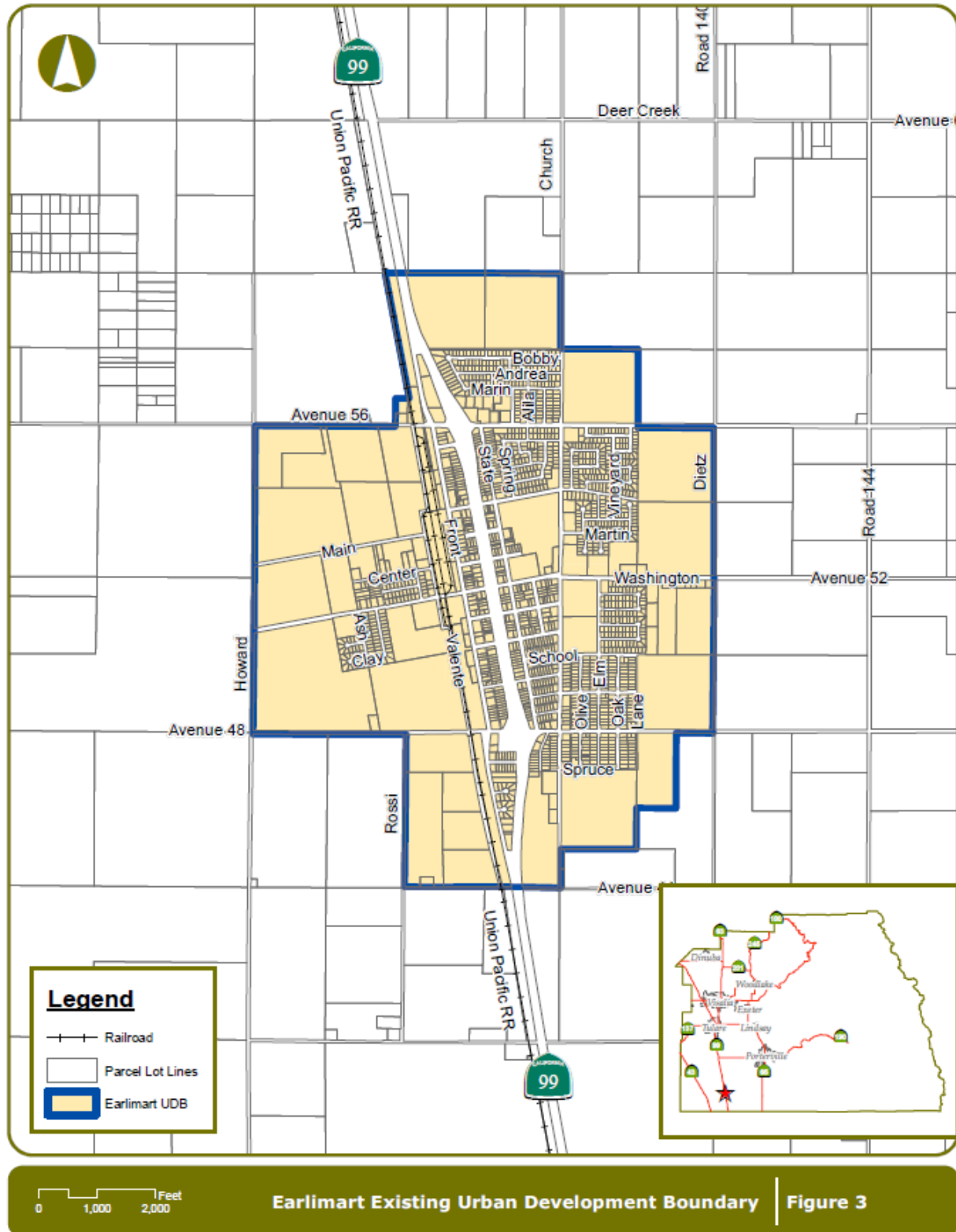
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Figure 2 - Earlimart Aerial Map



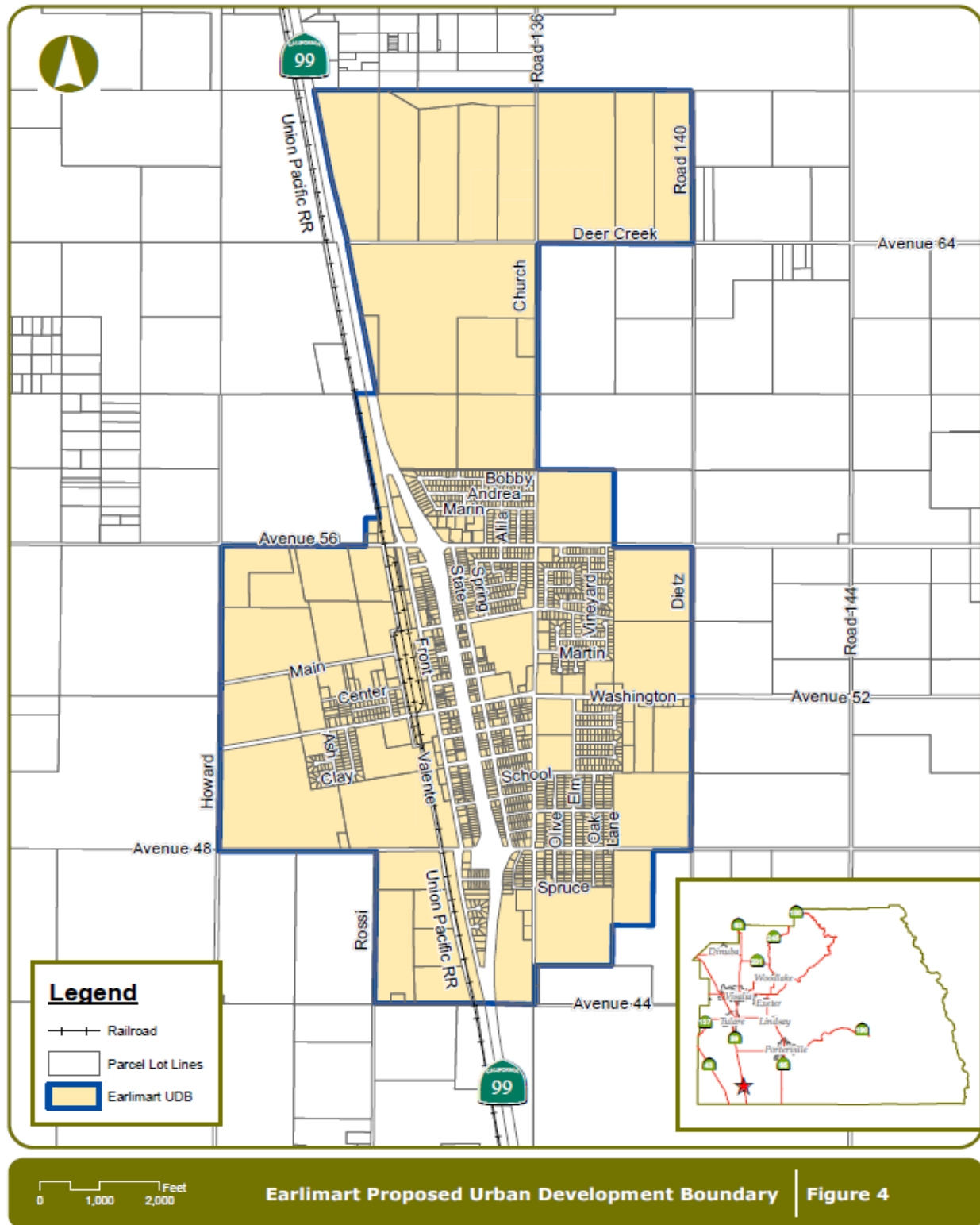
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Figure 3 - Existing Earlimart Urban Development Boundary



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Figure 4 - Proposed Earlimart Urban Development Boundary



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EARLIMART BACKGROUND REPORT

COMMUNITY PROFILE

Severely Disadvantaged Community

“Public Resources Code 75005. (g) states that a “disadvantaged community” means a community with a median household income less than 80% of the statewide average. “Severely disadvantaged community” means a community with a median household income less than 60% of the statewide average.”

The estimated median household income in Earlimart was \$23,803 across the 2011 – 2015 survey period. The median household income in the State of California during this same period was \$61,818. The estimated median household income in Earlimart is 38.50% of the median household income in the State of California; therefore, pursuant to PRC 75005(g), Earlimart is considered a severely disadvantaged community.

Climate

The southern San Joaquin Valley climate is influenced to a great extent by the Coast Ranges to the west which prevent the cool, moisture-laden maritime air from reaching the valley. Like most of California, the southern San Joaquin Valley (and therefore, Earlimart) experiences a Mediterranean climate – hot dry summers and cool, moist winters. Summer temperatures commonly exceed 100 degrees Fahrenheit, and the relative humidity is generally very low. Winter temperatures rarely exceed 70 degrees Fahrenheit, with daytime highs often below 60 degrees and during January the average nighttime low temperature is 35 degrees. Annual precipitation in the vicinity of Earlimart is about 7-8 inches, the most of which occurs between the months of November and April. Nearly all precipitation falls in the form of rain. There are periods in winter when the valley floor is covered with dense wet ground fog. Winds are typically light and from the north.

Topography

The topography of Earlimart is moderately sloped with elevation 280 feet above mean sea level. Earlimart consists of orchard/vineyard, agricultural field, ruderal (disturbed) lands that include the SR 99/railroad corridor, residential lands, irrigation infrastructure that includes fifteen irrigation basins, and a 3,300 linear foot reach of Deer Creek.

Existing Urban Development Boundary

The community of Earlimart is primarily rectangular in shape. Earlimart is bisected in a northwest-southeasterly direction by SR 99 and the Union Pacific Railroad which divides the community. The larger area of Earlimart is located on the east side of SR 99. The central commercial and industrial area is located between SR 99 and the Union Pacific Railroad on the west. The existing Earlimart Urban Development Boundary (UDB) area consists of approximately 1,399.7 acres (**see Figure 3**). Within the existing Earlimart UDB, approximately 764 acres are currently zoned for urban uses and approximately 633 acres are zoned for agricultural uses. Approximately 685 acres are currently developed for urban uses (598 acres of non-agricultural land and 87 acres of agricultural land); approximately 712 acres remain undeveloped for urban uses.

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Existing Land Use Plan

One of the most important purposes of the Earlimart Community Plan is to establish land use patterns and development policies and standards for the community for the planning period, through the year 2030. The general intent of the land use plan for Earlimart is to identify the most appropriate types and distribution of land uses for the community, based on environmental, circulation, infrastructure, services, opportunities and constraints, urban development boundary suitability analysis and other economic capacities and concerns discussed in the chapters of this Plan.

The County of Tulare, through existing policies, has encouraged both incorporated and unincorporated communities to establish urban development and land use patterns which are compact and contiguous. This policy position has reduced so-called “leap frog” development throughout the County, helping preserve agricultural lands, and minimize land use conflicts between urban and agricultural areas.

Residential

The greatest concentration of residential development in Earlimart occurs east of SR 99. This area contains a mix of older residential neighborhoods as well as tracts of newer homes. Both single family and multi-family residential development has occurred west of the railroad. The residential area west of the highway and east of Front Road is interspersed between industrial and commercial business. Residents of this area must cope with highway noise as well as noise and traffic from businesses in the area.

Multi-family Residential

There are two multi-family residential complexes located on East Washington Street.

Mobile Homes

Although there are no mobile home parks in Earlimart, approximately 43 mobile homes are located throughout Earlimart.

Community Commercial

There are several neighborhoods serving commercial uses along N. Front Street/S. Front Street. Along this street, there are two convenience markets, a tire shop, two restaurants, three barber shops, a record shop, Napa Auto Parts, and a Dollar Store.

Highway Commercial

White River Plaza includes Dollar General, Auto Zone, McDonalds, and there is a gas station on Sierra Avenue. There are also two gas stations on Armstrong Avenue. On N. Front Street, along the northern edge of Earlimart, there is an additional gas station and trucks scales on N. Front Street.

Industrial

Parcels adjacent to the Railroad are zoned Industrial between W. Sierra Road and Armstrong Avenue.

Schools

Earlimart is served by four schools Grades TK-5 attend Earlimart Elementary School, grades TK-5 attend Alila School, grades 6-8 attend Earlimart Middle School and grades 4-8 attend Earlimart Community Day School. High school students attend Delano High School located ten miles south.

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Parks

Earlimart Neighborhood Park is approximately 3.8 acres in size and is located on the southeast corner of the Earlimart Elementary School property bound by School Avenue and Elm Road. The site is owned by the Earlimart School District with whom the County has entered into a long-term joint powers agreement for development, operation and maintenance of the Park.

With collaboration from the Earlimart School District and the County, funding was received to develop a Park in Earlimart through the proposition 84 Statewide Park Development and Community Revitalization Program of 2008. The project scope is limited by the awarded grant amount of \$2,153,900. The park is 90% complete. Once the playground shade structure is installed and minor clean-up a Grand Opening Ceremony will be held in October 2017.

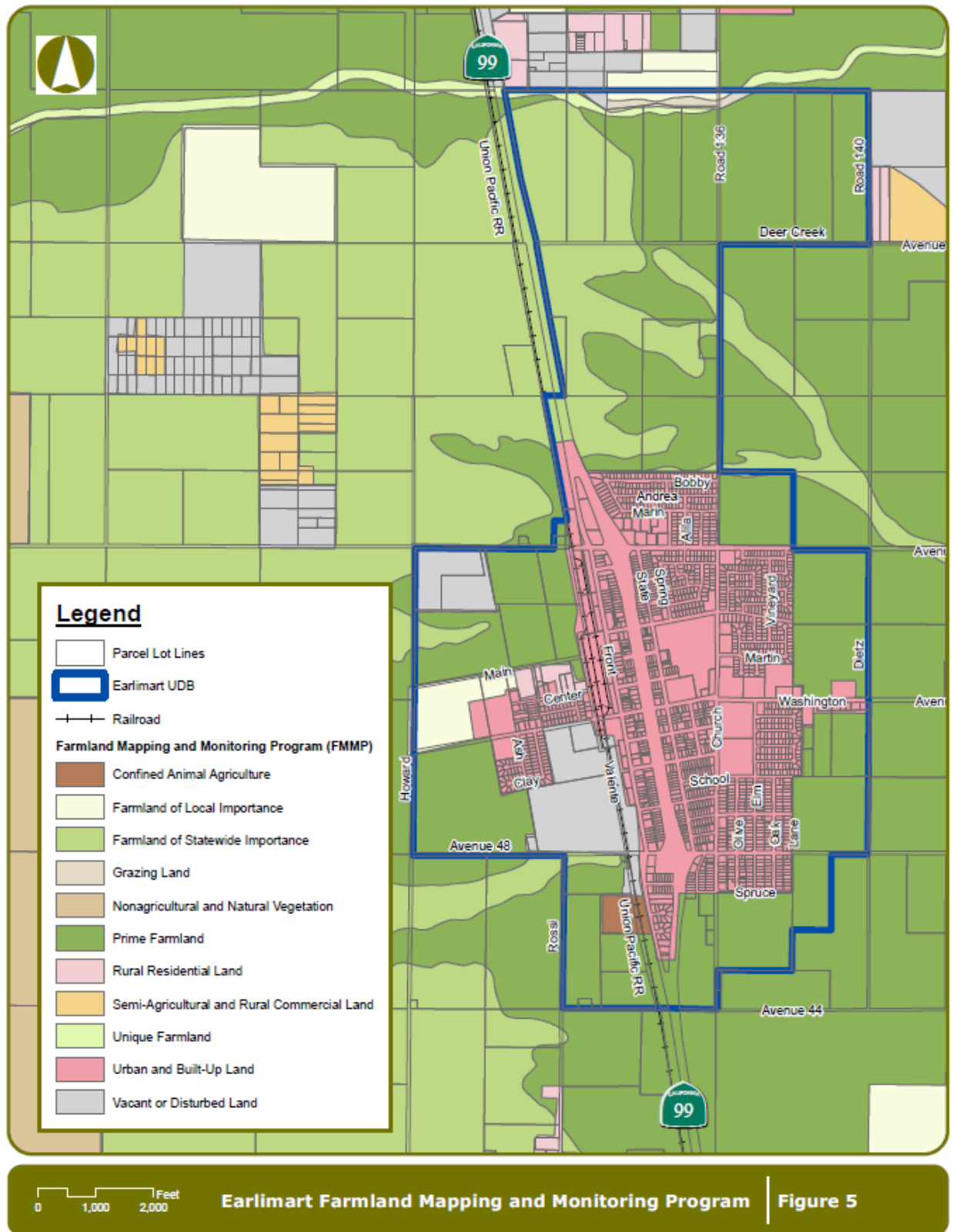
NATURAL AND CULTURAL RESOURCES

Agriculture

The area within the existing 1,399.7 acre UDB is designated in the 2014 FMMP maps (**see Figure 5**). Of these, approximately 617 acres are designated Urban and Built-up Land, approximately 562 acres are designated Prime Farmland, approximately 109 acres are designated Vacant or Disturbed Land, with approximately 56 acres designated Farmland of Statewide Importance, approximately 29 acres are designated as Farmland of Local Importance, which includes the area of the proposed new high school, and approximately 12 acres are designated Confined Animal Agriculture and approximately 12 acres are designated Rural Residential Land.

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Figure 5 - FMMP Map

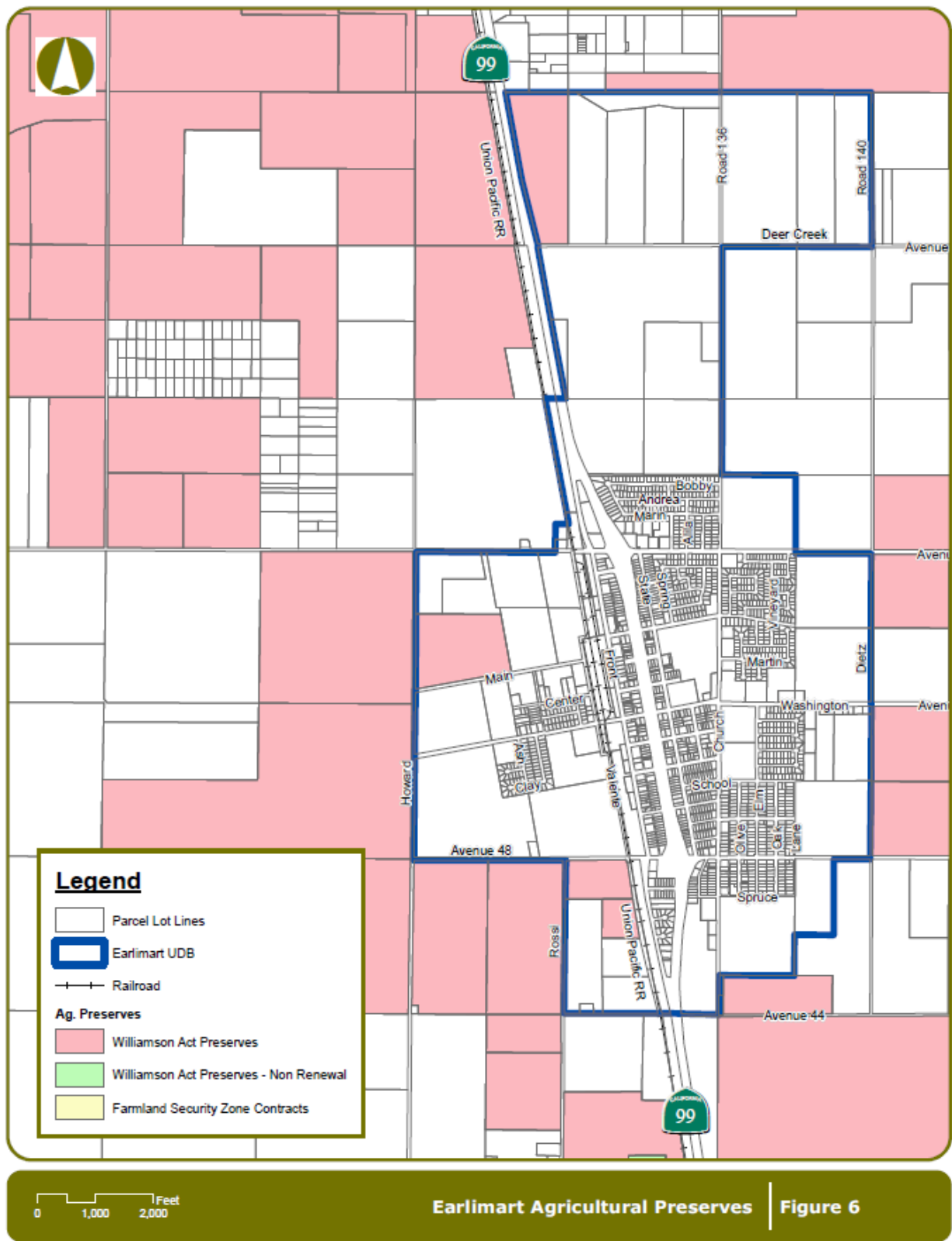


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There are four parcels that have entered into four land conservation contracts Williamson Act contracts (**see Figure 6**) within the planning area (proposed UDB). Agricultural preserve contracts are designed to keep productive farmland in agricultural use and prevent premature conversion to urban use. When a landowner enters into a contract, they receive a lower property tax rate. Within the Earlimart Urban Development Boundary there are currently 67.05 acres of land under contract. The proposed expansion of the Northern Earlimart Rezone Area will generate an addition 116.14 acres of Williamson Act Contracted land into the Earlimart Urban Development Boundary. Property owners must agree to keep their land in agricultural use for a 10-year period unless nonrenewal status is filed. If filing for nonrenewal, the property taxes will gradually increase back to normal rates over the course of the 9-year phase-out period. Contracts can be canceled before the 9-year period ends, if an approved alternative use satisfies the five findings of the Department of Conservation or if the project is found to be in the interest of the public. However, the property owner must pay a penalty equal to 12.5 percent of the unrestricted value of the property if approved.

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Figure 6 - Williamson Act Map



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Air Quality

The Earlimart Planning Area is within the San Joaquin Valley Air Basin (SJVAB) and under the jurisdiction of the San Joaquin Valley Air Pollution Control District (SJVAPCD). The SJVAB is classified non-attainment/severe for the State O_3 1-hour standard, non-attainment for the State O_3 8-hour standard, non-attainment for the State PM_{10} standard, non-attainment for the federal and State $PM_{2.5}$ standards, and attainment and/or unclassified for the remaining federal and State air quality standards. According to the Tulare County General Plan, the San Joaquin Valley has some of the worst air quality in the nation. The Carbon Monoxide (CO) and Nitrogen Oxide (NO_x) emissions are typically generated by motor vehicles (mobile sources). The Reactive Organic Gases (ROG) emissions are generated by mobile sources and agriculture. Although emissions have been shown to be decreasing in recent years, the SJVAB continues to exceed state and federal air quality emission standards.

Executive Order S-3-05, issued by Governor Schwarzenegger in 2005, established targets for greenhouse gas (GHG) emissions for the State. The Global Warming Solutions Act of 2006 (or Assembly Bill (AB) 32 directed the California Air Resources Board (CARB) to develop and adopt statewide GHG emission limits in order to reduce emission levels to those experienced in 1990, by the year 2020. In order to achieve those targets, CARB adopted the Climate Change Scoping Plan in December 2008.

Sustainable Communities and Climate Protection Act of 2008, also known as Senate Bill (SB) 375, builds upon AB 32 by requiring CARB to develop regional GHG emissions reduction targets for passenger vehicles. Then each Metropolitan Planning Organization (MPO) must prepare a Sustainable Communities Strategy (SCS) to demonstrate how the region will meet its targets. The SCS will be incorporated into the Regional Transportation Plan (RTP).

The SJVAPCD provides a list of potential air quality mitigation measures that are applicable to General Plan updates and community plans:

- Adopt air quality element/general plan air quality policies/specific plan policies.
- Adopt Local Air Quality Mitigation Fee Program.
- Fund TCM program: transit, bicycle, pedestrian, traffic flow improvements, transportation system management, rideshare, telecommuting, video-conferencing, etc.
- Adopt air quality enhancing design guidelines/standards.
- Designate pedestrian/transit oriented development areas on general plan/specific plan/planned development land use maps.
- Adopt ordinance limiting wood burning appliances/fireplace installations.
- Fugitive dust regulation enforcement coordinated with SJVUAPCD.
- Energy efficiency incentive programs.
- Local alternative fuels programs.
- Coordinate location of land uses to separate odor generators and sensitive receptors.

Air quality is directly related to land use; it is also related to the configuration of land, vegetation, climate, wind direction and velocity, and production of man-made impurities which change the natural qualities of the air. Because Earlimart is located near the southern end of the Valley with prevailing winds from the northwest, it is in a vulnerable position for the accumulation of adversely modified

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air, particularly when a temperature inversion occurs which holds down surface air along with its pollutants. Local air pollution sources within the general vicinity of Earlimart and within the community itself include traffic emissions from SR 99, individual vehicles, from local business and construction, and agricultural activities.

Biological Resources

“Tulare County exhibits a diverse ecosystems landscape created through the extensive amount of topographic relief (elevations range from approximately 200 to 14,000 feet above sea level). The County is essentially divided into three eco-regions. The majority of the western portion of the County comprises the Great Valley Section, the majority of the eastern portion of the County is in the Sierra Nevada Section, and a small section between these two sections comprises the Sierra Nevada Foothill Area.”¹

The planning area of Earlimart is void of native lands which would provide a habitat for threatened, candidate, proposed or endangered species; however, west of Earlimart are lands which still exist in their native state, grassland. The Pixley National Wildlife Refuge is also located to the north-east of Earlimart. These native lands are prime habitat for three Federal endangered, threatened or candidate species. They are San Joaquin kit fox (*Vulpes macrotis mutica*), Tipton kangaroo rat (*Dipodomys ntratojdes ntratojdes*), and Blunt-nosed leopard lizard (*Crotaphytus silus*). The biological evaluation concluded with the following findings (see **Figure 7 – CNDDDB Map**).

“Natural communities of special concern are those that are of limited distribution, distinguished by significant biological diversity, home to special status species, etc. [the California Department of Fish and Wildlife] is responsible for the classification and mapping of all natural communities in California. Natural communities are assigned state and global ranks per their degree of imperilment. Any natural community with a state rank of 3 or lower (on a 1-5 scale) is considered of special concern. Examples of natural communities of special concern near Earlimart include vernal pools and various types of riparian forest (Sawyer, Keeler-Wolf and Evens 2012). Except for the fragmented riparian corridor associated with the natural creek habitat of Deer Creek, all the vegetation associations present within Earlimart are man-made and dominated by non-native species, and therefore would not be considered natural communities of special concern. The Deer Creek corridor is the only portion of Earlimart that would be considered a natural community of special concern.”²

There are thirteen (13) Special Status plant species and twenty-one (21) Special Status animal species are known to occur in the general Earlimart vicinity. Field surveys were conducted by Live Oak Associates LOA in May of 2016, and it was determined that of the thirty-four (34) Special Status species, there was the possibility of only twelve (12) species to be in the Project area because of disturbances on the sites and the quality of habitat on and around the proposed Project sites. Based on findings by the Army Corp of Engineers in 2016, Deer Creek is considered non-jurisdictional, but has some riparian features per the California Department of Fish and Wildlife³

¹ Tulare County 2030 General Plan Recirculated EIR, p.3.11-5

² Live Oak Associates, Earlimart Community Plan Update Biological Evaluation 31 to 32

³ EIR Page 3.4.38

Figure 7 - Earlimart CNDDDB Map

Legend

- Parcel Lot Lines
- Earlimart UDB
- Railroad

CNDDB Symbology

- Plant (specific)
- Plant (non-specific)
- Plant (circular)
- Animal (specific)
- Animal (non-specific)
- Animal (circular)
- Multiple (non-specific)
- Multiple (circular)

Species and Locations:

- Atriplex cordulata* var. *erecticaulis*
- Atriplex subtilis*
- Athene cucularia*
- Gambelia sila*
- Dipodomys nitratoideus*
- Vulpes macrotis mutica*
- Delphinium recurvatum*
- Monolopia congdonii*
- Agelaius tricolor*
- Perognathus inornatus*
- Caulanthus californicus*
- Spea hammondi*
- Taxidea taxus*

Map Features:

- Highway 99
- Union Pacific Railroad
- Local Streets: Main, Center, Howard, Avenue 48, Avenue 44, Spruce, Olive, Elm, Cedar, Lark, Washington, Dietz, Avenue
- Landmarks: School, Church, Bobby, Andrea, Marin

Scale: 0, 1,000, 2,000 Feet

Earlimart California Natural Diversity Database (CNDDB) Figure 7

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Cultural Resources

The recorded history of Earlimart dates back more than two centuries to when Spanish missionaries and explorers were looking for areas to settle. The native Yokut Indians had established trails in the area that were utilized by the scout Kit Carson and the soldier-explorer John C. Fremont. These trails were evolved into roads as population in the Valley grew.

Tulare County's known and recorded cultural resources were identified through historical records, such as those found in the National Register of Historic Places, the Historic American Building Survey/Historic American Engineering Record (HABS/HAER), the California Register of Historic Resources, California Historical Landmarks, and the Tulare County Historical Society list of historic resources.

Due to the sensitivity of many prehistoric, ethnohistoric, and historic archaeological sites, locations of these resources are not available to the general public. The Information Center at California State University Bakersfield houses records associated with reported cultural resources surveys, including the records pertinent to sensitive sites, such as burial grounds, important village sites, and other buried historical resources protected under state and federal laws.

According to a search completed by the California Historical Resources Information System (CHRIS) on June 11, 2015, there is one previously identified historic-period resource, an earthen canal segment, has been recorded within the study area; a second resource, also an earthen canal, has been documented within a 1/2-mile radius of the study area. There are no other resources within or in the immediate vicinity of the study area that are listed on the National Register of Historic Places, the California Register of Historic Resources, California Points of Historical Interest, State Historic Landmarks, or the California Inventory of Historic Resources. Nine privately owner structures have been determined ineligible for National Register.

Native American Consultation

The Native American Heritage Commission (NAHC) was notified on June 26, 2015, regarding a Notice of Preparation of a Environmental Impact Report for General Plan Amendment No. 14-005 to update the Earlimart Community Plan. The Native American Heritage Commission maintains a contact list of Native American Tribes as having traditional lands located within the County's jurisdiction. Tulare County Resource Management Agency (RMA) staff contacted nine (9) Native American Tribes (see the Environmental Impact Report).

The County notified the NAHC of the Project again on August 8, 2016, requesting a Sacred Lands File search and a list of applicable tribes for consultation pursuant to AB 52. RMA staff contacted eight Native American tribal contacts by letter on August 15, 2016.

Geology & Seismic Hazards

The southern San Joaquin Valley is a broad arid plain, essentially level underlain by about 28,000 feet of marine and continental strata with the sediments derived from areas now occupied by mountain ranges. "Seismicity varies greatly between the two major geologic provinces represented in Tulare County. The Central Valley is an area of relatively low tectonic activity bordered by mountain ranges on either side. The Sierra Nevada Mountains, partially located within Tulare County, are the result of movement of tectonic plates which resulted in the creation of the mountain range. The Coast Range

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on the west side of the Central Valley is also a result of these forces, and the continued uplifting of Pacific and North American tectonic plates continues to elevate these ranges. The remaining seismic hazards in Tulare County generally result from movement along faults associated with the creation of these ranges.”⁴

“Groundshaking is the primary seismic hazard in Tulare County because of the county’s seismic setting and its record of historical activity. Thus, emphasis focuses on the analysis of expected levels of groundshaking, which is directly related to the magnitude of a quake and the distance from a quake’s epicenter. Magnitude is a measure of the amount of energy released in an earthquake, with higher magnitudes causing increased groundshaking over longer periods of time, thereby affecting a larger area. Groundshaking intensity, which is often a more useful measure of earthquake effects than magnitude, is a qualitative measure of the effects felt by population. The valley portion of Tulare County is located on alluvial deposits, which tend to experience greater groundshaking intensities than areas located on hard rock. Therefore, structures located in the valley will tend to suffer greater damage from groundshaking than those located in the foothill and mountain areas. However, existing alluvium valleys and weathered or decomposed zones are scattered throughout the mountainous portions of the county which could also experience stronger intensities than the surrounding solid rock areas. The geologic characteristics of an area can therefore be a greater hazard than its distance to the epicenter of the quake.”⁵

“There are three faults within the region that have been, and will be, principal sources of potential seismic activity within Tulare County. These faults are described below:

- **San Andreas Fault.** The San Andreas Fault is located approximately 40 miles west of the Tulare County boundary. This fault has a long history of activity, and is thus the primary focus in determining seismic activity within the county. Seismic activity along the fault varies along its span from the Gulf of California to Cape Mendocino. Just west to Tulare County lies the “Central California Active Area,” where many earthquakes have originated.
- **Owens Valley Fault Group.** The Owens Valley Fault Group is a complex system containing both active and potentially active faults, located on the eastern base of the Sierra Nevada Mountains. The Group is located within Tulare and Inyo Counties and has historically been the source of seismic activity within Tulare County.
- **Clovis Fault.** The Clovis Fault is considered to be active within the Quaternary Period (within the past two million years), although there is no historic evidence of its activity, and is therefore classified as “potentially active.” This fault lies approximately six miles south of the Madera County boundary in Fresno County. Activity along this fault could potentially generate more seismic activity in Tulare County than the San Andreas or Owens Valley fault systems. In particular, a strong earthquake on the Fault could affect northern Tulare County. However, because of the lack of historic activity along the Clovis Fault, inadequate evidence exists for assessing maximum earthquake impacts.”⁶

⁴ General Plan Background Report, page 8-5

⁵ General Plan Background Report, page 8-7

⁶ General Plan Background Report, pages 8-6 and 8-7

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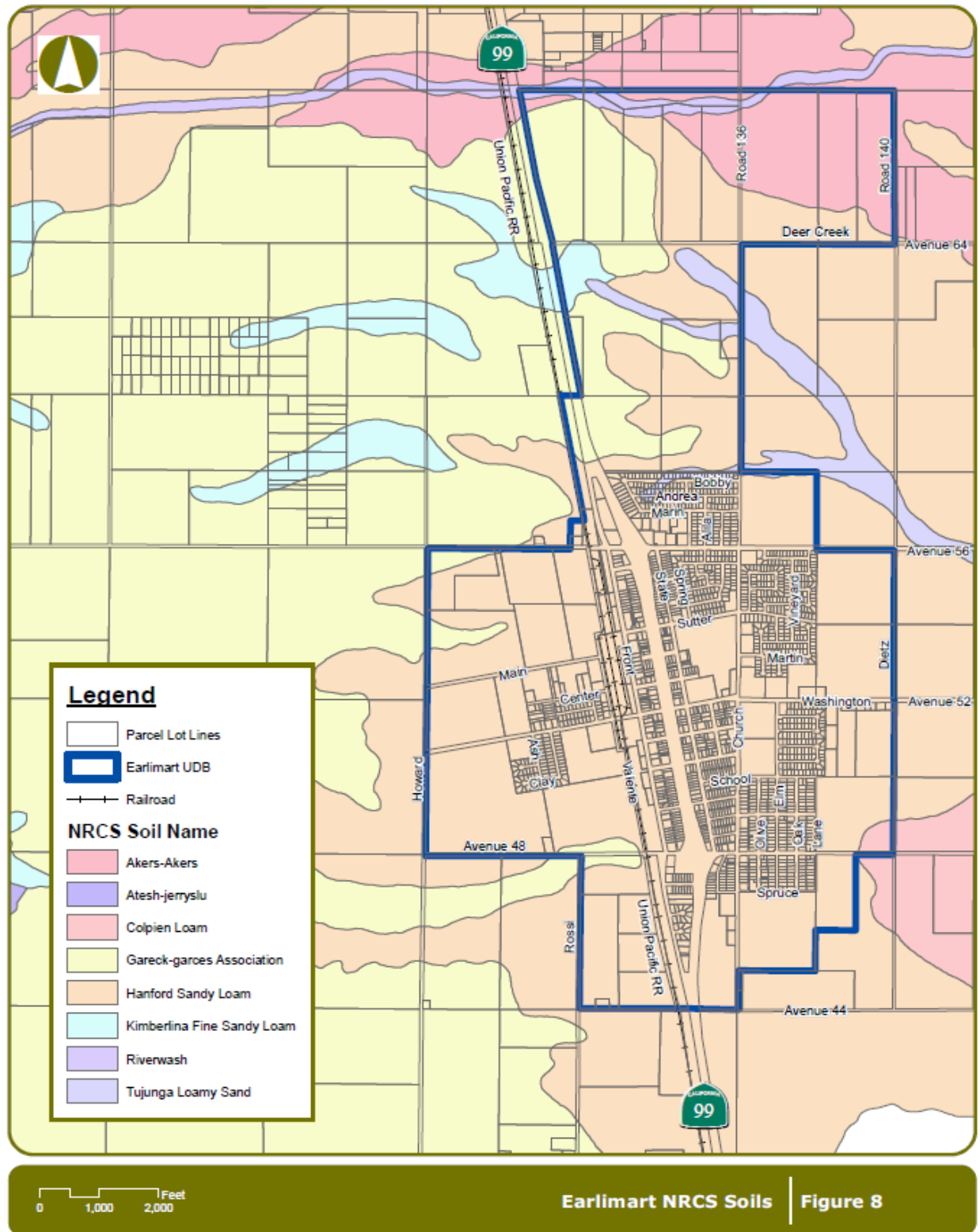
Soil Characteristics

The 1974 Five County Seismic Safety Element, adopted by the County of Tulare, places the Earlimart Planning Area within Seismic Zone VI. Seismic Zone VI includes the most of the eastern San Joaquin Valley, and is characterized by a relatively thin section of sedimentary rock overlying a granitic basement. Amplification of shaking that would affect low to medium-rise structure is relatively high, but the distance to either the San Andreas or Owens Valley faults (the expected sources of shaking) is sufficiently great that the effects should be minimal. Adherence to the requirements of the Uniform Building Code applicable to the Planning Area should be adequate to protect new structures from earthquake damage.

The existing Earlimart UDB rest on soils that are generally resistant to expansion is exhibit the highest potential for shrink-swell behavior. Damage to building foundations, roads and other structures is caused by the shrinking and swelling of soils as they become dry or wet. As shrink-swell potential increases, the soil is increasingly less suitable for buildings and roads unless corrective features are included in the design of the project.

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Figure 8 - NRCS Soils Map



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Table 1- Soils in Earlimart				
Map Unit	Soil Series	Parent Material	Drainage	Hydric
101	Akers-Akers, saline-sodic, complex slopes 0-2%	Alluvium derived from Granite	Well Drained	Yes, in depressions
108	Colpien loam, 0-2% slopes	Alluvium derived from Granite	Well Drained	No
119	Gareck-Garces association, 0-2% slopes	Alluvium derived from Granite, Garces Series are Saline-sodic soils, Gareck soils have been treated with agricultural chemicals	Very Deep Well Drained	Yes, in depressions
124	Hanford sandy loam, 0-2% slopes	Alluvium	Well Drained	Yes, in drainage ways
127	Kimberlina fine sandy loam 0-2% slopes	Mixed alluvium derived from igneous and/or sedimentary rock, moderately alkaline	Very Deep Well Drained	Yes
134	Riverwash	Alluvium		Yes, in drainage ways
138	Tujunga loam sand, 0-2% slopes	Alluvium derived from granite	Very Deep, somewhat excessively drained	Yes, in alluvial fans

Greenhouse Gases

“Executive Order S-3-05 was signed by Governor Schwarzenegger on June 1, 2005. This executive order established [GHG] emission reduction targets for California. Specifically, the executive order established the following targets:

- By 2010, reduce GHG emissions to 2000 levels.
- By 2020, reduce GHG emissions to 1990 levels.
- By 2050, reduce GHG emissions to 80 percent below 1990 levels.

The executive order additionally ordered that the Secretary of the California Environmental Protection Agency (Cal EPA) would coordinate oversight of the efforts among state agencies made to meet the targets and report to the Governor and the State Legislature biannually on progress made toward meeting the GHG emission targets. Cal EPA was also directed to report biannually on the impacts to California of global warming, including impacts to water supply, public health, and agriculture, the coastline, and forestry, and prepare and report on mitigation and adaptation plans to combat these impacts.

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In response to the EO [executive order], the Secretary of Cal EPA created the Climate Action Team (CAT), composed of representatives from the Air Resources Board; Business, Transportation, & Housing; Department of Food and Agriculture; Energy Commission; California Integrated Waste Management Board (CIWMB); Resources Agency; and the Public Utilities Commission (PUC). The CAT prepared a recommended list of strategies for the state to pursue to reduce climate change emission in the state...”⁷

In 2006, California passed the California Global Warming Solutions Act of 2006 (Assembly Bill 32; California Health and Safety Code Division 25.5, Sections 38500, et seq.), which requires the CARB to design and implement emission limits, regulations, and other measures, such that feasible and cost-effective statewide GHG emissions are reduced to 1990 levels by 2020.

The Sustainable Communities and Climate Protection Act of 2008, also known as Senate Bill (SB) 375, builds upon AB 32 by requiring CARB to develop regional GHG emissions reduction targets for passenger vehicles. Then each Metropolitan Planning Organization (MPO) must prepare a Sustainable Communities Strategy (SCS) to demonstrate how the region will meet its targets. The SCS will be incorporated into the Regional Transportation Plan (RTP).

The Air District adopted the *Climate Change Action Plan* (CCAP) in August 2008. “The CCAP directed the District Air Pollution Control Officer to develop guidance to assist Lead Agencies, project proponents, permit applicants, and interested parties in assessing and reducing the impacts of project specific greenhouse gas (GHG) emissions on global climate change.

On December 17, 2009, the San Joaquin Valley Air Pollution Control District (District) adopted the guidance: Guidance for Valley Land-use Agencies in Addressing GHG Emission Impacts for New Projects under CEQA, and the policy: District Policy – Addressing GHG Emission Impacts for Stationary Source Projects Under CEQA When Serving as the Lead Agency. The guidance and policy rely on the use of performance based standards, otherwise known as Best Performance Standards (BPS), to assess significance of project specific greenhouse gas emissions on global climate change during the environmental review process, as required by CEQA.

“The Tulare County Climate Action Plan (CAP) (last updated in 2016), serves as a guiding document for County of Tulare (“County”) actions to reduce greenhouse gas emissions and adapt to the potential effects of climate change. The CAP is an implementation measure of the 2030 General Plan Update. The General Plan provides the supporting framework for development in the County to produce fewer greenhouse gas emissions during Plan buildout. The CAP builds on the General Plan’s framework with more specific actions that will be applied to achieve emission reduction targets consistent with California legislation.”⁸

Flooding

According to the Federal Emergency Management Agency (FEMA) Flood Insurance Rate Map, Community-Panel Numbers 06107C2260E and 06107C1925E, a portion of Earlimart east of SR 99 is subject to 100-year flood (see **Figure 9**). The depth of this potential flood could range from one

⁷ Tulare County General Plan 2030 Update RDEIR, pages 3.4-4 to 3.4-5

⁸ Tulare County Climate Action Plan, page 1

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to three feet. This potential flood condition is caused by the White River, located south of Earlimart, and Deer Creek which is located north of Earlimart, damming up against SR 99 due to conditions which restrict flow under the highway. This situation causes land to flood north and south of the undercrossing. The Tulare County Flood Control District has indicated that this flooding condition is a result of an undersized culvert, sand accumulation and vegetation buildup in the channel. The department is currently investigating ways to eliminate this flood condition.

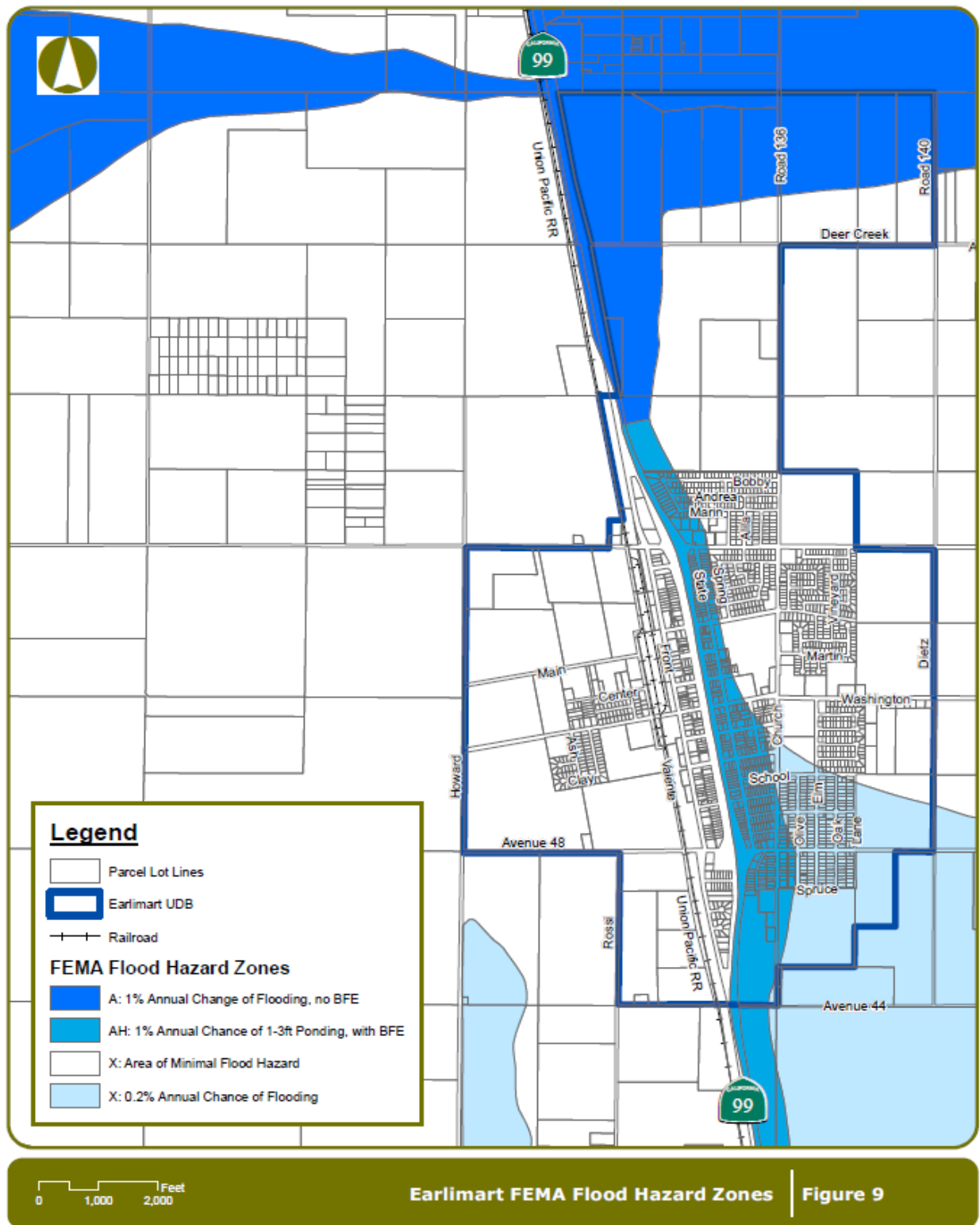
“Official floodplain maps are maintained by the Federal Emergency Management Agency (FEMA). “Floodplain” or “flood-prone area” means any land area susceptible to being inundated by water from any source. “Base Flood” is the flood having a one percent chance of being equaled or exceeded in any given year. “One-hundred-year flood” or “100-year flood” has the same meaning as “base flood.” “Special flood hazard area” is the land in the floodplain subject to a one percent or greater chance of flooding in any given year. “Floodway” means the channel of a river or other watercourse and the adjacent land area that must be reserved in order to discharge the base flood without cumulatively increasing the water surface elevation more than one (1) foot. The floodway is delineated on the Flood Boundary Floodway Map, on maps adopted by the State Reclamation Board when acting within its jurisdiction, and on the County Zoning Map (signified by the F-1 Primary Flood Plain Zone). The F-2 Secondary Flood Plain Combining Zone which is intended for application to those areas of the County which lie within the fringe area or setback of the flood plain and are subject to less severe inundation during flooding conditions than occur in the F-1 Zone.

FEMA determines areas subject to flood hazards and designates these areas by relative risk of flooding on a map for each community, known as the Flood Insurance Rate Map (FIRM). These areas are designated as Zone A, AO, A1-A30, AE, A99, or AH on the FIRM. A 100-year flood is considered for purposes of land use planning and protection of property and human safety. The boundaries of the 100-year floodplain are delineated by FEMA on the basis of hydrology, topography, and modeling of flow during predicted rainstorms. Within Earlimart there are areas of localized ponding and puddling that occur during heavy rainfall events. Additional projects will be required in the future to further expand stormwater drainage capacity. The elevation of building pads should eliminate the potential for loss of property should flooding occur. In Zone A, all buildings and structures must be elevated at least two feet above the higher natural ground elevation on the site.

The County of Tulare has taken steps to be a part of the National Flood Insurance Program (NFIP), which means the County of Tulare agreed to manage flood hazard areas by actively adopting minimum regulatory standards as set forth by Federal Emergency Management Agency (FEMA). The National Flood Insurance Program (NFIP) is administered by the (FEMA) to offer flood insurance to properties located in special flood hazard areas (SFHAs). Information about the NFIP, is available at the following website: www.fema.gov. As part of the county’s participation in the NFIP, individuals are eligible to obtain flood insurance. Future flood information is available on the County of Tulare Resource Management Agency website at: <http://tularecounty.ca.gov/rma/index.cfm/public-works/flood-hazard-information/flood-control-information/>. On June 16, 2009, Tulare County adopted the new Digital Flood Insurance Rate Maps (DFIRMs). Information is available to determine if a property is located in a SFHA by using the following FEMA Map Service Center link as follows: <https://msc.fema.gov/portal>.

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Figure 9 - Earlimart FEMA Flood Zone Maps

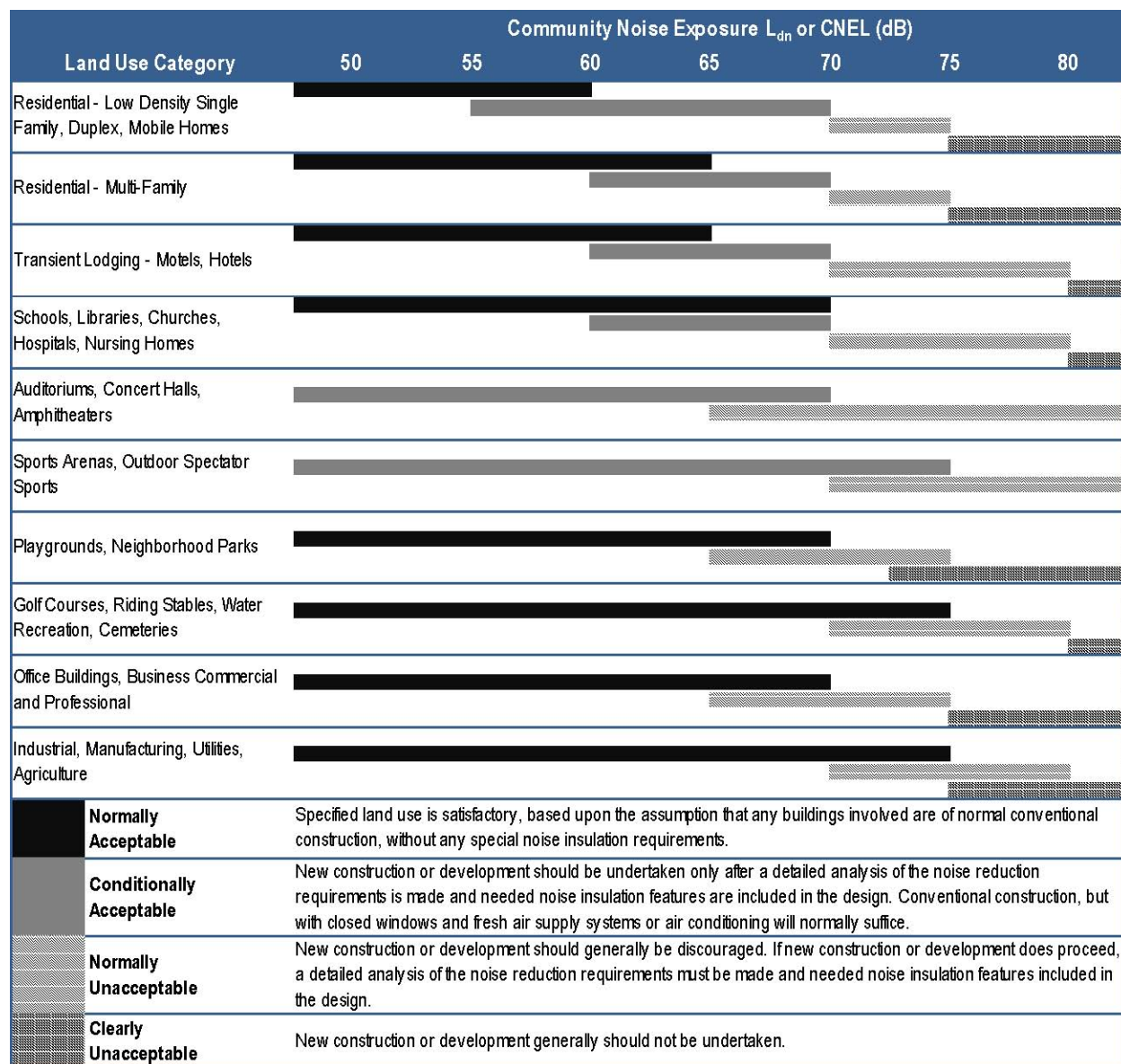


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Noise

State of California General Plan Guidelines (California Governor's Office of Planning and Research, 2003) identifies guidelines for the Noise Elements of city and county General Plans, including a sound level/land-use compatibility chart that categorized, by land use, outdoor L_{dn} ranges in up to four categories (normally acceptable, conditionally acceptable, normally unacceptable, and clearly unacceptable). These guidelines provide the State's recommendations for city and county General Plan Noise Elements (see Figure 10).

Figure 10 - Community Noise Exposure



Source: Tulare County General Plan

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There are a variety of sources that produce noise in the Earlimart Plan area and include traffic, railroad operations, and agricultural operations. Traffic noise is the most dominant source of ambient noise in the County, according to the Tulare County General Plan EIR. SR 99 runs through the Earlimart Plan Area and is the largest source of traffic noise in the area due to the high volumes of traffic. Noise from SR 99 adversely impacts an area through Earlimart making properties in close proximity to the highway less desirable for new housing construction.

Operations along the Union Pacific Railroad line are another significant source of noise in Earlimart. According to the Tulare County General Plan EIR, there are more than twenty (20) freight train operations per day along the Union Pacific rail line in Tulare County and may occur at any time of day or night. Noise levels are higher at at-grade crossings due to the warning horn. As such, Earlimart is impacted by warning horn noise whenever a train crosses Armstrong Avenue (Avenue 48) and Sierra Avenue (Avenue 56) thereby impacting adjacent land uses whenever a train passes through the community.

The 2010 Recirculated Environmental Impact Report (RDEIR) prepared for the Tulare County General Plan Update included the following information regarding freeway and railroad noise. Baseline traffic noise contours for major roads in the County were developed using Sound32 (Caltrans' computer implementation of the FHWA Traffic Noise Prediction Model). Table 3.5-3 in the RDEIR summarized the daily traffic volumes, and the predicted Ldn noise level at 100 feet from the roadway centerline is approximately 79 feet, and the distance from the roadway centerline to the 60-, 65-, and 70-dB-Ldn contours are 82 feet, 1,813 feet, and 3,907 feet respectively.

Mainline operations on the Union Pacific Railroad in Tulare County affect the community of Earlimart. According to the Trainmaster's office in Fresno, there are more than 20 freight train operations per day in the Tulare County Area. Passenger trains presently do not operate on Union Pacific tracks in Tulare County. Train speeds on the mainline are generally 45-65 mph and train movements may occur at any time during the night or day. According to the Wyle methodology, the above-described type and frequency of operation results in noise exposures of 65 and 60 dB Ldn at approximately 335 and 660 feet, respectively, from the center of the tracks for present operations, and at approximately 440 and 800 feet, respectively, from the center of the tracks for estimated future operations. Noise levels in the vicinity of grade crossings are somewhat higher than this due to the use of the warning horn.

The Noise Element identifies noise-impacted areas throughout Tulare County. These areas include lands which have existing or projected noise levels exceeding 60 decibels (dBA) Ldn. This decibel figure is considered to be the maximum normally acceptable noise level for single family residential areas. The two primary noise sources are SR 99 and the Union Pacific Railroad. Together, these noise sources place a portion of the urbanized portion of Earlimart within the 60 dB Ldn noise contour. Roadways and traffic noise are the dominant source of ambient noise in the County.

The Health and Safety Element of the Tulare County General Plan adopted two Countywide goals regarding noise in 2012. They are: 1) Protect the citizens of Tulare County from the harmful effects of exposure to excessive noise; and 2) Protect the economic base of Tulare County by preventing the encroachment of incompatible land uses near known noise-producing industries, railroads, airports and other sources. The Tulare County General Plan 2030 models noise contours for lands adjacent

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to freeways, airports, local industries and railroads for the base year (1986) and provides projected contours for the year 2010. The noise contours were prepared in terms of either the community noise equivalent level (CNEL) or day-night average decibel level (Ldn), which is descriptive of the total noise exposure at a given location for an annual average day.

The Noise Element includes performance standards for new residential or other noise-sensitive land uses which are to be located near noise-impacted areas. The Element indicates that these uses will not be permitted unless effective design measures can be integrated into the development to mitigate the impact of noise. **Table 2** summarizes the daily traffic volumes along SR 99 from Avenue 24 to Avenue 48 and Avenue 48 to Avenue 76.

Table 2 - Noise Contours SR 99					
Location	ADT	Distance (feet) to 70 Ldn Contour	Distance (feet) to 65 Ldn Contour	Distance (feet) to 60 Ldn Contour	Distance (feet) to 55 Ldn Contour
from Ave 24 to Ave 48	41,000	338	728	1,568	3,378
from Ave 48 to Ave 76	38,500	324	698	1,503	3,239

Source: Tulare County General Plan Background Report 2010

CIRCULATION

Street and Highway System

Functional classification is the process by which streets and highways are grouped into classes according to the type of service they provide. Streets and highways are classified according to their primary function and may be assigned into several basic classifications. The Board of Supervisors on February 28, 2017, approved inclusion of the Complete Streets Program as part of the Circulation Element of the Community Plan (**see Figure 11**):

- State Highways (which may be freeways, expressways or conventional highways)
- Arterials and Collectors
- Local Streets

State Highways connect regional destinations and generally pass through several jurisdictions. Traffic carrying capacity is maintained through access control at two-mile or more intervals, with shorter intervals between access points permitted in large urban areas.

Arterials serve as the principal network for cross-town traffic flow. They connect areas of major traffic generation within the community area and connect with important county roads and state highways. They also provide for the distribution and collection of through traffic to and from collector and local streets.

Collectors provide for traffic movement between arterial and local streets, traffic movement within and between neighborhoods and major activity centers, and limited direct access to abutting

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properties. Local streets provide for direct access to abutting properties and for very localized traffic movements within residential, commercial and industrial areas.

In recent years, the concept of “Complete Streets” has evolved. Under this concept, while streets may still carry a primary functional classification, the design of streets aims to allow all modes and trip purposes to be safely accommodated to the extent feasible and as warranted by local needs and conditions.

Existing Circulation and Traffic Conditions

“In the Earlimart area, State Route (SR) 99 is freeway with two travel lanes northbound and southbound. Three arterial streets, Armstrong Avenue (Avenue 56), Washington Street and Sierra Avenue (Avenue 48), cross Highway [SR] 99 at grade and connect the east side of Earlimart with the west side. Two streets parallel State Highway [SR] 99, State Street and Front Street. The latter provides access to Earlimart's commercial/industrial district. Since numerous packing houses and cold storage facilities are located along Front Street, the roadway attracts substantial truck traffic.

For local circulation, Washington Street is the most crucial to the community. It is centrally located and is one of three streets which crosses Highway [SR] 99. It connects the commercial/industrial district located west of the highway to the east side of the community which contains most of the residential development. It also serves as a major pedestrian corridor since it provides access to both elementary schools and will also serve Earlimart's planned High School.

Church Street is a two-lane north-south street to the east of State Street provides access to schools, shops and residences on the community's east side. Like Washington Street is important for trips internal to Earlimart[,] [a]ll other streets and roads in the community's planning area function essentially as local streets, mainly carrying traffic accessing abutting urban and rural properties.”⁹

To identify current traffic conditions, AM and PM peak hour turning movement counts were conducted at key intersections in the Earlimart area in June, 2016. Data on roadway approach lanes at intersection was collected at the same time. Based upon these data and methodologies prescribed by the County, traffic levels of service (LOS) were determined and the adequacy of the community's road network for serving current and future traffic demand was assessed.

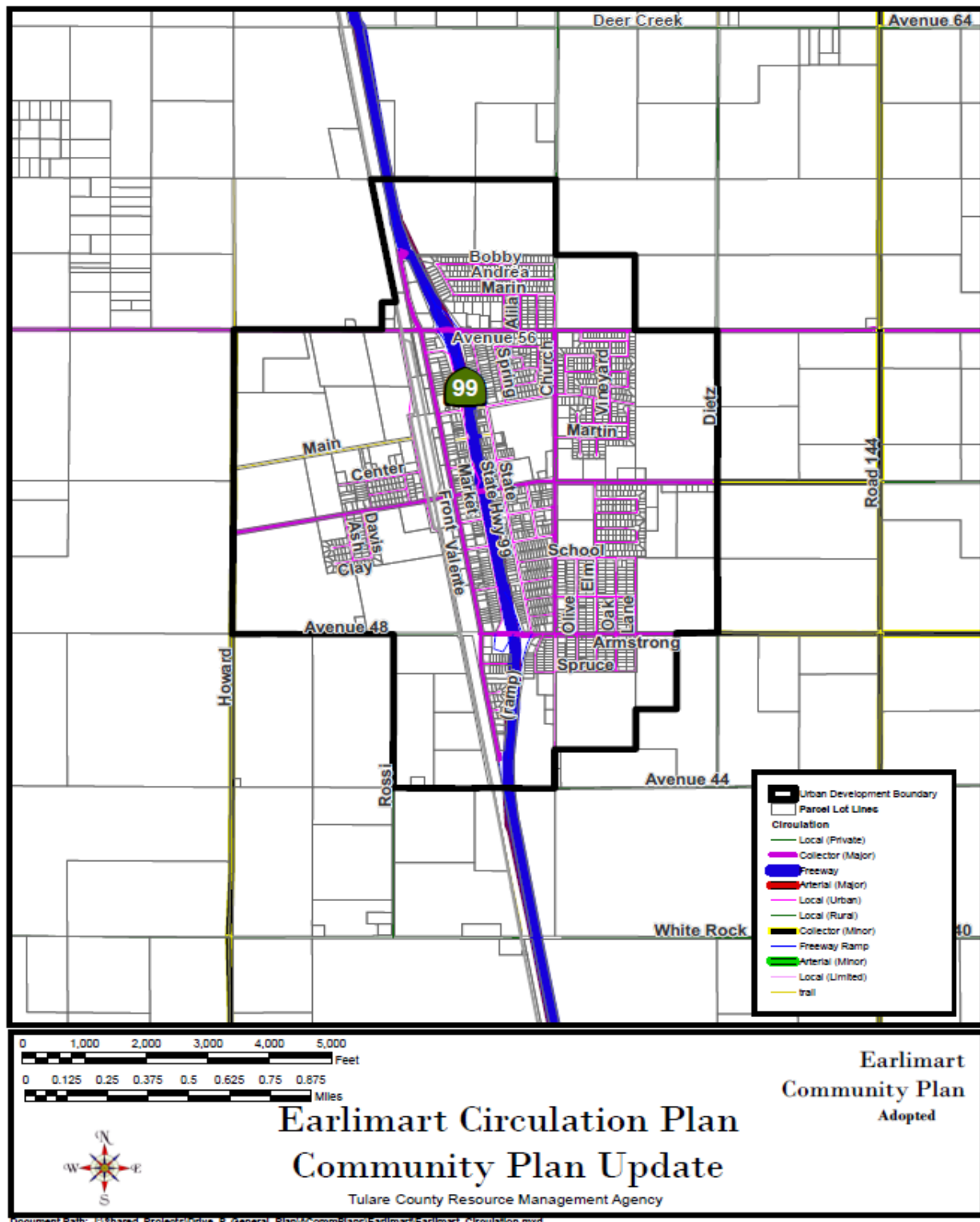
Data was collected at the following eight intersections and the adjacent roadway segments:

1. SR 99 NB Ramps at Sierra Avenue
2. SR 99 SB Ramps at Sierra Avenue
3. Sierra Avenue at State Street
4. Sierra Avenue at Church Street
5. Sierra Avenue at Front Street
6. SR 99 NB Off Ramp/State Street at Armstrong Avenue
7. SR 99 SB Off Ramp at Armstrong Avenue
8. Armstrong Avenue at Front Street

⁹ Earlimart Community Plan Traffic Impact Assessment and Circulation Plan, Page 5. Prepared by VRPA Technologies (included as Appendix “E” of this DEIR).

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Figure 11 - Earlimart Circulation Plan



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Existing Circulation and Traffic Conditions

Consultant VRPA Technologies prepared the earlier noted “Earlimart Community Plan Traffic Impact Assessment and Circulation Plan” (TIA, included as Attachment A-6) to establish a “baseline” (that is, beginning year) of current traffic/circulation conditions. The information evaluated to identify current conditions were A.M. and P.M. peak hour turning movement counts and data on roadway approach lanes at certain intersections data that enabled VRPA Technologies to conduct intersection capacity analysis and capture existing average daily traffic conditions to then forecast future transportation traffic projects.

Road Capacity and Level of Service (LOS)

LOS is categorized by two parameters, uninterrupted flow and interrupted flow. Uninterrupted flow facilities have no fixed elements, such as traffic signals, that cause interruptions in traffic flow (e.g., freeways, highways, and controlled access, some rural roads). Interrupted flow facilities have fixed elements that cause an interruption in the flow of traffic such as stop signs and signalized intersections. The definitions and measurements used for determining level of service in interrupted and uninterrupted conditions are shown in **Tables 3 and 4**.

In Tulare County, General Plan Policy **TC – 1.16 County Level of Service (LOS) Standards** states; “The County shall strive to develop and manage its roadway system (both segments and intersections) to meet a LOS of “D” or better in accordance with the LOS definitions established by the Highway Capacity Manual.”¹⁰

¹⁰ Tulare County General Plan 2030 Update, Part 1 – Goals and Policies Report. Page 13-7.

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Table 3 - Uninterrupted Traffic Flow Facilities

LEVEL OF SERVICE	DEFINITION
A	Describes free-flow operations. Free-Flow Speed (FFS) prevails on the freeway, and vehicles are almost completely unimpeded in their ability to maneuver within the traffic stream. The effects of incidents or point breakdowns are easily absorbed.
B	Represents reasonably free-flow operations, and FFS on the freeway is maintained. The ability to maneuver within the traffic stream is only slightly restricted, and the general level of physical and psychological comfort provided to drivers is still high. The effects of minor incidents and point breakdowns are still easily absorbed.
C	Provides for flow with speeds near the FFS of the freeway. Freedom to maneuver within the traffic stream is noticeably restricted, and lane changes require more care and vigilance on the part of the driver. Minor incidents may still be absorbed, but the local deterioration in service quality will be significant. Queues may be expected to form behind any significant blockages.
D	At this level speeds begin to decline with increasing flows, with density increasing more quickly. Freedom to maneuver within the traffic stream is seriously limited and drivers experience reduced physical and psychological comfort levels. Even minor incidents can be expected to create queuing, because the traffic stream has little space to absorb disruptions.
E	Describes operation at capacity. Operations on the freeway at this level are highly volatile because there are virtually no useable gaps within the traffic stream, leaving little room to maneuver within the traffic stream. Any disruption to the traffic stream, such as vehicles entering from a ramp or changing lanes, can establish a disruption wave that propagates throughout the upstream traffic flow. At capacity, the traffic stream has no ability to dissipate even the most minor disruption, and any incident can be expected to produce a serious breakdown and substantial queuing, the physical and psychological comfort afforded to drivers is poor.
F	<p>Describes breakdown, or unstable flow. Such conditions exist within queues forming behind bottlenecks. Breakdowns occur for a number of reasons:</p> <ul style="list-style-type: none">• Traffic incidents can temporarily reduce the capacity of a short segment, so that the number of vehicles arriving at a point is greater than the number of vehicles that can move through it.• Points of recurring congestion, such as merge or weaving segments and lane drops, experience very high demand in which the number of vehicles arriving is greater than the number of vehicles that can be discharged.• In analyses using forecast volumes, the projected flow rate can exceed the estimated capacity of a given location.

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Table 4 - Interrupted Traffic Flow Facilities	
LEVEL OF SERVICE	DEFINITION
A	Describes operations with a control delay of 10 s/veh or less and a volume-to- capacity ratio no greater than 1.0. This level is typically assigned when the volume-to- capacity ratio is low and either progression is exceptionally favorable or the cycle length is very short. If it is due to favorable progression, most vehicles arrive during the green indication and travel through the intersection without stopping.
B	Describes operations with a control delay between 10 and 20 s/veh and a volume-to-capacity ratio no greater than 1.0. This level is typically assigned when the volume-to- capacity ratio is low and either progression is highly favorable or the cycle length is short. More vehicles stop than with LOS A, with reasonably unimpeded travel between intersections.
C	Describes operations with control delay between 20 and 35 s/veh and a volume-to- capacity ratio no greater than 1.0. This level is typically assigned when progression is favorable or the cycle length is moderate. Individual cycle failures (i.e.one or more queued vehicles are not able to depart as a result of the insufficient capacity during the cycle) may begin to appear at this level. The number of vehicles stopping is significant, although many vehicles still pass through the intersection without stopping. May be longer queues and operations between locations may be more restricted.
D	Describes operations with control delay between 35 and 55 s/veh and a volume-to- capacity ratio no greater than 1.0. Travel speeds are about 40 percent below free flow speeds. This level is typically assigned when the volume-to-capacity ratio is high and either progression is ineffective or the cycle length is long. Many vehicles stop and individual cycle failures are noticeable.
E	Describes operations with control delay between 55 and 80 s/veh and a volume-to-capacity ratio no greater than 1.0. This level is typically assigned when the volume-to- capacity ratio is high, progression is unfavorable, and the cycle length is long. Individual cycle failures are frequent. Average travel speed is one-third of free flow speeds. The facility is generally at full capacity.
F	Describes operations with control delay exceeding 80 s/veh or a volume-to-capacity ratio greater than 1.0. This level is typically assigned when the volume-to-capacity ratio is very high, progression is very poor, and the cycle length is long. Most cycles fail to clear the queue. Extremely slow speeds with average delay of 80 seconds or more. Frequent stop and go conditions.

As indicated in the TIA, “Average Daily Traffic was estimated based on peak hour turning movement counts for local streets and roads[s], and Caltrans Traffic Volumes for SR 99.”¹¹ Existing traffic turning movements in the morning (A.M.) and afternoon (P.M.) peak, current peak hour levels of service (LOS) study intersections, and peak hour LOS for key roadway segments are the included in the TIA which concludes; **(see Tables 5 and 6)** it is evident that all intersections meet or exceed the County LOS D standard under all scenarios. In fact, all intersections and roadway segments currently perform at LOS C or better.”¹²

¹¹ Earlimart Community Plan Traffic Impact Assessment and Circulation Plan, Page 12. Prepared by VRPA Technologies (included as Appendix “E” of this DEIR).

¹² Ibid.

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Table 5 - Existing 2016 Intersection Analysis

Table 5 - Existing 2016 Intersection Analysis				
INTERSECTION	CONTROL	PEAK HOUR	EXISTING 2016	
			DELAY	LOS
1. Front Street/Sierra Street	All-Way Stop	A.M.	7.7	A
		P.M.	8.9	A
2. SR 99 SB Ramps/Sierra Avenue	One-Way Stop	A.M.	14.4	C
		P.M.	20.9	B
3.SR 99 NB Ramps/Sierra Avenue	One-Way Stop	A.M.	13.5	B
		P.M.	23.9	C
4. State Street/Sierra Avenue	One-Way Stop	A.M.	12.0	B
		P.M.	16.2	C
5.Chruch Road (Road 136)/Sierra Avenue	All-Way Stop	A.M.	8.6	A
		P.M.	10.6	B
6.Front Street/Armstrong Avenue	One-Way Stop	A.M.	9.9	A
		P.M.	10.4	B
7.SR 99 SB Off Ramp/Armstrong Avenue	One-Way Stop	A.M.	9.6	A
		P.M.	9.9	A
8. Market Road/Armstrong Avenue	One-Way Stop	A.M.	9.9	A
		P.M.	10.2	B
9. SR 99 NB On Ramp/Armstrong Avenue	Free	A.M.	-	A
		P.M.	-	A
10. SR 99 NB Off Ramp/Armstrong Avenue	Two-Way Stop	A.M.	11.1	B
		P.M.	12.6	B
<i>DELAY is measured in seconds</i> <i>LOS = Level of Service / BOLD denotes LOS standard has been exceeded</i> <i>For signalized and all-way stop controlled intersections, delay results show the average for the entire intersections.</i> <i>For one-way and two-way stop controlled intersections, delay results show the delay for the worst movement.</i>				

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Table 6 - Existing 2016 Segment Analysis

Table 6 - Existing 2016 Segment Analysis					
SEGMENT	DESCRIPTION	DIRECTION	PEAK HOUR	EXISTING 2016	
				VOLUME	LOS
1. Sierra Avenue: West of Front Street	Two-Lane Undivided	EB	A.M.	57	B
			P.M.	134	B
		WB	A.M.	81	B
			P.M.	124	B
2. Sierra Avenue: Front Street to State Street	Two-Lane Undivided	EB	A.M.	213	B
			P.M.	351	B
		WB	A.M.	259	B
			P.M.	321	B
3. Sierra Avenue: State Street to Church Street	Two-Lane Undivided	EB	A.M.	150	B
			P.M.	299	B
		WB	A.M.	241	B
			P.M.	305	B
4. Sierra Avenue: East of Church Street	Two-Lane Undivided	EB	A.M.	89	B
			P.M.	150	B
		WB	A.M.	105	B
			P.M.	170	B
5. Armstrong Avenue: Front Street to Church Street	Two-Lane Undivided	EB	A.M.	93	B
			P.M.	188	B
		WB	A.M.	108	B
			P.M.	172	B
LOS = Level of Service / BOLD denotes LOS standard has been exceeded					

Future Transportation Traffic Projections

“To project future traffic roadway conditions, a variety of sources were used. Historic population data indicate that as of the 2010 census, the CDP population was 8,537, up from 6,583 in 2000 census and 5,881 in 1990. This reflects an annual growth rate of just under two percent. The California Department of Finance and TCAG’s current RTP forecast indicates a slower population growth of about 1% per year.

Based on these and other data provided by TCAG and County planning staff, an overall rate of traffic growth of 1.3% per year was determined to be a reasonable annual forecast assumption. This rate of growth was applied to existing traffic count data to create future year (2032) traffic levels. This annual rate results in an overall growth in peak hour traffic of approximately 23% for the period 2016-2032. For consistency with TCAG and Caltrans forecasts, a lower annual traffic growth rate of 1% was applied to SR 99 volumes.”¹³

¹³ Ibid. 16 and 17.

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The TIA includes future year traffic/circulation projections for Year 2032. Intersection turning movements at Study intersections, delay, and LOS results at all intersections in both the A.M. and P.M. peak hours and peak hour LOS results for key roadway segments for Year 2032. As concluded in the TIA (**see Tables 7 and 8**); it is evident that all intersections meet or exceed the County LOS D standard with the addition of projected traffic. Indeed, all intersections and roadway segments will continue to perform at LOS C or better.”¹⁴

Table 7 - Future 2032 Intersection Analysis				
SEGMENT	CONTROL	PEAK HOUR	FUTURE 2032	
			DELAY	LOS
1. Front Street/Sierra Street	All-Way Stop	A.M.	8.1	A
		P.M.	9.1	A
2. SR 99 SB Ramps/Sierra Avenue	One-Way Stop	A.M.	19.5	C
		P.M.	23.2	C
3.SR 99 NB Ramps/Sierra Avenue	One-Way Stop	A.M.	14.9	B
		P.M.	20.6	C
4. State Street/Sierra Avenue	One-Way Stop	A.M.	13.4	B
		P.M.	18.2	C
5.Chruch Road (Road 136)/Sierra Avenue	All-Way Stop	A.M.	8.7	A
		P.M.	11.1	B
6.Front Street/Armstrong Avenue	One-Way Stop	A.M.	10.3	B
		P.M.	10.8	B
7.SR 99 SB Off Ramp/Armstrong Avenue	One-Way Stop	A.M.	10.0	B
		P.M.	10.2	B
8. Market Road/Armstrong Avenue	One-Way Stop	A.M.	10.1	B
		P.M.	10.2	B
9. SR 99 NB On Ramp/Armstrong Avenue	Free	A.M.	-	A
		P.M.	-	A
10. SR 99 NB Off Ramp/Armstrong Avenue	Two-Way Stop	A.M.	11.4	B
		P.M.	13.8	B
DELAY is measured in seconds LOS = Level of Service / BOLD denotes LOS standard has been exceeded For signalized and all-way stop controlled intersections, delay results show the average for the entire intersections. For one-way and two-way stop controlled intersections, delay results show the delay for the worst movement.				

¹⁴ Op. Cit. 17.

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Table 8 - Future 2030 Segment Analysis					
SEGMENT	DESCRIPTION	DIRECTION	PEAK HOUR	FUTURE 2032	
				VOLUME	LOS
1. Sierra Avenue: West of Front Street	Two-Lane Undivided	EB	A.M.	70	B
			P.M.	165	B
		WB	A.M.	100	B
			P.M.	153	B
2. Sierra Avenue: Front Street to State Street	Two-Lane Undivided	EB	A.M.	276	B
			P.M.	439	B
		WB	A.M.	349	B
			P.M.	401	B
3. Sierra Avenue: State Street to Church Street	Two-Lane Undivided	EB	A.M.	193	B
			P.M.	372	B
		WB	A.M.	311	B
			P.M.	379	B
4. Sierra Avenue: East of Church Street	Two-Lane Undivided	EB	A.M.	112	B
			P.M.	187	B
		WB	A.M.	134	B
			P.M.	211	B
5. Armstrong Avenue: Front Street to Church Street	Two-Lane Undivided	EB	A.M.	126	B
			P.M.	237	B
		WB	A.M.	163	B
			P.M.	218	B

LOS = Level of Service / **BOLD** denotes LOS standard has been exceeded

Caltrans Highway 99 (2003) Concept Report

The Caltrans 2003 Transportation Concept Report (TCR) shows Earlimart is within segment 14 of SR 99, which is planned to be widened from a 4 to 6 lane freeway and ultimately widened to an 8-lane facility.

Road Conditions

“There are various roadways in Earlimart that are in need of repair (see **Table 9**). Over time, roadway pavement can become damaged or begin to fail due to fatigue, aging, or surface abrasion. The binding agent within road pavement becomes rigid and less flexible as time passes and the surface of the pavement may start losing aggregates. If timely maintenance does not occur, potholes will start to occur within the road.”¹⁵

Table 9 - Road Maintenance Strategies			
No.	Roadway	Limits	Repair Code
1	Alfalfa Road	Washington Avenue to Center Avenue	CHIP
2	Alila Street	Kovacevich Street to Andrea Avenue	CHIP
3	Armstrong Avenue	Rossi Road to Valente Road	CHIP
4	Armstrong Avenue	Elm Road to Dietz Road	CHIP
5	Ash Street	Clay Avenue to Washington Avenue	CHIP
6	Bent Ranch Avenue	Spring Road to Dove Road	CHIP
7	Cable Avenue	Rossi Road to Valente Road	CHIP
8	Camelia Drive	Mariposa Road to Primavera Court	CHIP
9	Cannon Avenue	Elm Road to Lane Avenue	CHIP

¹⁵ Tulare County Housing Element – Action Program 9, page 4-7

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Table 9 - Road Maintenance Strategies

10	Cardinal Avenue	Muscat Street to Thompson Road	CHIP
11	Cedar Avenue	Front Street to east end	GRX
12	Center Avenue	Valente Road to west end	CHIP
13	Center Avenue	Front Street to Market Road	CHIP
14	Chaparral Street	Front Street to east end	ACST
15	Church Street	Armstrong Avenue to Tulare Avenue	GRX
16	Church Street	Cable Avenue to Spruce Avenue	RCST
17	Church Street	Tulare Avenue to Franklin Avenue	CHIP
18	Church Street	Franklin Avenue to Washington Avenue	GRX
19	Church Street	Sutter Avenue to Sierra Avenue	CHIP
20	Church Street	Sierra Avenue to Bobbi Avenue	OLAY
21	Citrus Road	Washington Avenue to Center Avenue	CHIP
22	Clay Avenue	Ash Street to west end	ACST
23	Clay Avenue	Ash Street to Davis Street	CHIP
24	Clay Avenue	Front Street to Market Road	CHIP
25	Davis Street	Clay Avenue to Washington Avenue	CHIP
26	Dietz Road	Armstrong Avenue to Washington Avenue	CHIP
27	Dietz Road	Washington Avenue to Sierra Avenue	GRX
28	Dove Road	Bent Ranch Avenue to Kovacevich Street	CHIP
29	Dove Road	Sierra Avenue to Marin Avenue	CHIP
30	Earlimart Avenue	Quail Avenue to Kovacevich Street	CHIP
31	Elm Road	Spruce Avenue to Armstrong Avenue	CHIP
32	Franklin Avenue	State Street to west end	RCST
33	Franklin Avenue	Elm Road to Lane Avenue	CHIP
34	Front Street	Cedar Avenue to south end	CHIP
35	Front Street	Cedar Avenue to Armstrong Avenue	GRX
36	Front Street	Armstrong Avenue to Sutter Avenue	CHIP
37	Front Street	Sutter Avenue to north end	GRX
38	Kelly Avenue	Front Street to Market Road	CHIP
39	Kenneth Avenue	Elm Road to Lane Avenue	OLAY
40	Kern Avenue	State Street to Church Street	GRX
41	Kovacevich Street	Spring Road to Church Road	CHIP
42	Lane Avenue	Franklin Avenue to Cannon Avenue	CHIP
43	Lane Avenue	Kenneth Avenue to Mary Ann Avenue	CHIP
44	Lincoln Avenue	Alfalfa Road to Citrus Road	CHIP
45	Marin Avenue	Molly Road to Church Road	CHIP
46	Market Road	Armstrong Avenue to Clay Avenue	CHIP
47	Market Road	Clay Avenue to Washington Avenue	OLAY
48	Market Road	Sutter Avenue to Kelly Avenue	CHIP
49	Mary Ann Avenue	Elm Road to Lane Avenue	OLAY
50	Muscat Court	South of Martin Avenue to north of Parade Ave	CHIP
51	Muscat Street	Cardinal Avenue to Sierra Avenue	CHIP
52	Olive Road	Spruce Avenue to Armstrong Avenue	GRX
53	Olympia Street	Front Street to east end	OLAY
54	Quail Avenue	Spring Road to Dove Road	CHIP
55	Rhoden Court	Ash Street to west end	CHIP
56	Rossi Road	Cable Avenue to Armstrong Avenue	RCST
57	School Avenue	Church Road to Elm Road	GRX
58	School Avenue	Lane Road to east end	GRX
59	Sierra Avenue	Front Street to State Street	OLAY
60	Sierra Avenue	State Street to Church Road	CHIP

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Table 9 - Road Maintenance Strategies

61	Sierra Avenue	Thompson Road to Dietz Road	CHIP
62	Spring Road	Sutter Avenue to Kovacevich Street	CHIP
63	Spruce Avenue	State Drive to Church Road	GRX
64	Spruce Avenue	Church Road to Oak Road	CHIP
65	State Street	Spruce Avenue to School Avenue	CHIP
66	State Street	School Avenue to Clay Avenue	GRX
67	State Street	Clay Avenue to Washington Avenue	CHIP
68	State Street	Sutter Avenue to Sierra Avenue	GRX
69	Sutter Avenue	Muscat Street to east end	CHIP
70	Tulare Avenue	State Street to Church Street	GRX
71	Valente Road	Cable Avenue to Sierra Avenue	CHIP
72	Washington Avenue	Church Road to Dietz Road	CHIP
73	Washington Avenue	Ash Street to Citrus Road	GRX
74	Washington Avenue	Citrus Road to Front Street	CHIP
75	Wilson Avenue	State Street to west end	RCST

OLAY = Overlay resurfacing operation

CHIP = Chip Seal

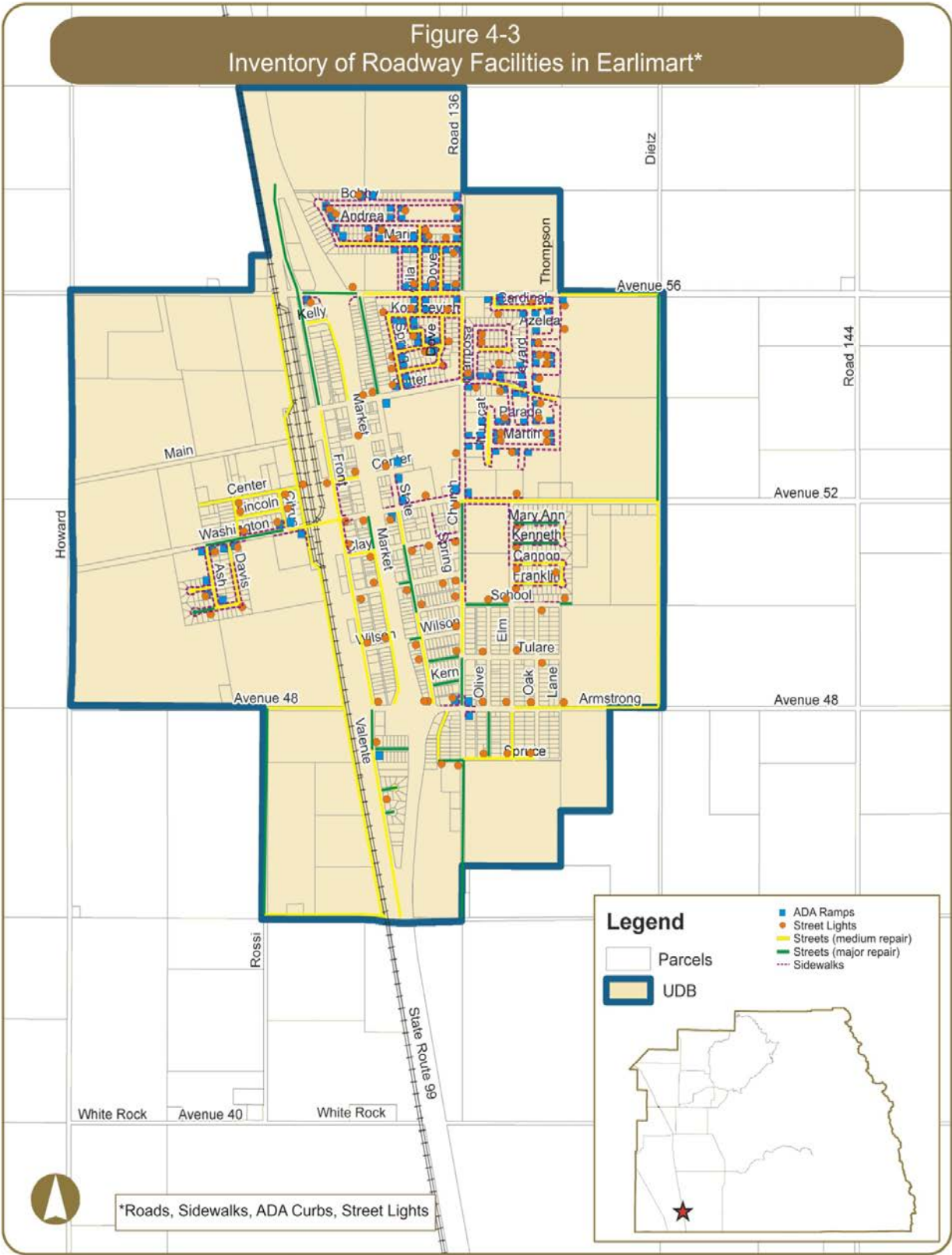
GRXx = Grind and remix

ACST = asphalt reconstruction

RCST = cold mix reconstruction

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Figure 12 - Inventory of Roadway Facilities in Earlimart



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Sidewalks

Sidewalks are typically separated from a roadway by a curb and accommodate pedestrian travel. They improve mobility for those with disabilities and are also an important part of walking routes to schools. They provide the space for pedestrians to travel within the public right-of-way while being separated from vehicles and bicycles.

The 2010 California Building Code identifies a clear width minimum of 48 inches for sidewalks. This clear width minimum is the walkway width that is completely free of obstacles and not necessarily the sidewalk width. However, the 48-inch minimum does not provide sufficient passing space or space for two-way travel. Therefore, the guidelines state that for sidewalks less than 5 feet in width, passing lanes (wide enough for wheelchairs) shall be provided at 200-foot intervals. However, the clear width may be reduced to 3 feet if the enforcing agency determines that compliance with the 4-foot clear sidewalk width would create an unreasonable hardship due to right-of-way restrictions, natural barriers or other existing conditions.

The County and VRPA Technologies surveyed existing sidewalks within the Community. **Table 10** identifies the location of existing sidewalks in Earlimart. **Figure 12** also displays this information graphically. The sidewalks represented in **Table 10** and **Figure 12** do not distinguish between ADA compliant sidewalks and noncompliant sidewalks. The majority of sidewalks represented below were constructed prior to current ADA guidelines and would be considered non-ADA compliant facilities. Such noncompliant facilities would require complete reconstruction to be considered ADA compliant.

Table 10 - Location of Existing Sidewalks			
No.	Roadway	Limits	Location
1	Alila Street	Kovacevich Street to Andrea Avenue	East side
2	Alila Street	Kovacevich Street to Andrea Avenue	West side
3	Andrea Avenue	Molly Road to Church Street	North side
4	Andrea Avenue	Molly Road to Church Street	South side
5	Armstrong Avenue	State Street to Church Street	North side
6	Ash Street	Washington Avenue to Clay Avenue	East side
7	Ash Street	Washington Avenue to Clay Avenue	West side
8	Azalea Avenue	Vineyard Road to Thompson Road	North side
9	Azalea Avenue	Vineyard Road to Thompson Road	South side
10	Azalea Court	Mariposa Road to La Primavera Avenue	North side
11	Azalea Court	Mariposa Road to La Primavera Avenue	South side
12	Bent Ranch Avenue	Spring Road to Dove Road	North side
13	Bent Ranch Avenue	Spring Road to Dove Road	South side
14	Bent Ranch Avenue	Vineyard Road to Thompson Road	North side
15	Bent Ranch Avenue	Vineyard Road to Thompson Road	South side
16	Bent Ranch Avenue	Muscat Road to Thompson Road	North side
17	Bent Ranch Avenue	Muscat Road to Thompson Road	South side
18	Bobbi Avenue	Molly Road to Church Street	North side
19	Bobbi Avenue	Molly Road to Church Street	South side
20	Bobbi Avenue	Bobbi Avenue to Andrea Avenue	East side
21	Bobbi Avenue	Bobbi Avenue to Andrea Avenue	West side

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Table 10 - Location of Existing Sidewalks

No.	Roadway	Limits	Location
22	Camelia Drive	Mariposa Road to La Primavera Avenue	North side
23	Camelia Drive	Mariposa Road to La Primavera Avenue	South side
24	Cannon Avenue	Elm Road to Lane Avenue	North side
25	Cannon Avenue	Elm Road to Lane Avenue	South side
26	Cardinal Avenue	Muscat Road to Thompson Road	North side
27	Cardinal Avenue	Muscat Road to Thompson Road	South side
28	Center Avenue	Church Road to Muscat Road	North side
29	Center Avenue	Church Road to Muscat Road	South side
30	Church Road	School Avenue to Kovacevich Street	East side
31	Church Street	Washington Avenue to Center Avenue	West side
32	Church Street	Sutter Avenue to Bobbi Avenue	West side
33	Clay Avenue	Davis Street to west end	North side
34	Clay Avenue	Davis Street to west end	South side
35	Clay Avenue	State Street to Church Street	South side
36	Clay Avenue	Spring Road to Church Street	North side
37	Davis Street	Washington Avenue to Clay Avenue	East side
38	Davis Street	Washington Avenue to Clay Avenue	West side
39	Diane Street	Andrea Avenue to Marin Avenue	East side
40	Diane Street	Andrea Avenue to Marin Avenue	West side
41	Dove Road	Kovacevich Street to Bent Ranch Avenue	East side
42	Dove Road	Kovacevich Street to Bent Ranch Avenue	West side
43	Dove Road	Marin Avenue to Sierra Avenue	East side
44	Dove Road	Marin Avenue to Sierra Avenue	West side
45	Earlimart Avenue	Kovacevich Street to Quail Avenue	East side
46	Earlimart Avenue	Kovacevich Street to Quail Avenue	West side
47	Earlimart Avenue	Marin Avenue to Sierra Avenue	East side
48	Earlimart Avenue	Marin Avenue to Sierra Avenue	West side
49	Elm Road	Cannon Avenue to Washington Avenue	West side
50	Elm Road	Kenneth Avenue to Washington Avenue	East side
51	Franklin Avenue	Elm Road to Lane Avenue	North side
52	Franklin Avenue	Elm Road to Lane Avenue	South side
53	Front Street	Center Avenue to Clay Avenue	East side
54	Kenneth Avenue	Elm Road to Lane Avenue	North side
55	Kenneth Avenue	Elm Road to Lane Avenue	South side
56	Kovacevich Street	Spring Road to Church Street	North side
57	Kovacevich Street	Spring Road to Church Street	South side
58	La Primavera Avenue	Azalea Court to Camelia Drive	East side
59	La Primavera Avenue	Azalea Court to Camelia Drive	West side
60	Marin Avenue	Molly Road to Church Street	North side
61	Marin Avenue	Molly Road to Church Street	South side
62	Mariposa Road	Azalea Court to Sutter Avenue	East side
63	Mariposa Road	Azalea Court to Sutter Avenue	West side
64	Martin Avenue	Muscat Road to Thompson Road	North side
65	Martin Avenue	Muscat Road to Thompson Road	South side

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Table 10 - Location of Existing Sidewalks

No.	Roadway	Limits	Location
66	Mary Ann Avenue	Elm Road to Lane Avenue	North side
67	Mary Ann Avenue	Elm Road to Lane Avenue	South side
68	Molly Road	Marin Avenue to Bobbi Avenue	East side
69	Molly Road	Marin Avenue to Bobbi Avenue	West side
70	Muscat Road	Sierra Avenue to Cardinal Avenue	East side
71	Muscat Road	Sierra Avenue to Cardinal Avenue	West side
72	Muscat Road	Sutter Avenue to Bent Ranch Avenue	East side
73	Muscat Road	Sutter Avenue to Bent Ranch Avenue	West side
74	Muscat Road	North of Parade Avenue to South of Martin Avenue	East side
75	Muscat Road	North of Parade Avenue to South of Martin Avenue	West side
76	Parade Avenue	Muscat Road to Thompson Road	North side
77	Parade Avenue	Muscat Road to Thompson Road	South side
78	Primavera Court	Sutter Avenue to Parade Avenue	East side
79	Primavera Court	Sutter Avenue to Parade Avenue	West side
80	Primavera Court	Martin Avenue to south end	East side
81	Primavera Court	Martin Avenue to south end	West side
82	Quail Avenue	Spring Road to Dove Road	North side
83	Quail Avenue	Spring Road to Dove Road	South side
84	Rhoden Court	Ash Street to west end	North side
85	Rhoden Court	Ash Street to west end	South side
86	School Avenue	Elm Road to Lane Avenue	North side
87	Sierra Avenue	Front Street to SR 99	South side
88	Sierra Avenue	State Street to Church Street	South side
89	Sierra Avenue	Earlimart Avenue to Church Street	North side
90	Sierra Avenue	Muscat Road to Thompson Road	South side
91	Spring Road	Washington Avenue to Clay Avenue	East side
92	Spring Road	Kovacevich Street to Sutter Avenue	East side
93	Spring Road	Kovacevich Street to Sutter Avenue	West side
94	State Street	Center Avenue to Washington Avenue	East side
95	State Street	Center Avenue to Washington Avenue	West side
96	Sutter Avenue	Spring Road to east end	North side
97	Sutter Avenue	Church Street to east end	South side
98	Thompson Road	Sierra Avenue to Azalea Avenue	East side
99	Thompson Road	Sierra Avenue to Azalea Avenue	West side
100	Thompson Road	Bent Ranch Avenue to Bent Ranch Avenue	East side
101	Thompson Road	Bent Ranch Avenue to Bent Ranch Avenue	West side
102	Thompson Road	Martin Avenue to north end	East side
103	Thompson Road	Martin Avenue to north end	West side
104	Vineyard Road	Cardinal Avenue to Parade Avenue	East side
105	Vineyard Road	Cardinal Avenue to Parade Avenue	West side
106	Washington Avenue	Ash Street to Alfalfa Road	South side
107	Washington Avenue	200' west of Citrus Road to Valente Road	South side
108	Washington Avenue	Alfalfa Road to Citrus Road	North side
109	Washington Avenue	Church Road to Elm Road	South side

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Table 10 - Location of Existing Sidewalks

No.	Roadway	Limits	Location
110	Washington Avenue	Market Road to Elm Road	North side

ADA Curb Ramps

The Americans with Disabilities Act (ADA) of 1990 included design requirements for persons with disabilities in the public rights-of-way. Curb ramps are an important part of making sidewalks and street crossings accessible to people with disabilities (especially those who use wheelchairs). An ADA compliant curb ramp is a short ramp cutting through or built up to a curb. It consists of the ramp itself which is sloped to allow wheelchair access from the street to the sidewalk and flared sides that bring the curb to the level of the street.

Curb ramps are most typically found at intersections, but can also be located near on-street parking, transit stations and stops, and midblock crossings. Title II regulations require curb ramps at existing and new facilities.

The County of Tulare completed a survey of ADA compliant ramps within the communities in August 2012. According to the survey, there are various ADA compliant curb ramps located within Earlimart and are listed in **Table 11** and displayed in **Figure 12**.

Table 11 - Location of Existing ADA Ramps

No.	East-West Roadway	North-South Roadways	Location
1	Andrea Avenue	Molly Road	NE Corner
2	Andrea Avenue	Molly Road	SE Corner
3	Andrea Avenue	Diane Street	SE Corner
4	Andrea Avenue	Diane Street	SW Corner
5	Andrea Avenue	Bobbi Avenue	NE Corner
6	Andrea Avenue	Bobbi Avenue	NW Corner
7	Andrea Avenue	Church Road	NW Corner
8	Armstrong Avenue	Church Street	NE Corner
9	Armstrong Avenue	Church Street	NW Corner
10	Azalea Avenue	Vineyard Road	NE Corner
11	Azalea Avenue	Vineyard Road	SE Corner
12	Bent Ranch Avenue	Muscat Lane	SE Corner
13	Bent Ranch Avenue	Vineyard Road	NE Corner
14	Bent Ranch Avenue	Vineyard Road	NW Corner
15	Bent Ranch Avenue	Vineyard Road	SE Corner
16	Bent Ranch Avenue	Vineyard Road	SW Corner
17	Bent Ranch Avenue	Thompson Road	NW Corner
18	Bent Ranch Avenue	Spring Road	SE Corner
19	Bent Ranch Avenue	Spring Road	NE Corner
20	Bobbi Avenue	Molly Road	SE Corner
21	Bobbi Avenue	Between Molly Road and Bobbi Ave	NE Corner
22	Bobbi Avenue	Between Molly Road and Bobbi Ave	NW Corner
23	Bobbi Avenue	Bobbi Avenue	SE Corner
24	Bobbi Avenue	Bobbi Avenue	SW Corner

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Table 11 - Location of Existing ADA Ramps

No.	East-West Roadway	North-South Roadways	Location
25	Bobbi Avenue	Church Road	NW Corner
26	Bobbi Avenue	Church Road	SW Corner
27	Camelia Drive	Vineyard Road	NE Corner
28	Camelia Drive	Thompson Road	SW Corner
29	Camelia Drive	Vineyard Road	SE Corner
30	Cardinal Avenue	Muscat Lane	NE Corner
31	Cardinal Avenue	Vineyard Road	SE Corner
32	Cardinal Avenue	Vineyard Road	SW Corner
33	Cardinal Avenue	Thompson Road	NW Corner
34	Cardinal Avenue	Thompson Road	SW Corner
35	Cedar Avenue	Front Street	SE Corner
36	Center Avenue	State Street	NE Corner
37	Center Avenue	State Street	SE Corner
38	Center Avenue	Church Street	NE Corner
39	Center Avenue	Church Street	SE Corner
40	Center Avenue	Muscat Lane	NW Corner
41	Center Avenue	Muscat Lane	SW Corner
42	Clay Avenue	Ash Street	NE Corner
43	Kovacevich Street	Spring Road	SE Corner
44	Kovacevich Street	Earlimart Avenue	SE Corner
45	Kovacevich Street	Earlimart Avenue	SW Corner
46	Kovacevich Street	Alila Street	NE Corner
47	Kovacevich Street	Alila Street	NW Corner
48	Marin Avenue	Molly Road	NE Corner
49	Marin Avenue	Diane Street	NE Corner
50	Marin Avenue	Earlimart Avenue	SE Corner
51	Marin Avenue	Earlimart Avenue	SW Corner
52	Marin Avenue	Alila Street	NE Corner
53	Marin Avenue	Alila Street	NW Corner
54	Marin Avenue	Alila Street	SE Corner
55	Marin Avenue	Alila Street	SW Corner
56	Marin Avenue	Dove Road	SE Corner
57	Marin Avenue	Dove Road	SW Corner
58	Marin Avenue	Church Road	NW Corner
59	Marin Avenue	Church Road	SW Corner
60	Martin Avenue	Muscat Lane	NE Corner
61	Martin Avenue	Muscat Lane	SE Corner
62	Martin Avenue	Primavera Court	SE Corner
63	Martin Avenue	Primavera Court	SW Corner
64	Martin Avenue	Thompson Road	NW Corner
65	North of Quail Avenue	Earlimart Avenue	SW Corner (at bend)
66	Parade Avenue	Muscat Lane	NE Corner
67	Parade Avenue	Muscat Lane	NW Corner
68	Parade Avenue	Muscat Lane	SE Corner

EARLIMART COMMUNITY PLAN UPDATE

Table 11 - Location of Existing ADA Ramps

No.	East-West Roadway	North-South Roadways	Location
69	Parade Avenue	Primavera Court	NW Corner
70	Parade Avenue	Vineyard Road	NE Corner
71	Parade Avenue	Vineyard Road	NW Corner
72	Parade Avenue	Thompson Road	NW Corner
73	Parade Avenue	Thompson Road	SW Corner
74	Quail Avenue	Spring Road	NE Corner
75	Quail Avenue	Spring Road	SE Corner
76	Quail Avenue	Earlimart Avenue	NE Corner
77	Quail Avenue	Earlimart Avenue	NW Corner
78	Quail Avenue	Dove Road	NW Corner
79	Quail Avenue	Dove Road	SW Corner
80	Rhoden Court	Ash Street	NW Corner
81	Rhoden Court	Ash Street	SW Corner
82	Sierra Avenue	Muscat Lane	SE Corner
83	Sierra Avenue	Muscat Lane	SW Corner
84	Sierra Avenue	Thompson Road	SE Corner
85	Sierra Avenue	Thompson Road	SW Corner
86	Sierra Avenue	Front Street	SE Corner
87	Sierra Avenue	Earlimart Avenue	NE Corner
88	Sierra Avenue	Alila Street	NE Corner
89	Sierra Avenue	Alila Street	NW Corner
90	Sierra Avenue	Alila Street	SE Corner
91	Sierra Avenue	Alila Street	SW Corner
92	Sierra Avenue	Dove Road	NE Corner
93	Sierra Avenue	Dove Road	NW Corner
94	Sierra Avenue	Church Road	NW Corner
95	Sierra Avenue	Church Road	SW Corner
96	South of Kovacevich St.	Earlimart Avenue	NE Corner (at bend)
97	Sutter Avenue	State Street	SE Corner
98	Sutter Avenue	Spring Road	NE Corner
99	Sutter Avenue	Spring Road	NW Corner
100	Sutter Avenue	Church Street	NE Corner
101	Sutter Avenue	Church Street	SE Corner
102	Sutter Avenue	Mariposa Road	NE Corner
103	Sutter Avenue	Mariposa Road	NW Corner
104	Sutter Avenue	Muscat Lane	NE Corner
105	Sutter Avenue	Muscat Lane	NW Corner
106	Sutter Avenue	Primavera Court	SE Corner
107	Sutter Avenue	Primavera Court	SW Corner
108	Sutter Avenue	Vineyard Road	NE Corner
109	Sutter Avenue	Vineyard Road	NW Corner
110	Sutter Avenue	Vineyard Road	SE Corner
111	Sutter Avenue	Vineyard Road	SW Corner
112	Washington Avenue	Ash Street	SE Corner

EARLIMART COMMUNITY PLAN UPDATE

Table 11 - Location of Existing ADA Ramps

No.	East-West Roadway	North-South Roadways	Location
113	Washington Avenue	Ash Street	SW Corner
114	Washington Avenue	Davis Street	SE Corner
115	Washington Avenue	Davis Street	SW Corner
116	Washington Avenue	Citrus Road	NE Corner
117	Parade Avenue	Vineyard Road	NW Corner
118	Washington Avenue	Citrus Road	NW Corner
119	Washington Avenue	Valente Road	SW Corner
120	Washington Avenue	State Street	SW Corner
121	Washington Avenue	State Street	NE Corner
122	Washington Avenue	Church Street	NE Corner

Source: County of Tulare Public Works, August 2013

Street Lights

Street lights are typically located at the edge of roadways on top of utility poles. They are illuminated at night and improve the visibility and safety of the roadway and sidewalk by increasing motorist visibility and improving nighttime pedestrian security. They can also reduce nighttime pedestrian crashes by increasing the awareness of drivers relative to pedestrians.

Table 12 identifies the location of existing street lights that are maintained by Tulare County, in Earlimart, as well as their specifications. **Figure 12** also displays this information graphically. The below table specifies the locations, the pole number, lumens, pole type, arm direction and utility provider. Pole numbers can be arbitrary and are used to match the pole specifications with its location. Lumens measure the amount of light emitted from the bulb (the more lumens the brighter the light). The pole type "W" represents a wood post for which the light is commonly shared with a Utility provider. Similarly, "M" represents metal and "C" represents concrete.

Table 12 - Existing Street Lights

No.	East-West Roadway	North-South Roadway	Location	Pole	Lumens	Pole	Arm Directio	Utility
1	135' South of Olympia Street	Front Street	East Side	4097790E	5800	W	W	SCE
2	Andrea Avenue	Alila Street	SE Corner	N/A	5800	C	N	SCE
3	Andrea Avenue	Diane Street	SE Corner	4347745E	5800	C	N	SCE
4	Andrea Avenue	Molly Road	NE Corner	4347742E	5800	C	W	SCE
5	Andrea Avenue	Church Street	SW Corner	N/A	5800	C	E	SCE
6	Armstrong Avenue	Olive Road	NW Corner	1342465E	5800	W	S	SCE
7	Armstrong Avenue	Front Street	NE Corner	345598E	5800	W	S	SCE
8	Armstrong Avenue	Market Road	NE Corner	4320584E	5800	W	S	SCE
9	Armstrong Avenue	State Street	NW Corner	995215E	5800	W	S	SCE
10	Armstrong Avenue	Church Street	NW Corner	318547E	5800	W	S/E	SCE
11	Armstrong Avenue	Elm Road	NW Corner	1342466E	5800	W	S	SCE
12	Armstrong Avenue	Oak Road	NW Corner	1342467E	5800	W	S	SCE
13	Armstrong Avenue	Lane Avenue	NE Corner	1342471E	5800	W	S	SCE
14	At bend	Earlimart Avenue	West Side	2353355E	5800	O	N/E	SCE
15	Azalea Avenue	Thompson Road	NE Corner	4399513E	5800	C	W	SCE
16	Azalea Avenue	Vineyard Road	SE Corner	4381438E	5800	C	N	SCE
17	Azelia Court	Mariposa Road	SE Corner	N/A	5800	C	N/A	SCE

EARLIMART COMMUNITY PLAN UPDATE

Table 12 - Existing Street Lights

No.	East-West Roadway	North-South Roadway	Location	Pole	Lumens	Pole	Arm Directio	Utility
18	Bent Ranch Avenue	Spring Road	NW Corner	2353351E	5800	O	E	SCE
19	Bent Ranch Avenue	Dove Road	South Side	2353359E	5800	O	N/W	SCE
20	Bent Ranch Avenue	Thompson Road	NW Corner	4399515E	5800	C	E	SCE
21	Bent Ranch Avenue	Vineyard Road	SE Corner	4381436E	5800	C	N	SCE
22	Bent Ranch Avenue	Muscat Road	NE Corner	4381434E	5800	C	S	SCE
23	Bobbi Avenue	Molly Road	SE Corner	4381809E	5800	C	N	SCE
24	Bobbi Avenue	Diane Street	NW Corner	4227088E	5800	C	S	SCE
25	Bobbi Avenue	Earlimart Avenue	SE Corner	4381808E	5800	C	N	SCE
26	Bobbi Avenue	Church Street	SW Corner	4381807	5800	C	E	SCE
27	Camelia Drive	Vineyard Road	SE Corner	4381437E	5800	C	N	SCE
28	Camelia Drive	Thompson Road	SW Corner	4399514E	5800	C	E	SCE
29	Camelia Drive	Mariposa Road	NE Corner	N/A	5800	C	N/A	SCE
30	Cannon Avenue	Elm Road	SE Corner	1401017E	5800	W	W	SCE
31	Cardinal Avenue	Muscat Road	SE Corner	N/A	5800	C	N	SCE
32	Cardinal Avenue	Vineyard Road	NE Corner	4381439E	5800	C	S	SCE
33	Cardinal Avenue	Thompson Road	East Side	4399512E	5800	C	W	SCE
34	Cedar Avenue	Front Street	NE Corner	1761149E	5800	W	W	SCE
35	Center Alignment	Church Street	NW Corner	953.89E	5800	W	S/E	SCE
36	Center Avenue	Alfalfa Road	SE Corner	2236399E	5800	O	N	SCE
37	Center Avenue	Citrus Road	SE Corner	2296396E	5800	O	N	SCE
38	Center Avenue	State Street	NW Corner	4431T	5800	W	E	SCE
39	Center Avenue	Market Road	NW Corner	934921E	5800	W	E	SCE
40	Center Avenue	Valente Road	East Side	765316E	5800	W	W	SCE
41	Center Avenue	Front Street	West Side	496327E	5800	W	W	SCE
42	Clay Avenue	Market Road	SW Corner	2282350E	5800	W	E	SCE
43	Clay Avenue	Ash Street	South Side	2282307E	5800	O	N	SCE
44	Clay Avenue	Davis Street	SE Corner	2282350E	5800	O	N/W	SCE
45	Clay Avenue	Front Street	NE Corner	1193068E	5800	W	W	SCE
46	Clay Avenue	State Street	SE Corner	1955144E	5800	W	W	SCE
47	Clay Avenue	Spring Road	SW Corner	7440039E	5800	W	N	SCE
48	Clay Avenue	Church Street	SW Corner	599842E	5800	W	N/E	SCE
49	Franklin Avenue	Church Street	SW Corner	528902E	5800	W	E	SCE
50	Franklin Avenue	Elm Road	SE Corner	1401015E	5800	W	W	SCE
51	Franklin Avenue	Front Street	SE Corner	4067587E	5800	W	W	SCE
52	Franklin Avenue	State Street	SW Corner	3194T	5800	W	E	SCE
53	Franklin Avenue	Spring Road	South Side	527350E	5800	W	N	SCE
54	Franklin Avenue	Market Road	NW Corner	1293924E	5800	W	E	SCE
55	Kenneth Avenue	Elm Road	SE Corner	1401019E	5800	W	W	SCE
56	Kovacevich Street	Spring Road	West Side	2353353E	5800	O	E	SCE
57	Kovacevich Street	60' east of Earlimart	North Side	2353354E	5800	O	S	SCE
58	Kovacevich Street	Dove Road	NE Corner	2353357E	5800	O	S	SCE
59	Lincoln Avenue	Alfalfa Road	NE Corner	2296398E	5800	O	W	SCE
60	Lincoln Avenue	Citrus Road	East Side	2296395E	5800	O	W	SCE
61	Main Avenue	Market Road	East Side	2282348E	5800	W	W	SCE
62	Marin Avenue	Alila Street	NE Corner	2286997E	5800	C	N	SCE
63	Marin Avenue	Dove Road	NE Corner	2286996E	5800	C	N	SCE
64	Marin Avenue	Church Street	SW Corner	2286995E	5800	C	E	SCE
65	Marin Avenue	Earlimart Avenue	NW Corner	4261646E	5800	C	S	SCE
66	Marin Avenue	Diane Street	NW Corner	N/A	5800	C	S	SCE
67	Martin Avenue	Thompson Road	NW Corner	4509678	5800	C	E	SCE
68	Martin Avenue	Primavera Court	SW Corner	4509677E	5800	C	N	SCE
69	Martin Avenue	Muscat Road	NE Corner	4509676E	5800	C	W	SCE
70	Mary Ann Avenue	Elm Road	SE Corner	1401021E	5800	W	W	SCE

EARLIMART COMMUNITY PLAN UPDATE

Table 12 - Existing Street Lights

No.	East-West Roadway	North-South Roadway	Location	Pole	Lumens	Pole	Arm Directio	Utility
71	North of Franklin	Lane Avenue	West Side	1955143E	5800	W	E	SCE
72	Parade Avenue	Muscat Road	SE Corner	N/A	5800	C	W	SCE
73	Parade Avenue	Primavera Court	NW Corner	4483116E	5800	C	S	SCE
74	Parade Avenue	Vineyard Road	NE Corner	4483117E	5800	C	S	SCE
75	Parade Avenue	Thompson Road	SW Corner	4483118E	5800	C	E	SCE
76	Quail Avenue	Spring Road	West Side	2353352E	5800	O	E	SCE
77	Quail Avenue	Earlimart Avenue	SE Corner	2353356E	5800	O	N	SCE
78	Quail Avenue	Dove Road	NE Corner	2353358E	5800	O	W	SCE
79	Rhoden Court	Ash Street	SW Corner	4122708E	5800	N/A	E	SCE
80	School Avenue	Olive Road	North Side	2191757E	5800	W	S	SCE
81	School Avenue	Oak Road	SE Corner	2111468	5800	W	N	SCE
82	School Avenue	State Street	NE Corner	2111469	5800	W	W	SCE
83	School Avenue	Church Street	NW Corner	671032E	5800	W	S/E	SCE
84	School Avenue	Elm Road	NW Corner	1342599	5800	W	S	SCE
85	School Avenue	Lane Avenue	NE Corner	1342452	5800	W	S/W	SCE
86	Sierra Avenue	Front Street	SE Corner	1751125	9500	W	N/W	SCE
87	Sierra Avenue	State Street	NW Corner	1401140	9500	W	S	SCE
88	Sierra Avenue	Church Street	NW Corner	2277692	9500	W	S	SCE
89	Sierra Avenue	Alila Street	NW Corner	2286999	5800	C	S	SCE
90	Sierra Avenue	Dove Road	NW Corner	286998E	5800	C	S	SCE
91	Spruce Avenue	Church Street	SW Corner	995397E	5800	W	N/E	SCE
92	Spruce Avenue	Elm Road	NW Corner	2191523	5800	W	S	SCE
93	Spruce Avenue	Olive Road	NW Corner	1342457	5800	W	S/W	SCE
94	Spruce Avenue	Oak Road	NW Corner	1342459	5800	W	S/W	SCE
95	Spruce Avenue	State Street	SE Corner	731519E	5800	W	N	SCE
96	Sutter Avenue	Market Road	NE Corner	670657E	5800	W	S/W	SCE
97	Sutter Avenue	State Street	NW Corner	744738E	5800	W	S/E	SCE
98	Sutter Avenue	Spring Road	NW Corner	4091697	5800	W	S	SCE
99	Sutter Avenue	Mariposa Road	South Side	4342666	5800	C	N	SCE
100	Sutter Avenue	Primavera Court	SW Corner	4581433	5800	C	N	SCE
101	Sutter Avenue	Vineyard Road	SE Corner	4581455	5800	C	N	SCE
102	Sutter Avenue	Church Street	NE Corner	N/A	5800	C	W	SCE
103	Tulare Avenue	Elm Road	NE Corner	2111470	5800	W	W	SCE
104	Tulare Avenue	State Street	NW Corner	9720T	5800	W	E	SCE
105	Tulare Avenue	Church Street	NW Corner	533018E	5800	W	S/E	SCE
106	Tulare Avenue	Olive Road	NW Corner	1342454	5800	W	S	SCE
107	Tulare Avenue	Oak Road	SE Corner	1342455	5800	W	N	SCE
108	Washington Avenue	Alfalfa Road	NE Corner	2296397	5800	O	S	SCE
109	Washington Avenue	Citrus Road	NW Corner	2296594	5800	O	S	SCE
110	Washington Avenue	Ash Street	SE Corner	2282308	5800	C	N	SCE
111	Washington Avenue	Davis Street	SE Corner	2282305	5800	O	N	SCE
112	Washington Avenue	Spring Road	North Side	2191627	5800	W	S	SCE
113	Washington Avenue	Market Road	SW Corner	2017639	5800	W	N	SCE
114	Washington Avenue	Front Street	SE Corner	450866E	5800	W	W	SCE
115	Washington Avenue	State Street	SW Corner	2272788	5800	W	E	SCE
116	Washington Avenue	Church Street	SW Corner	275779E	5800	W	N/E	SCE
117	Washington Avenue	Elm Road	NE Corner	1401024	9500	W	S	SCE
118	Wilson Avenue	Front Street	NE Corner	1893094	5800	W	W	SCE
119	Wilson Avenue	Market Road	NW Corner	2164999	5800	W	S	SCE
120	Wilson Avenue	State Street	SW Corner	1342596	5800	W	N/E	SCE
121	Wilson Avenue	Church Street	NW Corner	671036E	5800	W	E	SCE

Source: Tulare County Public Works, March 2013

EARLIMART COMMUNITY PLAN UPDATE

PUBLIC TRANSPORTATION

Transit

The Tulare County Area Transit (TCaT) Agency operates fixed-route services that link communities with each other and with Visalia and Tulare's urban transit systems. Earlimart is connected via TCaT Route 20 to the City of Tulare and its transit center (**see Figure 13**). TCaT Route 20 also connects Delano (Kern County) in the south. Route 20 has ten northbound and southbound buses serving Earlimart on weekdays and four buses in each direction on Saturdays and Sundays. Stops are located at the Church Street and Sutter Avenue and United Health Center on Washington Avenue near State Street. In Tulare, transfers can be made to connect to Visalia, and the remainder of the TCaT public transit system. TCaT vehicles are wheelchair accessible and all full-size buses include bike racks. TCaT supports several specialized transportation programs, including shared-ride car and vanpool services, social service dial-a-ride, and specialized services for seniors and persons with disabilities

EARLMART COMMUNITY PLAN UPDATE

Figure 13 - Transit Route - South County Route 20

20 South County Route



Pass Sales Outlets

Dinuba Transit Center	189 Merced St., Dinuba
Porterville Transit Center	35 W. Oak St., Porterville
Tulare County Government Plaza	5961 S. Mooney Blvd., Visalia
Tulare Transit Center	360 N. 'K' St., Tulare
Visalia Transit Center	Oak & Santa Fe, Visalia

Southbound

WEEKDAY SERVICE												
TULARE o Transit Center	MATHENY TRACT Pratt & Abadie	TIPTON Bank of Visalia	TIPTON Memorial Building	PIXLEY Pixley Med Center	TEVISTON Avenue 76	EARLMART Church & Sutter	EARLMART United Health Center	DELANO Transit Center	DELANO Valencia Mkt.	DELANO Regional Med Center	DELANO Regional Food Center	
5:45	5:55	6:09	6:11	6:21	6:26	6:32	6:35	—	—	—	—	
6:45	6:55	7:09	7:11	7:21	7:26	7:32	7:35	7:48	7:53	7:56	—	
7:27	7:37	7:51	7:53	8:03	8:08	8:14	8:17	8:30	8:35	8:38	—	
9:04	9:14	9:28	9:30	9:40	9:45	9:51	9:54	10:07	10:12	10:15	—	
10:41	10:51	11:05	11:07	11:17	11:22	11:28	11:31	11:44	11:49	11:52	11:57	12:12
12:18	12:28	12:42	12:44	12:54	12:59	1:05	1:08	1:21	1:26	1:29	—	—
1:43	1:53	2:07	2:09	2:19	2:24	2:30	2:33	2:46	2:51	2:54	2:59	3:14
2:37	2:47	3:01	3:03	3:13	3:18	3:24	3:27	3:40	3:45	3:48	—	—
4:53	5:03	5:17	5:19	5:29	5:34	5:40	5:43	5:56	6:01	6:04	—	—
5:51	6:01	6:15	6:17	6:27	6:32	6:38	6:41	6:54	6:59	7:02	—	—
WEEKEND SERVICE												
8:30	8:40	8:54	8:56	9:06	9:11	9:17	9:20	9:33	9:38	9:41	—	—
10:53	11:03	11:17	11:19	11:29	11:34	11:40	11:43	11:56	12:01	12:04	—	—
2:06	2:16	2:30	2:32	2:42	2:47	2:53	2:56	3:09	3:14	3:17	—	—
4:29	4:39	4:53	4:55	5:05	5:10	5:16	5:19	5:32	5:37	5:40	—	—

Northbound

WEEKDAY SERVICE											
DELANO Regional Med Center	DELANO Valencia Mkt.	DELANO Transit Center	EARLMART Church & Sutter	EARLMART United Health Center	TEVISTON Avenue 76	PIXLEY Pixley Med Center	TIPTON Bank of Visalia	TIPTON Memorial Building	MATHENY TRACT Pratt & Abadie	TULARE o Transit Center	
—	—	—	—	6:35	6:41	6:46	6:56	6:58	7:12	7:22	
—	—	7:53	8:09	8:12	8:18	8:23	8:33	8:35	8:49	8:59	
—	—	8:35	8:51	8:54	9:00	9:05	9:15	9:17	9:31	9:41	
—	—	10:12	10:28	10:31	10:37	10:42	10:52	10:54	11:08	11:18	
12:27	12:32	12:35	12:48	12:51	12:57	1:02	1:12	1:14	1:28	1:38	
—	—	1:26	1:42	1:45	1:51	1:56	2:06	2:08	2:22	2:32	
3:29	3:34	3:37	3:50	3:53 o	—	—	—	—	—	—	
—	—	3:45	4:01	4:04	4:10	4:15	4:25	4:27	4:41	4:51	
—	—	6:01	6:17	6:20	6:26	6:31	6:41	6:43	6:57	7:07	
—	—	6:59	7:15	7:18	7:24	7:29	7:39	7:41	7:55	8:05	
WEEKEND SERVICE											
—	—	9:38	9:53	9:56	10:02	10:07	10:17	10:19	10:33	10:43	
—	—	12:01	12:16	12:19	12:25	12:30	12:40	12:42	12:56	1:06	
—	—	3:14	3:29	3:32	3:38	3:43	3:53	3:55	4:09	4:19	
—	—	5:37	5:52	5:55	6:01	6:06	6:16	6:18	6:32	6:42	

Light type = AM

Bold type = PM

o Free transfer to Northbound 4:04 PM bus

o Free transfer to TIME

o Transfer from TIME with additional fee

o Transferencia gratis para autobus hacia el Norte 4:04 PM bus

o Transferencias a TIME gratis

o Transferencias de TIME con cargo adicional

EARLIMART COMMUNITY PLAN UPDATE

Pedestrian Traffic

Many pedestrians walk in the streets of Earlimart where the sidewalks are not continuous down roadways. Vehicles cannot distinguish roadway with walking paths when curbs do not exist.

“Pedestrian paths are primarily developed as part of the roadway and trail systems of a community and reflect the interconnected nature of circulation and transportation systems as a whole. Constructing wide streets increases the distance a pedestrian must travel to cross a street, thereby making it inconvenient for public use and inhibiting pedestrian circulation in the community. Currently, limited continuous sidewalks are provided along major routes in the community. In addition to connecting available pedestrian resources, the communities have prioritized the completion of sidewalks along safe routes to school. Along Washington Ave. Enhanced pedestrian crossings and sidewalks are considered in areas where high pedestrian and school children demand occurs (such as to and around schools)”.¹⁶

Bicycle Traffic

The 1984 and the 1988 Regional Transportation Plan (adopted October, 1988) prepared by the Tulare County Association of Governments (TCAG) provides for a regional bicycle network intended to provide a safe alternative mode of travel. In Tulare County, bicycle travel is not yet considered a major mode of transportation and bicycles are rarely seen outside of cities and towns. The current bicycle plan provides for connections between the major urban areas and recreation facilities in the County and is expected to be satisfactory for the foreseeable future.

AMTRAK

The Corcoran AMTRAK station, located 25 miles to the northwest in Kings County, is the closest station to Earlimart providing passenger rail service. The San Joaquin Joint Powers Authority (SJJPA) is comprised of ten agencies (including TCAG) currently oversees the operation of seven daily trains serving this station. Service is provided to points north including San Francisco and Sacramento and to points south including Bakersfield and the Los Angeles basin (via connecting Amtrak Thruway bus service).

Aviation

The nearest operational general aviation airport is Mefford Field in Tulare, 20 miles north of Earlimart. A general aviation facility (Harmon Field) on the west side of Pixley, about three miles north of Earlimart, was closed in the 1990's.

Visalia Municipal Airport, 35 miles north, offers passenger service to Los Angeles. Meadows Field Airport, Bakersfield's principal commercial airport, is 36 miles south of Earlimart and offers direct flights to Denver, San Francisco, and Phoenix. Fresno Yosemite International Airport (FAT), 85 miles northeast of Earlimart, is the principal passenger and airfreight airport in the central San Joaquin Valley.

¹⁶ Tulare County Complete Streets – Earlimart, January 2007, page 11

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State Route (SR 99)

State Route 99 is a major route between cities within the Central Valley. Highway 99 is the primary route between the City of Fresno to the north and the City of Bakersfield to the south. SR 99 traverses Earlimart in north/south fashion and in effect, has divided the community geographically. Most of the residential development is located on the east side to the Freeway. Most of the commercial and industrial areas are located in the west side of the Freeway. Access to SR 99 is accessible from Armstrong Avenue and West Sierra Avenue.

Union Pacific Railroad

Union Pacific Railroad (formerly Southern Pacific Transportation Company) line is located to the west of Highway 99 and runs parallel to this Freeway. This railroad line is located between Front Street and Valente Road.

INFRASTRUCTURE

“In July 2003 Tulare County Local Agency Formation Commission (LAFCO) adopted a Municipal Service Review (MSR) exemption policy that identifies the agencies that would be subject to a review and the extent of that review. The agencies in Tulare County were divided into three (3) categories: agencies subject to a full comprehensive study; agencies subject to a questionnaire study; and agencies exempt from a MSR study. The Earlimart Community Services District (CSD) is subject to a full comprehensive study. The policy further identifies that the services subject to review shall be:

- Police protection
- Fire protection
- Water and wastewater
- Solid waste collection and disposal
- Streets and traffic circulation
- Power generation and distribution
- Health Care”¹⁷

Energy Natural Gas/Electricity

Southern California Edison (SCE) is the main provider of electrical power and natural gas in Tulare County, which maintains an extensive network of high-voltage and low-voltage electrical lines, substations, natural gas mains, and related facilities. In addition to power produced by its plants, SCE purchases power from other producers for use within its service area.

On a region wide basis, electrical demand has increased while the available power supply has remained constant. Thus, during peak demand periods, the reserve capacity of the overall system has dropped at times to under 3%. In response, SCE has planned for more stringent measures as reserve capacity diminishes. These measures include voluntary cutbacks, cutbacks for major users with whom SCE has arrangements, and rolling blackouts.

¹⁷ Earlimart Public Utility District MSR, page 4-6

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The California Public Utilities Commission (CPUC) Rule 20 includes policies and procedures for the undergrounding of overhead power lines. Rule 20A projects are funded by utility ratepayers for projects of public benefit.

Southern California Edison has three proposed Rule 20A projects in Earlimart: 1) Washington Avenue (Avenue 52) from to S. Elm Street; 2) Armstrong Avenue (Avenue 48) from to S. Lane Street; and, 3) Church Street from Washington Avenue to Armstrong Avenue. The formation of an underground utility district for this project will be considered by the Board of Supervisors.

Southern California Gas provides natural gas services to Earlimart.

Undergrounding Electricity (Rule 20A)

The California Public Utilities Commission (CPUC) Rule 20 includes policies and procedures for the undergrounding of overhead power lines. Rule 20A projects are funded by utility ratepayers for projects of public benefit.

Southern California Edison has three proposed Rule 20A projects in Earlimart: 1) Washington Avenue (Avenue 52) from State Route 99 to S. Elm Street; 2) Armstrong Avenue (Avenue 48) from State Route 99 to S. Lane Street; and, 3) Church Street from Washington Avenue to Armstrong Avenue. The formation of an underground utility district for this project will be considered by the Board of Supervisors.

Water Supply

Tulare County, including the Earlimart Community Plan Area is located within the Tulare Lake Basin. The County also has four river watersheds providing water to the county. Earlimart lies within the Kaweah Watershed and receives its local water supply primarily from the Kaweah River and operations of Terminus Reservoir/Lake Kaweah. The Tulare County General Plan states the groundwater quality is generally satisfactory for crop irrigation and urban uses.

“Domestic water and sewer service in Earlimart is provided by the Earlimart Public Utility District (PUD [or District]) which was formed in December 1954.”¹⁸ See additional analysis in the Constraints to Economic Development Section below in reference to water supply.

The most recent Inspection Report provided by California Department of Public Health (CDPH, June 2013) describes the Earlimart Public Utility District (District) as; “District is classified as a community water system. The water system has 1,568 active connections and serves a permanent population of approximately 8,300.”¹⁹ (See Appendix “F”) The District’s source of supply consists of ground water from four active wells. The wells are identified as follows: New Tulare Street Well, Clay Street Well, Front Street Well, and May Ann Well.” The wells are described in the CDPH letter of June 2013 as follows:

- *New Tulare Street Well (5410021-001)* - The New Tulare Street Well was drilled in 1981 to a depth of 860 feet. The well has a 16-inch casing and is perforated from a depth of 400 feet to 840 feet.

¹⁸ *Tulare County Housing Element – Action Program 9, page 4-1*

¹⁹ *Inspection Report provided by California Department of Public Health (June 2013), page 3*

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The well is capable of producing 1,015 gpm. The well is equipped with a flow meter for measuring production. There is an 8,000-gallon pressure tank located at this well site. The well discharges to the pressure tank and then the distribution system.²⁰

- *Front Street Well (5410021-002)* - The Front Street Well was drilled in 1989 to a depth of 740 feet. The well has a 14-inch casing and is perforated from a depth of 350 feet to 700 feet. The well is capable of producing 608 gpm. The well is equipped with a flow meter for measuring production. There is an 8,000-gallon pressure tank located at this well site. The well discharges to the pressure tank and then the distribution system.²¹
- *Mary Ann Well (5410021-003)* - The Mary Ann Well was drilled in 1973 to a depth of 810 feet. At a later date, the District drilled down another 40 feet. The casing is perforated from a depth of 470 feet to 800 feet. The well casing is 14-inches in diameter. The well is capable of producing 866 gpm. There is also an auxiliary 100 horsepower John Deere Diesel powered engine with an above ground 500-gallon steel fuel tank for use in the event of an electrical power outage. There is an 8,000-gallon capacity pressure tank at this well site. The well discharges to the pressure tank and then the distribution system.²²
- *Clay Street Well (5410021-004)* - The Clay Street Well was drilled in 1960 to a depth of 800 feet. The well is constructed with a 14-inch casing that is perforated from a depth of 400 feet to 800 feet. The well is capable of producing 779 gpm. The water well is metered. There is a 6,000-gallon capacity pressure tank at the well site. The well discharges to the pressure tank and then the distribution system.²³

Adequacy of Water Supply

Table 13 displays the average day, maximum day and peak hour demands from four active groundwater wells to meet the water system demand during the last three years for the Earlimart Public Utility District.

Table 13 - Water Demand EPUD 2003-2012			
<i>Year</i>	<i>Average Day</i>	<i>Maximum Day</i>	<i>Peak Hour</i>
2012	947	2,151	3,226
2011	946	2,218	3,327
2010	953	2,083	3,125
2009	1018	1,949	2,923
2008	963	2,218	3,327
2007	926	2,419	3,629
2006	923	2,077*	3,115
2005	NIA	NIA	NIA
2004	1,151	2,218	3,327
2003	1,142	2,570*	3,855
* Calculated using Average Day Demand and a peaking factor of 2.25 Source: EPUD Sanitary Survey			

²⁰ Op. Cit. 3 and 4.

²¹ Op. Cit. 4.

²² Op. Cit.

²³ Op. Cit.

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Storage Facilities

The Earlimart PUD does not have any water storage facilities. **Table 14** shows the District's overall water capacity provided by its four wells.

Table 14 - Source Capacity	
<i>Source</i>	<i>Capacity (gpm)</i>
Front Street Well	608
Mary Ann Well	866
New Tulare Street Well	1,015
Clay Street Well	779
Total Capacity	3,268
Source: Earlimart Public Utility District Sanitary Survey	

“The combined capacity of the four active sources is approximately 3,268 [gallons per minute] gpm. Based on the current water usage, the District's system has adequate capacity to supply the average day and maximum day demand. According to the California Waterworks Standards, the District should be able to meet maximum day demand (2,151 gpm) with the largest producing well offline. If the New Tulare Street Well is offline (1,051 gpm), the District can still meet maximum day demand (producing 2,253 gpm). The District has implemented a water conservation program to ensure adequate source capacity and system pressure during peak hour demands. If the District were to lose any of the existing wells, it would be difficult for the District to meet peak hour demands as there is no storage in the system.”²⁴

Municipal Service Review (MSR) 2006

In March 2006, the Tulare County Local Agency Formation Commission (LAFCO) Board adopted a Municipal Service Review (MSR) for Group 1 which included Earlimart. The following summarizes discusses the Earlimart PUD (**see Figure 14**) as contained in the Tulare County Local Agency Formation Commission's (LAFCO's) Municipal Service Review Final Report (March 2006):

“Domestic water and sewer service in Earlimart is provided by the Earlimart Public Utility District (PUD) which was formed in December 1954. According to the Municipal Service Review 2006 (MSR), the PUD began requiring water meters for all new development in 2000 but very little development has occurred since then indicating that the majority of the PUD's water connections are currently unmetered. Water meters will also be installed on existing properties when they change ownership.”²⁵

“Earlimart's water supply is derived from four 600 feet deep underground wells, which pump at a consistent water level of approximately 250 feet. The four wells provide high quality water requiring no chlorination or treatment. The four wells have a total maximum production efficiency of [approximately] 3,300 GPM, or 4.75 MGD. Wells are located throughout the community at the following locations: one in the southern portion of the community between Valente Road and SR 99;

²⁴ Inspection Report provided by California Department of Public Health (June 2013), page 5

²⁵ Ibid.

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one located on property at the southeast corner of Tulare Street and Elm Street; one located on property at the southwest corner of Clay Street and Church Street; and one located on property at the corner of Mary Ann Avenue and Lane Road.

The community water system supports 1,485 total service connections including 57 commercial connections, 1,424 residential connections, and 4 school connections. In 2000, Earlimart PUD started requiring water meters for all new development; however, very little development has occurred since then indicating that the majority of the Districts water system is un-metered. Water meters are also to be installed when properties change ownership.”²⁶

Table 15 - Existing Water and Wastewater Connections in Earlimart					
Drinking Water			Wastewater		
No. of Existing Connections**	Capacity*	Available	No. of Existing Connections**	Capacity*	Available
1523	1688	165	1548	2085	537
* Tulare County LAFCO Group 1 Municipal Service Reviews, page 5-10					
** Earlimart Public Utility District; August 2015					

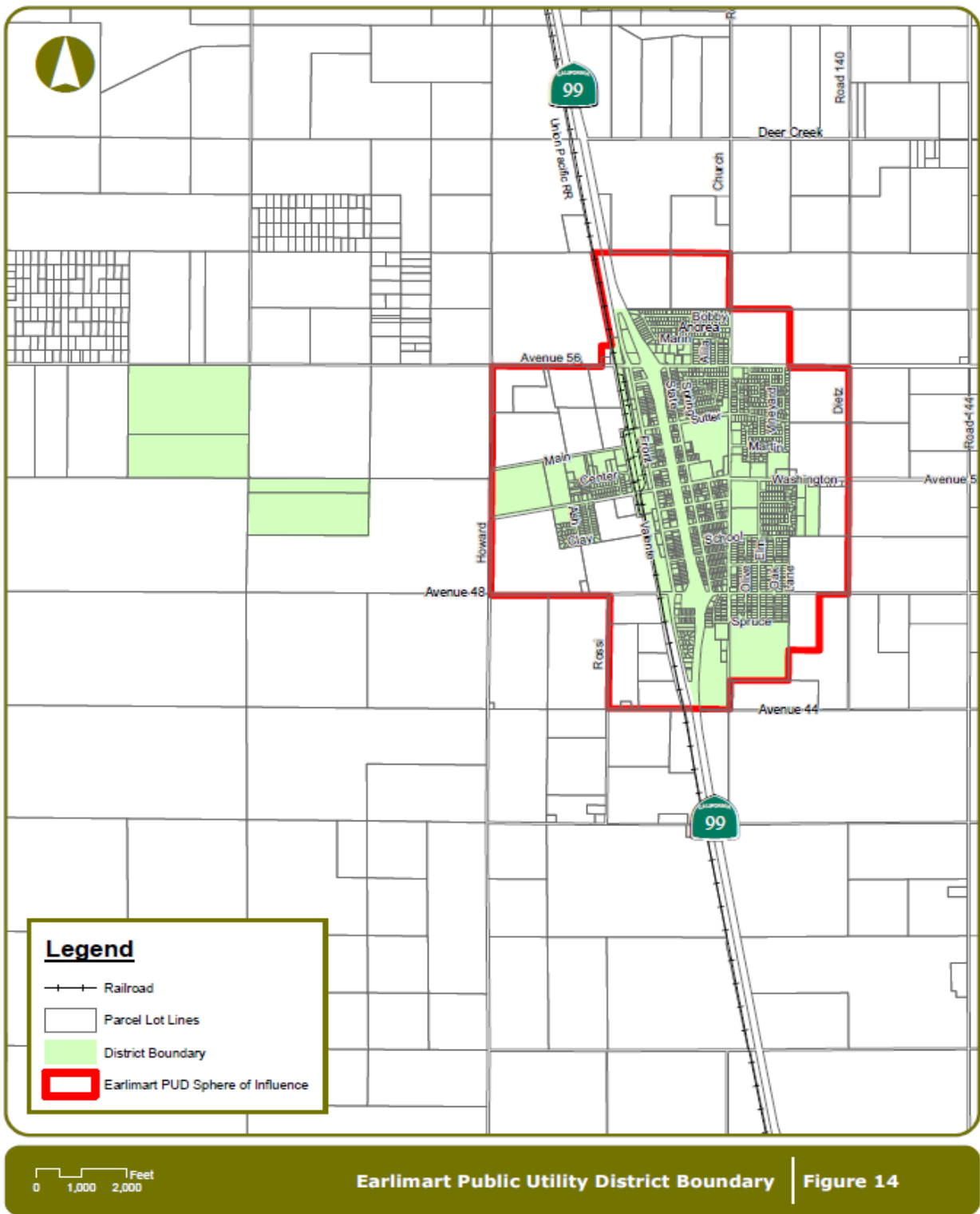
“[O]nly two of the four wells were in production year round. The maximum production occurred in the month of July and totaled 66.173 million gallons, or approximately 203 acre feet. The District indicated that engineering reports and evaluations of the water system are prepared and updated by Keller-Wegley Engineering, Inc. After several requests, these documents have not been provided making it difficult to make specific determinations with regard to the water infrastructure (capacity, condition, financing, planned improvements, etc.) of the District. It is likely that the District will need to continue to repair and/or replace older pipelines in the water system. Additionally, it is likely that the District will need to supplement its water supply to support additional development within its SOI. It is recommended that LAFCO complete a comprehensive review of any water system planning reports prior to any SOI updates to ensure that proper facilities planning has taken place for any proposed SOI expansion area.”²⁷

²⁶ Tulare County LAFCO Group 1 Municipal Service Reviews, page 5-10

²⁷ Tulare County LAFCO Group 1 Municipal Service Reviews, page 5-10 to 5-11

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Figure 14 - Earlimart PUD



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“Tulare County Improvement Standards require that the construction of water source facilities shall comply with the requirements of Bulletin No. 74, “Water Well Standards” prepared by the State of California Department of Water Resources. The Tulare County Improvement Standards also establish specific requirements for quantity and quality of water to be delivered to a system. Some of these requirements are summarized below.

- The quantity of water delivered to the distribution system within a subdivision from all source and storage facilities for a period of two hours shall be the maximum domestic demand plus a fire flow quantity of not less than 500 GPM for single family residential, 1,500 GPM for multi-family residential, commercial, and light manufacturing, and 2,500 GPM for heavy manufacturing.
- For systems up to 625 customer units (equivalent dwelling units) the domestic quantity shall not be less than $Q = 100 + 25 * \sqrt{N}$, and $Q = 100 + N$ for more than 625 customer units at sufficient pressure to provide a minimum pressure of 25 PSI to each lot served; where Q equals the rate of flow in GPM delivered from the combined source facilities to the distribution system, and N equals the total number of customer units where each customer unit is equivalent to one for a single family dwelling on a normal subdivision lot. Other types of development shall be assigned appropriate customer unit values by the Engineer as experience with the distribution system or locality indicates.
- The minimum source and domestic demand storage design requirements shall be in accordance with Plate No. WS-11 of Section IV of the Tulare County Improvement Standards.
- The quality of water supplied for human consumption shall conform to Sections 3, 4 and 5 of the latest United States Public Health Service Drinking Water Standards. Samples will be taken and tests made by the County Department of Health Services for bacteriological determination of potability.
- Chemical and physical tests for potability shall be performed by a commercial laboratory certified by the State Department of Health Services for performance of chemical and physical analysis and the costs thereof shall be borne by the sub-divider.”²⁸

“Assuming 1,500 equivalent dwelling units (EDUs), in order to meet Tulare County Improvement Standards, the Earlimart PUD water system would need to be capable of delivering a combined flow rate (from all source and storage facilities) of 3,100 GPM (1,500 GPM fire flow, and 1,600 GPM domestic demand) for a period of two hours while maintaining a minimum pressure of 25 PSI to each lot served. The District’s water system is capable of delivering a source flow of 3,300 GPM, and includes pneumatic pressure tanks for storage.”²⁹ “With a total supply source available of 3,300 GPM, it is estimated that the current system could support approximately 200 additional equivalent dwelling units. These calculations indicate that the District’s water system is operating at approximately 88% of its capacity, indicating that an additional well will be required in the near future to support additional development.”³⁰

²⁸ *Ibid.*, 5-11

²⁹ *Op. Cit.*

³⁰ *Op. Cit.*, 5-12

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Figure 15 shows the inventory of water services in Earlimart including drain inlets, sumps, storm water pipes, active water wells, and water lines. As the map was prepared in 2012, the number of drinking water connections is currently at 1,523 and availability is at 165 (**see Table 15**).

“The community water system supports 1,485 total service connections including 57 commercial connections, 1,424 residential connections, and 4 school connections. In 2000, Earlimart PUD started requiring water meters for all new development; however, very little development has occurred since then indicating that the majority of the District’s water system is un-metered. Water meters are also to be installed when properties change ownership.”³¹

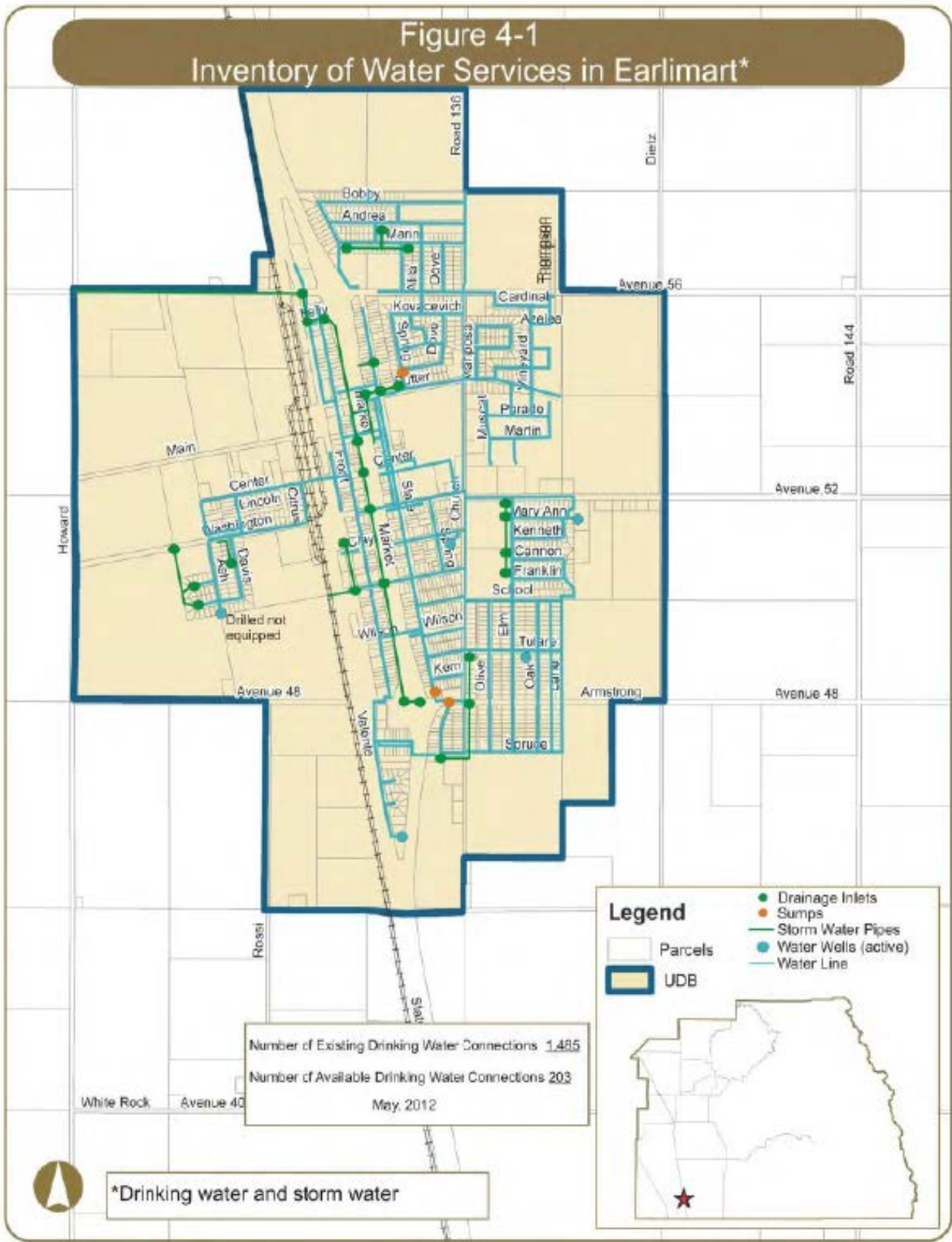
“Assuming 1,500 equivalent dwelling units (EDUs), in order to meet Tulare County Improvement Standards the Earlimart PUD water system would need to be capable of delivering a combined flow rate (from all source and storage facilities) of 3,100 GPM (1,500 GPM fire flow, and 1,600 GPM domestic demand) for a period of two hours while maintaining a minimum pressure of 25 PSI to each lot served. The District’s water system is capable of delivering a source flow of 3,300 GPM, and includes pneumatic pressure tanks for storage.”³²

³¹ *Op. Cit.*, 5-10

³² *Op. Cit.*, 5-12

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Figure 15 - Inventory of Water Services in Earlimart



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As indicated in the LAFCO MSR, the system currently meets the requirements of the Tulare County Improvement Standards. Based upon a calculation performed in accordance with General Order 103, published by the California Public Utilities Commission, it is estimated that the District's current water system could support approximately 200 additional EDUs. It is likely that the District will need to continue to repair and/or replace older pipelines in the water system. Additionally, it is likely that the District will need to supplement its water supply to support additional development within its SOI (i.e., the addition of wells to the system). It is recommended that LAFCO complete a comprehensive review of any water system planning reports prior to any SOI updates to ensure that proper facilities planning has taken place for any proposed SOI expansion area.³³

According to the Tulare County Housing Element – Action Program 9 Existing Infrastructure Analysis, the number of drinking water connections in Earlimart was 1,688. In May 2012, 1,485 existing connections were used of the 1,688-capacity resulting in 203 available connections to the system. **(see Table 15).** Currently, there are no connections available, but EPUD is planning increase capacity by seeking Proposition 1 monies for a new well.

Groundwater Quality

Specific objectives outlined in the Water Quality Control Plan are listed below:³⁴

- **Bacteria:** In ground waters designated MUN, the concentration of total coliform organisms over any 7-day period shall be less than 2.2/100 ml.
- **Chemical Constituents:** Ground waters shall not contain chemical constituents in concentrations that adversely affect beneficial uses.
- **Pesticides:** No individual pesticide or combination of pesticides shall be present in concentrations that adversely affect beneficial uses.
- **Radioactivity:** Radionuclides shall not be present in ground waters in concentrations that are deleterious to human, plant, animal, or aquatic life, or that result in the accumulation of radionuclides in the food web to an extent that presents a hazard to human, plant, animal or aquatic life.
- **Salinity:** All ground waters shall be maintained as close to natural concentrations of dissolved matter as is reasonable considering careful use and management of water resources.
- **Tastes and Odors:** Ground waters shall not contain taste- or odor producing substances in concentrations that cause nuisance or adversely affect beneficial uses.
- **Toxicity:** Ground waters shall be maintained free of toxic substances in concentrations that produce detrimental physiological responses in human, plant, animal, or aquatic life associated with designated beneficial use(s).

According to the California Water Plan, the key ground water quality issues include the following.³⁵

- **Salinity:** Salinity is the primary contaminant affecting water quality and habitat in the Tulare Lake region. Because the groundwater basin in the San Joaquin Valley portion of the region is an internally drained and closed basin, salts, much of which are introduced into the basin with imported water supplies, build up in the soil and groundwater. Salt contained in the imported water supply is the primary source of salt circulating in the Tulare Lake region. The California

³³ *Op. Cit.*, 5-13

³⁴ *Water Quality Control Plan for the Tulare Lake Basin, page III-7 to III-8*

³⁵ *California Water Plan Update 2009, Tulare Lake, page TL-22 to TL-24*

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Aqueduct, Friant-Kern Canal, and to a less extent Delta Mendota Canal supply most of the higher quality surface irrigation water in the Tulare Lake region. The quality of this supply may be impaired by the recirculation of salts from the San Joaquin River to the Delta Mendota Canal intake pump, leading to a greater net accumulation of salts in the basin. Delivery data from the two major water projects in California indicate there is a substantial amount of salt being transported from the Delta to other basins throughout the state. Annual import of salt into the Tulare Lake region is estimated to be 1,206 thousand tons of salt. In situ dissolution of salts and pumping from the underlying confined aquifer are important secondary sources.

- **Sedimentation and Erosion:** In the Central Valley, erosion is occurring from the headwaters down to the valley floor. Although naturally occurring, erosion can be accelerated by timber harvest activities, land use conversion, rural development, and grazing. Excessive soil erosion and sediment delivery can impact the beneficial uses of water by (1) silting over fish spawning habitats; (2) clogging drinking water intakes; (3) filling in pools creating shallower, wider, and warmer streams and increasing downstream flooding; (4) creating unstable stream channels; and (5) losing riparian habitat. Timber harvesting in the riparian zone can adversely affect stream temperatures by removing stream shading, a concern for spawning and rearing habitat for salmonids. Thousands of miles of streams are potentially impacted, and the lack of resources has prevented a systematic evaluation of these impacts.
- **Nitrates and Groundwater Contaminates:** Groundwater is a primary water supply, but in many places, it is impaired or threatened because of elevated levels of nitrates and salts that are derived principally from irrigated agriculture, dairies, discharges of wastewater to land, and from disposal of sewage from both community wastewater systems and septic tanks. As population has grown, many cities have struggled to fund improvements in wastewater systems. High TDS content of west-side water is due to recharge of streamflow originating from marine sediments in the Coast Range.

Naturally occurring arsenic and human-made organic chemicals—pesticides and industrial chemicals—in some instances have contaminated groundwater that is used as domestic water supplies in this region. In some cases, nitrates are from natural sources. Agricultural pesticides and herbicides have been detected throughout the Central Valley, but primarily along the east side where soil permeability is higher and depth to groundwater is shallower. The most notable agricultural contaminant is dibromochloropropane (DBCP), a now-banned soil fumigant and known carcinogen once used extensively on grapes.

Wastewater

“[T]he Earlimart Public Utility District (PUD) provides sanitary sewer collection and treatment services (**see Figure 17**) to residents within the District. According to District staff, there are currently 1,485 connections to the Districts sewer system including 1,424 residential connections, 57 commercial connections, and 4 school connections (**see Table 15**). This capacity is not currently available but would be available after improvements are made to the system. Raw sewage is collected in a series of collection pipes ranging in size from 6 to 15 inches and then transported to a WWTF that is owned and operated by the Earlimart PUD. There are three sewer lift stations in the system. The District has applied for \$750,000 grant to install a new sewer line in which the District will match the grant with \$250,000 in funding. An additional 15” trunk line will be added under Washington

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Street to Road 128 towards the plant, to the west of Earlimart. The District indicated that no additional development is to be approved until the new sewer line is in place. See additional analysis in the Constraints to Economic Development Section below in reference to waste water capacity. It is believed that the “full” capacity of the pipe is 240,000 gallons per day.

The District operates a WWTF located west of the community near the southeast quadrant of Avenue 56 and Road 120. The WWTF is operated under the provisions of Order No. 98-140 issued by the California Regional Water Quality Control Board. The WWTF consists of bar screen, an aerated grit chamber, two communitors in parallel, a clarigester, and oxidation ponds. Effluent from the oxidation ponds is stored in three retention ponds that have a total surface area of 20 acres. Order No. 98-140 prescribes that the monthly average discharge shall not exceed 0.8 MGD. The District indicated that recent improvements to the plant including the construction of additional oxidation ponds have brought the plant’s capacity up to 1.24 MGD. As prescribed by Order No. 98-140, when a California registered civil engineer has certified that the WWTF can reliably treat 1.24 MGD, the monthly average requirements specified in Order No. 98-140.

“The District has indicated that the daily flow during summer months is approximately 880,000 GPD and about 600,000 to 700,000 GPD during winter months indicating that there is no significant inflow and infiltration into the collection system during the winter months. This is an indication that the collection system is in adequate operating condition. Upon an engineer’s certification to reliably treat 1.24 MGD, the WWTF would have additional capacity to treat approximately 360,000 GPD. Based upon the available capacity at the WWTF, it is estimated that approximately 600 additional connections (EDUs) to the system could be supported. Although there is remaining capacity in the system, the District indicated that the plant was constructed in 1956 and needs upgrading including electrical upgrades. Intermediate upgrades to the plant occurred in 1973 and 1986 according to the Districts Engineer.”³⁶

Storm Drainage

“A storm drainage system is designed to drain excess rain and groundwater (from roads, sidewalks, etc.) to some point where it is discharged into a channel, ponding basin, or piped system. The system itself typically consists of pipes connecting inlets and is facilitated by curbs and gutters, manholes, and sumps. The operation of the system consists of runoff being collected in the inlets and transported by pipes to a discharge location. Manholes provide access to storm drain pipes for inspection and cleanout. A sump is a shallow, artificial pond designed to infiltrate storm water through permeable soils into the groundwater aquifer. It does not typically discharge to a detention basin.

Storm drainage systems should be designed so they have adequate capacity to accommodate runoff that enters the system for the design frequency and should also be designed considering future development. An inadequate roadway drainage system could result in the following:

- ✓ Water overflowing the curb and entering adjacent property leading to damage.
- ✓ Accelerated roadway deterioration and public safety concerns may occur due to excessive water accumulation on roadways.

³⁶ *Op. Cit.*

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- ✓ Over saturation of the roadway structural section due to immersion will lead to pavement deterioration.”³⁷

The location of existing drainage inlets and sumps in Earlimart are listed in **Table 16**.

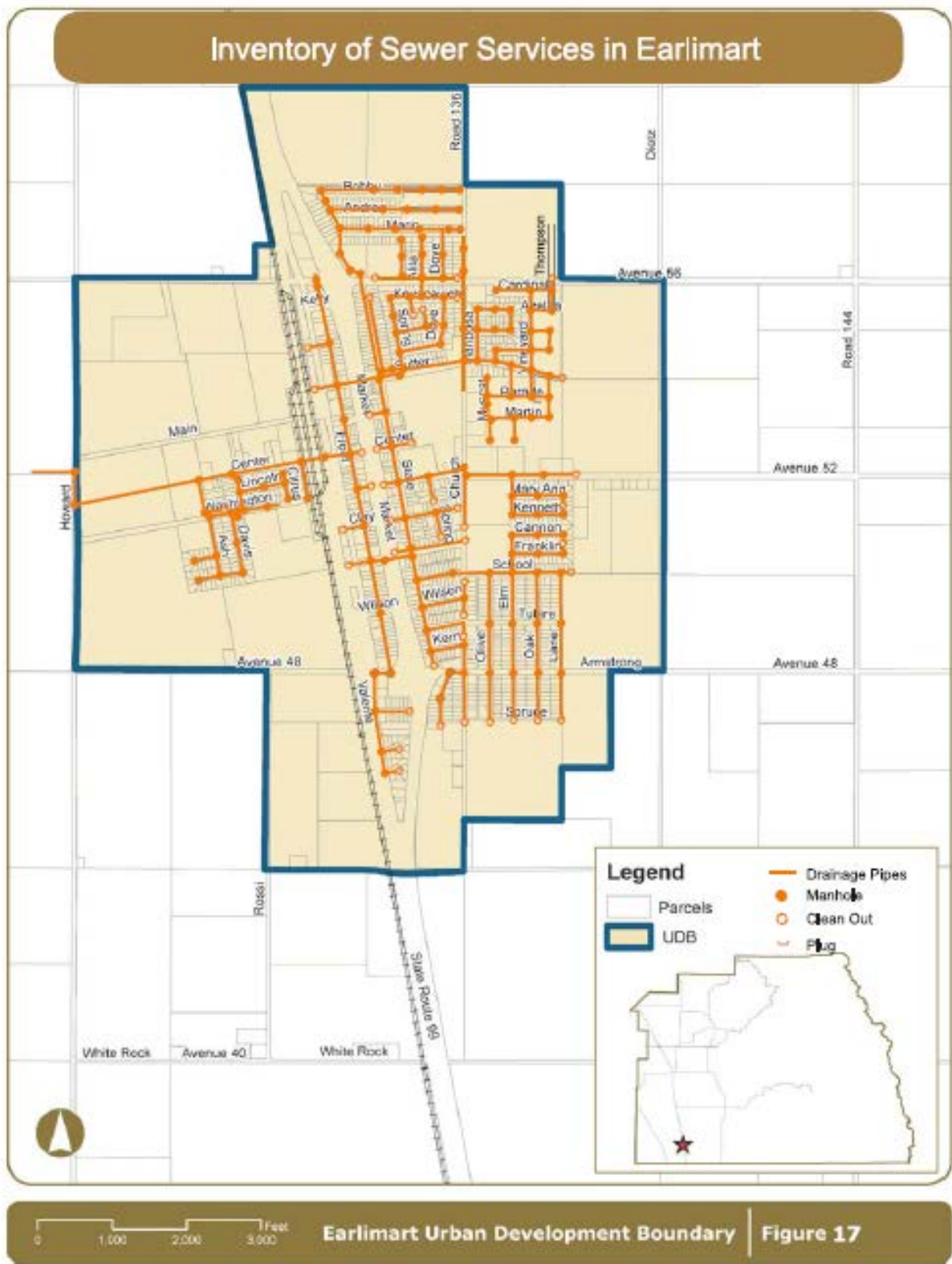
Table 16 - Location of Existing Storm Drainage Facilities

Table 17 - Location of Existing Storm Drainage Facilities			
No.	East-West Roadway	North-South Roadway	Type
1	Andrea Avenue	Diane Street	Inlet
2	Armstrong Avenue	Market Road	Inlet
3	Armstrong Avenue	east of Market Road	Inlet
4	Armstrong Avenue	Church Street	Inlet
5	Armstrong Avenue	west of State Street	Sump
6	Center Avenue	Market Road	Inlet
7	Clay Avenue	west of Ash Street	Inlet
8	Clay Avenue	Front Street	Inlet
9	Elm Road	south of Washing Street	Inlet
10	Elm Road	north of Mary Ann Avenue	Inlet
11	Elm Road	north of Cannon Avenue	Inlet
12	Elm Road	Frankline Avenue	Inlet
13	Franklin Avenue	Market Road	Inlet
14	Franklin Avenue	Front Street	Inlet
15	Kelly Avenue	Front Street	Inlet
16	Kelly Avenue	Market Road	Inlet
17	Main Avenue	Market Road	Inlet
18	Marin Avenue	Molly Road	Inlet
19	Marin Avenue	Earlimart Avenue	Inlet
20	north of Sutter Avenue	State Street	Inlet
21	Rhoden Court	west of Ash Street	Inlet
22	Sierra Avenue	Front Street	Inlet
23	south of Bent Ranch Avenue	Spring Road	Sump
24	south of Washington Avenue	Davis Street	Inlet
25	Spruce Avenue	State Street	Inlet
26	State Street	north of Armstrong Avenue	Sump
27	Sutter Avenue	Spring Road	Inlet
28	Sutter Avenue	State Street	Inlet
29	Sutter Avenue	west of State Street	Inlet
30	Tulare Avenue	Church Street	Inlet
31	Washington Avenue	Market Road	Inlet
32	Washington Avenue	west of Ash Street	Inlet

³⁷ Tulare County Housing Element – Action Program 9, pages 4-2 to 4-3

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Figure 17 - Inventory of Sewer Services in Earlimart



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PUBLIC SERVICES

The Public Services the County provides to the Earlimart Community includes policing (sheriff), fire protection, recreation, schools, and libraries. The Community Plan addresses these services below and discusses the operation of the facilities within and outside the Community.

Tulare County Sheriff

The Tulare County Sheriff's Department has a Pixley Substation covering 552 square miles and serving a rural population to include the unincorporated communities of Alpaugh, Allensworth, Tevison, Earlimart, Pixley and Tipton which includes approximately 45,000 people. The Substation runs a four-shift operation which includes 16 deputies, five sergeants and one lieutenant. There are a minimum of three deputies and one sergeant in the field at all times. The substation is open for walk-ins from 8:00 am to 5:00 pm. After hours, there is a phone provided outside the substation that calls directly into our dispatch center.

Tulare County Fire Department

Tulare County has historically contracted with the California Division of Forestry (CDF) to provide fire protection services in unincorporated areas of the County. On December 20, 2005, the Tulare County Board of Supervisors decided to terminate their fire protection contract with CDF and create the Tulare County Fire Department, staffed by county employees and to be responsive to the direction set by the Board of Supervisors after 78 years of contractual service. The contract termination date was set for July 1, 2007. At the same time, the Board decided to create a county fire department, staffed by county employees and responsive to the direction set by the board of supervisors.



The result was a county fire department staffed by state employees and whose budget was directly related to salary increases negotiated by the CDF Firefighters, Bargaining Unit 8. In February of 2006, Steve Sunderland was hired as the Fire Chief of the new Tulare County Fire Department. He immediately proceeded to hire his executive team to help him set in place the necessary elements that would allow for a smooth transition from CDF to a County Fire Department by July 1, 2007. He hired three Division Chiefs; Ted Mendoza for Operations, John Crivello for Training and Joe Garcia for Fire Prevention. Steve Sunderland retired August 13, 2011 after 5½ years of service as the Department's first Fire Chief. Joe Garcia was appointed by the Tulare County Board of Supervisors on October 23, 2011. Joe Garcia retired in January 2017, Tulare County Board of Supervisors appointed Charlie Norman as the current Fire Chief.

Fire protection and emergency medical services are provided for Earlimart by the Tulare County Fire Department. The community is served by Tulare County Fire Department Station #28, located at 808 East Washington Avenue, in Earlimart. Engine 28 and KME 50' Aerial Ladder Truck 28 are assigned to this location.

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One hundred twenty-seven fire hydrants (see **Table 18**) are found within Earlimart. These fire hydrants are located within the County right-of-way. **Figure 18** displays Existing Fire Hydrants in Earlimart.

Table 18 - Location of Existing Fire Hydrants			
No.	Location	No.	Location
1	Bobbi Avenue and Molly Road	65	Ash Street south of Washington Avenue
2	Bobbi Avenue north of Andrea Avenue	66	Clay Avenue and Davis Street
3	Bobbi Avenue and Road 136	67	Clay Avenue and Ash Street
4	Front Street north of Avenue 56	68	Clay Avenue west of Ash Street
5	Andrea Avenue and Diane Street	69	Rhoden Court west of Ash Street
6	Andrea Avenue and Alila Street	70	Washington Avenue west of Ash Street
7	Andrea Avenue and Road 136	71	Washington Avenue west of Ash Street
8	Marin Avenue and Diane Street	72	Washington Avenue west of Ash Street
9	Marin Avenue and Earlimart Avenue	73	Washington Avenue west of Fruit
10	Marin Avenue and Alila Street	74	Center Avenue and Front Street
11	Marin Avenue and Dove Road	75	Center Avenue and Front Street
12	Marin Avenue and Road 136	76	Front Street south of Center Avenue
13	Avenue 56 east of Front Street	77	Washington Avenue west of Front Street
14	Avenue 56 and Earlimart Avenue	78	Center Avenue and State Street
15	Avenue 56 and Alila Street	79	Center Avenue west of Church Road
16	Avenue 56 and Dove Road	80	Washington Avenue and Church Road
17	Avenue 56 and Muscat Road	81	Washington Avenue and Church Road
18	Avenue 56 and Thompson Road	82	Washington Avenue west of Church Road
19	Kelly Avenue and Front Street	83	Washington Avenue and Spring Road
20	Spring Road south of Kovacevich Street	84	Washington Avenue and State Street
21	Kovacevich Street and Earlimart Avenue	85	Clay Avenue and Spring Road
22	Kovacevich Street and Dove Road	86	Clay Avenue west of Church Road
23	Church Road south of Kovacevich Street	87	Clay Avenue and State Street
24	Earlimart Avenue and Quail Avenue	88	Avenue 52 and Elm Road
25	Spring Road south of Quail Avenue	89	Avenue 52 east of Church Road
26	Azalea Court and Mariposa Road	90	Avenue 52 east of Elm Road
27	Cardinal Avenue and Vineyard Road	91	Mary Ann Avenue and Elm Road
28	Azalea Avenue and Vineyard Road	92	Lane Avenue south of Mary Ann Avenue
29	Azalea Avenue and La Primavera Avenue	93	Kenneth Avenue east of Elm Road
30	Camelia Avenue and La Primavera Avenue	94	Cannon Avenue east of Elm Road
31	Camelia Avenue and Mariposa Road	95	Lane Avenue south of Cannon Avenue
32	Azalea Avenue and Thompson Road	96	Franklin Avenue east of Elm Road
33	Bent Ranch Avenue and Thompson Avenue	97	Spring Road north of Franklin Avenue
34	Bent Ranch Avenue and Thompson Avenue	98	Franklin Avenue and State Street
35	Vineyard Road south of Bent Ranch Avenue	99	Franklin Avenue east of Front Street
36	Sutter Avenue east of Vineyard Road	100	Wilson Avenue east of Front Street
37	Sutter Avenue and Vineyard Road	101	State Street north of School Avenue
38	Sutter Avenue and Muscat Road	102	School Avenue and Church Street
39	Sutter Avenue and Mariposa Road	103	School Avenue and Elm Road

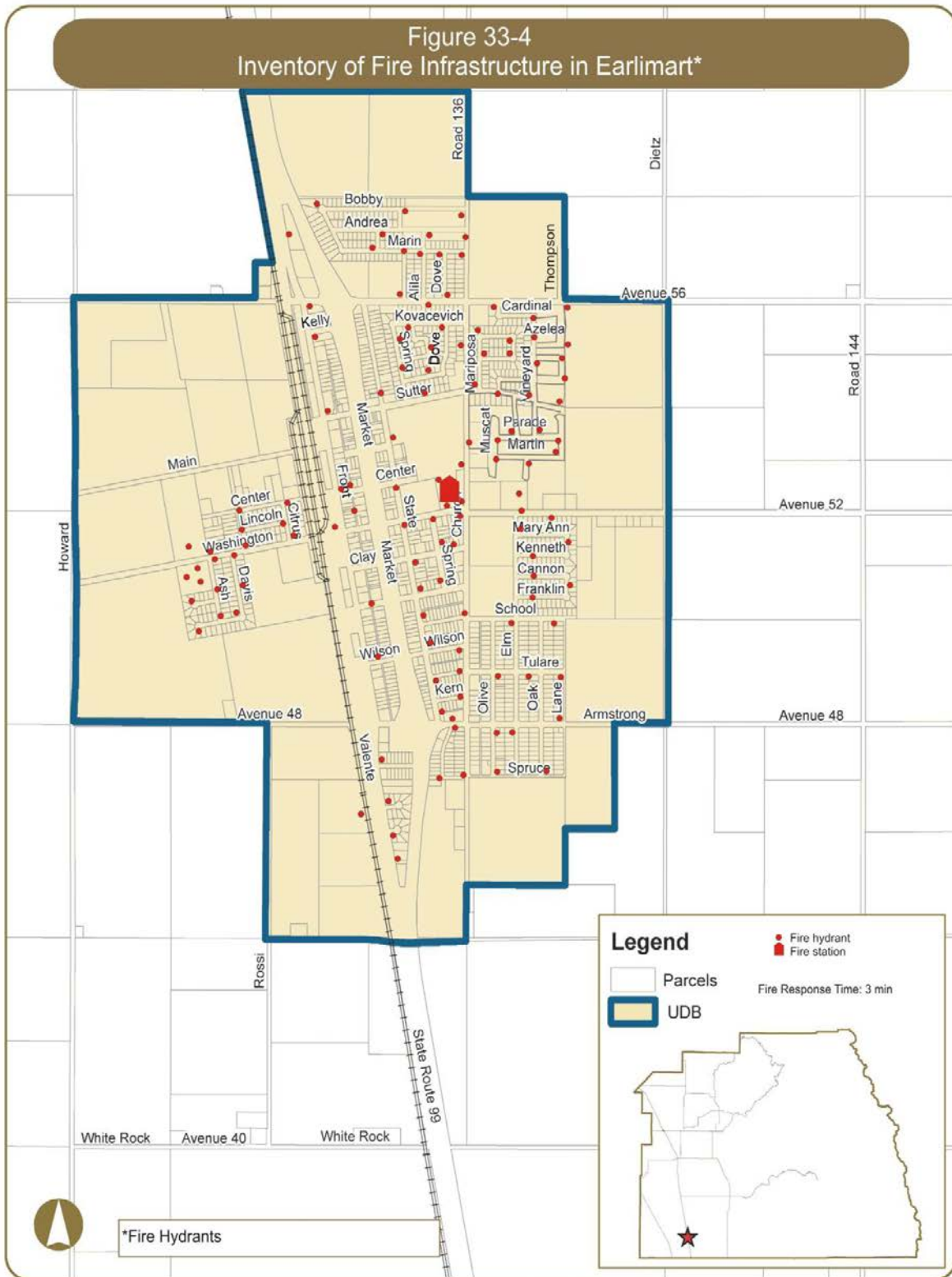
EARLIMART COMMUNITY PLAN

Table 18 - Location of Existing Fire Hydrants

No.	Location	No.	Location
40	Spring Road north of Bent Ranch Avenue	104	School Avenue east of Oak Road
41	Bent Ranch Avenue east of Spring Road	105	Wilson Avenue and State Street
42	Sutter Avenue east of Spring Road	106	Wilson Avenue and Church Street
43	State Street north of Sutter Avenue	107	Tulare Avenue and State Street
44	Sutter Avenue and Front Street	108	Tulare Avenue and Church Street
45	Main Avenue and State Street	109	Kern Avenue and Church Street
46	Parade Avenue and La Primavera Court	110	Tulare Avenue and Olive Road
47	Parade Avenue and Vineyard Road	111	Tulare Avenue east of Elm Road
48	Parade Avenue and Thompson Road	112	Tulare Avenue and Lane Avenue
49	Martin Avenue and Thompson Road	113	Armstrong Avenue and Lane Avenue
50	Martin Avenue and La Primavera Court	114	Armstrong Avenue and Elm Road
51	Martin Avenue and Muscat Court	115	Armstrong Avenue and Olive Road
52	Parade Avenue and Muscat Court	116	Armstrong Avenue and State Street
53	Center Avenue and Church Road	117	Armstrong Avenue and State Street
54	Center Avenue and Church Road	118	State Street north of Armstrong Avenue
55	Center Avenue and Alfalfa Road	119	Spruce Avenue and Oak Road
56	Lincoln Avenue and Alfalfa Road	120	Spruce Avenue and Olive Road
57	Center Avenue and Curtis Road	121	Spruce Avenue and Church Street
58	Lincoln Avenue and Curtis Road	122	Spruce Avenue and State Street
59	Washington Avenue and Curtis Road	123	Cedar Avenue and Front Street
60	Washington Avenue and Alfalfa Road	124	Olympia Street and Front Street
61	Washington Avenue and Davis Street	125	Chaparral Street and Front Street
62	Davis Street south of Washington Avenue	126	Front Street south of Chaparral Street
63	Washington Avenue and Ash Street	127	Valente Road north of Cable Street
64	Washington Avenue and Ash Street		

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Figure 18 - Inventory of Fire Infrastructure



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Schools

“A total of 48 school districts provide education throughout Tulare County. Of the 48 school districts, seven are unified districts providing educational services for kindergarten through 12th grade. The remaining 41 districts consist of 36 elementary school districts and four high school districts. Many districts only have one school.”³⁸

Earlimart is served by four schools. Grades TK-5 attend Earlimart Elementary School, grades TK-5 attend Alila School, grades 6-8 attend Earlimart Middle School and grades 4-8 attend Earlimart Community Day School.³⁹ High school students attend Delano High School located ten miles south.”⁴⁰

According to records from the California Department of Education, enrollment in the Earlimart School District has increased from 1,155 students in the 1992 - 1993 fiscal year to 1,953 students in 2013 - 2014 fiscal years.⁴¹ Of these students approximately 97 percent are Hispanic, 0.2 percent Asian, 1.3 percent Filipino, 0.6 percent are White, and 0.3 percent are African American.⁴² California Department of Education records indicate that in the 2015 – 2016 school year, of 1,961 registered children in the Earlimart School District, 1,691 participated in the Free or Reduced Price Meal (FRPM), indicating that approximately 86 percent of the district’s students are receiving free or reduced price meals.

Community college educational services are provided by College of Sequoias Community College District in Visalia and as secondary choices, Porterville College in Porterville, and Kings River Junior College in Reedley. California State University, Fresno is a four-year university about 60 miles north of Tipton in Fresno, and California State University, Bakersfield is a four-year university about 60 miles south of Tipton in Bakersfield.

New High School Project

The proposed New High School project would be constructed near the western edge of the Earlimart Urban Development Boundary (UDB). When completed, the high school will provide secondary-level educational needs with an emphasis on agricultural training and student preparation in agricultural sector-related careers. The high school will be designed to accommodate approximately 350 students. As described by Delano Joint Union High School District (School District), this project will not result in recreational facilities included as part of a “typical” high school facility. The Preliminary site plan for this facility includes a design for both a baseball diamond and soccer field. Recreational facilities located at this site will support student activities and may be available for public use outside of school hours.

³⁸ General Plan Background Report, pages 7-75 and 7-76

³⁹ Earlimart School District. (2017). <http://www.earlimart.org/>

⁴⁰ Tulare County RMA, 2017. Earlimart Community Plan Update (), page 35.

⁴¹ Ed Data, 2017. <http://www.ed->

[data.k12.ca.us/App_Resx/EdDataClassic/jfTwoPanel.aspx?#bottom=/_layouts/EdDataClassic/profile.asp?Tab=0&level=06&reportnumber=16&county=54&district=71902&school=00000000](http://www.ed-data.k12.ca.us/App_Resx/EdDataClassic/jfTwoPanel.aspx?#bottom=/_layouts/EdDataClassic/profile.asp?Tab=0&level=06&reportnumber=16&county=54&district=71902&school=00000000)

⁴² Ibid. <http://www.ed->

[data.k12.ca.us/App_Resx/EdDataClassic/jfTwoPanel.aspx?#bottom=/_layouts/EdDataClassic/profile.asp?Tab=1&level=06&reportnumber=16&county=54&district=71902&school=00000000#studentsbyraceethnicity](http://www.ed-data.k12.ca.us/App_Resx/EdDataClassic/jfTwoPanel.aspx?#bottom=/_layouts/EdDataClassic/profile.asp?Tab=1&level=06&reportnumber=16&county=54&district=71902&school=00000000#studentsbyraceethnicity)

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As noted earlier, this Project would result in the construction of a high school with an emphasis on agricultural training and student preparation in agricultural sector-related careers to provide secondary-level educational needs of approximately 350 students. On-site recreational opportunities are anticipated to meet the recreational needs as determined by the School District. As the New High School project will accommodate the needs of its own student population, faculty and staff (and potentially may become a site of public recreation outside of regular school hours).

Parks

Earlimart Neighborhood Park is approximately 3.8 acres in size and is located on the southeast corner of the Earlimart Elementary School property bound by School Avenue and Elm Road. The site is owned by the Earlimart School District with whom the County has entered into a long-term joint powers agreement for development, operation and maintenance of the Park.

With collaboration from the Earlimart School District and the County, funding was received to develop a Park in Earlimart through the proposition 84 Statewide Park Development and Community Revitalization Program of 2008. The project scope is limited by the awarded grant amount of \$2,153,900.

This project will include construction of a children's play area, open turf play area, picnic facilities, pathways system, multi-use event area, gateway features, community art, security fencing, drought resistant landscaping and a biofiltration swale. The site is also intended to provide limited storm water detention during a 25-year flood event for the surrounding neighborhood to the South and the school property.

Colonel Allensworth State Park

Colonel Allensworth State Park is located to approximately 7 miles west of Earlimart. “In August 1908 Colonel Allen Allensworth and four other settlers established a town founded, financed and governed by African Americans. Their dream of developing an abundant and thriving community stemmed directly from a strong belief in programs that allowed blacks to help themselves create better lives. By 1910, Allensworth’s success was the focus of many national newspaper articles praising the town and its inhabitants.

An unavoidable set of circumstances made it impossible for the residents of this tiny town located 30 miles north of Bakersfield to achieve their founders’ dreams over the long term. But the town did remain home to a handful of families and individuals throughout the 20th century, and true to the courage and resolve of its founders, the town has survived and persevered, earning the well-deserved title “The town that refused to die.”

“In 1974 California State Parks purchased land within the historical townsite of Allensworth, and it became Colonel Allensworth State Historic Park. Today a collection of restored and reconstructed early 20th-century buildings—including the Colonel’s house, historic schoolhouse, Baptist church, and library—once again dots this flat farm country, giving new life to the dreams of these visionary pioneers.

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With continuing restoration and special events, the town is coming back to life as a state historic park. The park's visitor center features a film about the site. A yearly rededication ceremony reaffirms the vision of the pioneers.”⁴³

Libraries

“The Earlimart Library was established in October, 1914 in the Alila School. It offered the usual 50 books of a County deposit station. In October 1915, the library was moved to Mr. Ough’s Store. Branch was discontinued in December, 1915, and then re-established in February 1916, in the school house. The library was then moved from store to store with store owners as custodians. Then, in 1926, it was moved to the Adams family store where it stayed until 1940. During WWII, the library was in the home of Reverend and Mrs. Swift. The present Branch was built in 1959, and in 1963, Earlimart was the second most active County Branch, lending 67,176 books.”⁴⁴ (see Table 19 – Library Location and Hours).



Table 19 - Library Location & Hours

Branch	Address	Service Hours
Earlimart	780 East Washington St. P.O. Box 12153 Earlimart, CA 93219-2153	Tuesday: 9:00 am – 1:00 pm, 2:00 pm – 5:00 pm Wednesday: 9:00 am – 1:00 pm, 2:00 pm – 5:00 pm Thursday: 9:00 am – 1:00 pm, 2:00 pm – 5:00 pm Friday: 9:00 am – 1:00 pm, 2:00 pm – 5:00 pm

Library hours current as of May 2017, <http://www.tularecountylibrary.org/earlimartbranch.html>

⁴³ http://www.parks.ca.gov/?page_id=583

⁴⁴ <http://www.tularecountylibrary.org/earlimartbranch.html>

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THE NEED FOR A COMMUNITY PLAN

Purpose

The Earlimart Community Plan was adopted in 1988. Conditions in Earlimart have changed and policies and implementation strategies should be updated to address existing conditions. This community plan update will be used to foster economic development by identifying opportunities for development. This community plan is also a part of the implementation of the San Joaquin Valley Regional Blueprint, Tulare County Regional Blueprint, Sustainable Highway 99 Corridor Plan and the Tulare County 2030 General Plan.

Relationship to the General Plan

Tulare County's General Plan provides a comprehensive statement of the objectives, themes and policies which the community is seeking to achieve in the areas of land use, growth management, community design, transportation, open space, parks and public facilities, environmental conservation, health and safety, noise, and housing. This Community Plan, as an instrument which promulgates and is an extension of the General Plan, incorporates, by definition, the stated general objectives, themes and policies and, where more specific objectives and policies are stated, makes reference to such objectives and policies and provides further elaboration on the ways in which the Community Plan is responsive to this guidance. Relevant General Plan goals, policies, and programs that provide direction and input to this Community Plan are provided in this document. In addition, this plan has specific policies for the Community of Earlimart.

San Joaquin Valley Regional Blueprint

"The San Joaquin Valley Blueprint is the result of an unprecedented effort of the eight Valley Regional Planning Agencies (RPA), that include the Fresno Council of Governments, the Kern Council of Governments, the Kings County Association of Governments, the Madera County Transportation Commission, the Merced County Association of Governments, the San Joaquin Council of Governments, the Stanislaus Council of Governments, and the Tulare County Association of Governments, to develop a long-term regional growth strategy for the future of the San Joaquin Valley. Following three years of visioning and outreach by the eight Valley RPAs, the Regional Policy Council (RPC), the decision-making body for the Valleywide process, adopted the Valley Blueprint in April 2009.

The Blueprint is a long-range vision for a more efficient, sustainable, and livable future for the Valley. The Valleywide Blueprint is made up three elements: a 2050 growth scenario diagram that identifies areas of existing development, new development, and future regional transit and highway improvements; a Valleywide average target density of 6.8 units per acre for new residential growth to the year 2050; and a set of 12 Smart Growth Principles. Importantly, the Blueprint recognizes and incorporates by reference the visioning and outreach efforts undertaken by the eight Valley Regional Planning Agencies."⁴⁵

⁴⁵ San Joaquin Valley Blueprint Roadmap Guidance Framework, page i

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Tulare County Regional Blueprint

“TCAG and its member agencies felt that it was important to prepare a Tulare County Regional Blueprint that clarified Tulare County’s role in the Blueprint process. The Tulare County Regional Blueprint is a stand-alone policy document that is consistent with the San Joaquin Valley Regional Blueprint. This document represents Tulare County’s local vision and goals as a participant in the San Joaquin Valley Regional Blueprint process.”⁴⁶ Key elements of the preferred growth scenario outlined in the Tulare County Regional Blueprint include 25% increase in overall density and focused growth in urban areas.

TCAG, Tulare County Regional Bicycle Transportation Plan, Regional Transportation Plan (RTP) and Sustainable Communities Strategy

TCAG in 2014 updated a Regional Bicycle Plan that does not include any bicycle facilities through the Community of Earlimart. TCAG funded the grant for this Complete Streets Policy and in the RTP Action Element describes bicycle circulation patterns and pedestrian policies focusing on the Americans with Disabilities Planning Strategies and Transportation Demand Management to increase pedestrian activity. In addition, rail and goods movement is part of the Sustainable Communities Strategy in lieu of utilizing diesel powered freight trucks.

Tulare County Climate Action Plan (TCAP)

Under State Assembly Bill 32 and Senate Bill 375, the Tulare County CAP calls for a reduction on a project (over 50 vehicles) by project basis of 6% through a mixture of measures that are spelled out in Appendix J of the CAP. Utilization of alternative means of transportation will reduce GHG emissions and will help projects and the region meet their targets. TCAP was last updated in 2016.

Senate Bill 244, Housing Element Implementation

Senate Bill (SB) 244, passed by the California Legislature and signed into law in 2011, requires California municipalities analyze the inequality and infrastructure deficits within disadvantaged unincorporated communities (DUCs), which lack basic community infrastructure like sidewalks, safe drinking water, and adequate waste processing. As a part of this process and the implementation of the Housing Element the County continues to identify housing related infrastructure needs, such as; water, sewer, natural gas or streetlights, using community needs assessments, housing condition surveys, public comments at community meetings, redevelopment implementation plans and amendments, community plans and other relevant information from the Health & Human Services Agency (HHSA) Environmental Health Services, Regional Water Quality Control Board, public utility districts, community services districts and other agencies. The County of Tulare prepared a 2016 Disadvantage Unincorporated Communities Assessment (infrastructure needs assessment) of the County in conjunction with SB 244 and Action Program 9 as part of the 2015 Tulare County Housing Element Update.

Sustainable Highway 99 Corridor Plan

The Sustainable Highway 99 Corridor Plan is a plan that serves as a foundation planning document to guide the preparation of future community plans for unincorporated and economically disadvantaged communities along the corridor, and provide sustainable planning practices, standards

⁴⁶ Tulare County Regional Blueprint, page 3

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and strategies for the abundant agricultural and natural lands in between the communities. The primary project objectives of this plan are as follows: Promote Public Health, Promote Equity, Increase Affordable Housing, Revitalize Urban and Community Centers, Protect Natural Resources and Agricultural Lands, Reduce Automobile Usage and Fuel Consumption, Improve Infrastructure Systems, Promote Energy Efficiency and Conservation, Strengthen the Economy.

Tulare County 2030 General Plan Implementation

This Community Plan is intended to implement the Tulare County 2030 General Plan. The General Plan Policies relevant to this Community Plan are outlined in General Plan Policies Section below. In addition to the General Plan Policies, this Community Plan outlines policies specific to Earlimart. Following are the ways in which this Community Plan implements the General Plan:

- Update Zoning Map to match the Community Plan Land Use Map.
- Addition of Design Standards to replace use permit standards.
- Update Zoning text to outline allowed uses in this Community Plan.
- Introduction of a Mixed-Use Overlay Zoning District.
- Provides a Market Analysis of the Earlimart Area.
- Provides an updated analysis of Earlimart's population and housing characteristics.
- Defines an economic development strategy.

Community Plan Update Project Description and Objectives

On January 28, 2014 the Tulare County Board of Supervisors (BOS) approved, the Planning Branch proposal to update the Earlimart Community Plan. The Earlimart Community Plan Update General Plan Amendment (GPA 14-005) components are described later in this section, and will become consistent with the Tulare County General Plan 2030 Update⁴⁷, and will include the following primary goals and objectives.

1. Land Use and Environmental Planning - Promote development within planning areas next to the Regional Highway 99 Corridor in order to implement the following General Plan goals:
 - a) Update the affected Urban Development Boundaries to include newly expanded Enterprise Zone areas;
 - b) Ensure that the text and mapping of the Community Plan Designations and Zoning Reclassifications address various development matters such as encouraging Agricultural Adaptive Reuse activities, recognizing Non-Conforming Use activities, and facilitating Ministerial Permit approvals;
 - c) Encourage infill development within Urban Development Boundaries, thereby discouraging leapfrog development within Tulare County;
 - d) Reduce development pressure on agriculturally-designated lands within the Valley Floor, thereby encouraging agricultural production to flourish;
 - e) Reduce vehicle miles travelled throughout the County, thereby positively affecting air quality and greenhouse gas reduction; and

⁴⁷ Tulare County General Plan 2030 Update may also be referred to as General Plan or 2030 General Plan.

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- f) Help to improve the circulation, transit and railroad transportation system within this community, including, but not limited to, laying the groundwork for the construction of key projects such as Safe Routes to Schools, Complete Streets, and Bike Lanes/Pedestrian Paths.
- 2. Improvements for a “disadvantaged community” - It is expected that the community planning areas will be improved for the following reasons:
 - a) With faster project processing resulting from an updated community plan, increased employment opportunities are more likely to be provided by the private sector as proposed project developments can be approved as expeditiously as possible;
 - b) Increased housing grant awards are more likely to occur based on updated community plans that are consistent with the policies of the General Plan 2030 Update (August 2012) and the recently adopted Housing Element 2015 Update (November 2015); and
 - c) With updated community plans, enhanced infrastructure grant awards are more likely, thereby providing access to funding to install or upgrade road, water, wastewater, and storm water facilities.
- 3. Strengthening Relationship with Tulare County Association of Governments (TCAG) - An important benefit of this expedited community plan process will be the opportunity for Tulare County RMA to strengthen the County’s relationship with the TCAG in that this and other community plans will help to facilitate the funding and implementation of several key transportation programs such as Safe Routes to Schools, Complete Streets, and Bike/Pedestrian Projects.

By pursuing these transportation programs through a heightened collaborative process, the likelihood of getting actual projects in the ground will be realized faster than historically achieved. In doing so, these communities and others can become safer and healthier by providing a more efficient transportation network.

The Earlimart Community Plan Update consists of five (5) components: property land use designation and rezoning; update to the Earlimart Mixed Use Zone; Complete Streets (adopted February 28, 2017), and road maintenance programs; construction of a new high school within the existing Urban Development Boundary; and the northern expansion of the Urban Development Boundary to include the existing Eden Produce Company property located outside of the existing Urban Development Boundary.

COMMUNITY OUTREACH PROCESS

The County of Tulare received a grant from the Strategic Growth Council (SGC) to assist in the preparation of this plan. In efforts to invite and make local residents aware of the Strategic Growth Council (SGC) projects, Self-Help Enterprises (SHE) staff performed various outreach methods to promote community participation and make local residents aware of the SGC project.

- 1. SHE staff developed an informational flyer.
- 2. SHE staff distributed and posted flyers around town at local business, clinics and post office. SHE talked with business owners about the project and invited them to attend the meeting.

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3. SHE staff distributed flyers at the local food pantry where over 250 residents receive food. At this event, SHE explained the project and the importance in participating.
4. SHE partnered with the Earlimart Unified School District to distribute flyers to their students. Meeting flyers were sent home from school with 2,600 students of the ESD.
5. Meeting invitations were sent to the Earlimart Public Utility District and Earlimart School District Board members.
6. During the door to door community outreach process, SHE staff explained the goals of the project and talked with residents about the community benefits of participating in the SGC community meeting.

Community Meeting

Self-Help Enterprises held a SGC community meeting in Earlimart on Wednesday, November 4, 2015 at 6:00 pm. The meeting was scheduled in the evening to make it easier for working residents to attend. The meeting was held at the Veterans Memorial Building, 712 E. Washington Ave in Earlimart. This is an ideal meeting location because it is centrally located and most residents know where the Memorial Building is. The purpose of the meeting was to discuss Earlimart improvement needs, gather community data, and report the findings of the meeting in a final report. Over thirty people attended the meeting. Twenty- eight community surveys were collected. Representatives from Proteus, United Health Center, Earlimart Town Council and a local group, Rural Committee for Community Advancements were present at this community meeting.



Community Survey

A survey was developed as a tool to gather a variety of community information about multiple topics. The survey asks about the following community related topics: schools, libraries, housing, zoning, parks, shopping opportunities, access to gas stations, access to medical facilities, natural gas, internet access, transportation options, walkability, roads, street safety, flooding, fire, safety, infrastructure, water quality & quantity, wastewater, storm water drainage, multimodal opportunities, and the priority of various improvement needs. Residents were encouraged to add information and comments to the survey.

After careful discussion at the Earlimart SGC community input meeting, residents concluded that the following is a list of Earlimart priority improvement needs

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Priority Improvements

1. Community Safety/Police Patrol
2. Sidewalks
3. Drainage
4. Street Lights
5. Affordable Housing
6. Supermarket
7. Speedbumps
8. Road Conditions/Street Repairs
9. Sewage Capacity

Sheriff Presence

The Tulare County Sheriff Department (TCSO) is responsible for patrolling the community of Earlimart. Residents report that the Sheriff's Department response time is unacceptable and that there is limited Sheriff patrols within the community. Residents are worried about the rate of crimes and response times. Residents feel that the existing level of officers assigned to the area is limited and has an influence on the local crime rate and makes criminals feel unstoppable in Earlimart. Over the last few years, Earlimart has seen a rise in gang violence and residents believe that an increased police presence would help with that problem.

Safety

Some residents are worried for their safety and feel that the Sheriff's department should be doing more to alleviate the recent rise in gang related crimes. The community is very concerned with the rise in shootings and drug related violence over the last couple of years. The recent crime activity is driving some residents away and has caused some people to move to neighboring communities such as Delano and Tulare.

Storm Water Drainage

Earlimart residents report that the community does not have adequate storm water drainage. Large puddles form and the water does not drain because there is nowhere for the water to go. Residents report that some streets flood during the rainy season making it difficult to drive and walk on certain roads. After it rains the stagnant water attracts insects and mosquitos causing additional problems. The community needs curbs and gutters to help with this problem.

Road Improvements

Road conditions are a big concern to Earlimart residents. It is reported that road conditions are poor and some roads need work, streets have potholes, cracks and bumps. Drivers feel unsafe and are worried about damage to their vehicles while driving on some Earlimart roads. According to residents, Earlimart roads are not maintained adequately.

The following roads are a priority to the residents:

- Washington Ave (Avenue 52).
- State Street.
- Front Road.
- Armstrong Ave (Avenue 48).

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Sidewalks

Many streets in Earlimart need sidewalks, some residents that were surveyed reported that the street they live on does not have a sidewalk. People report that this is a problem for kids while walking to school and for parents who push a stroller through town. The lack of sidewalks becomes an even bigger problem during the rainy season. When it rains, residents and kids are forced to walk to school in the mud and through big puddles of water. Sometimes the traffic travels at a high rate of speed and the absence of sidewalks to provide a clear separation between walking paths and the road creates a safety hazard for pedestrians.

Street Lights

Residents report dark streets at night due to little or no street lighting and that most people stay indoors at night because they do not feel safe walking on a dark street. At night, visibility is limited and residents indicate that this is a major safety concern for them, especially since the Sheriff's department has limited patrols in Earlimart. Residents are especially concerned about how this affects the safety of local children. Residents state that some streets do not have light poles while other streets have light poles but the light bulb is out. The community feels this issue should be easy to solve by simply replacing the bulb. However, residents have asked the County to address this issue and have been told that Tulare County does not maintain all of the lights as some belong to Southern California Edison. Residents report being told that there are costs to them for the installation of a new light pole on their street. Residents have been asking the County of Tulare for help to solve this problem for many years and that this issue needs to be treated as a higher priority.

Neighborhood Park

Survey results show that most residents use the local schools green space or the street to be physically active. Residents are concerned about the health and safety of their children, they worry that the safety of their children are at risk when they play on the street. The community is also concerned for the health of their children from a lack of physical activity that comes from not having a safe place to be physically active. The County of Tulare received a grant to build a community park in Earlimart, the park project is a partnership with the Earlimart School District. The community is anxious to see this park happen and is also concerned that there has been slow progress made, they feel that the County needs to treat this project as a higher priority. The completion of the Earlimart Park is a priority to the community and they want to see this project completed in a timely manner as scheduled and as promised. The park is scheduled to be completed in late 2017. Currently the nearest community park is located in the neighboring community of Pixley, six (6) miles away. It is not practical for residents to travel that far to use the park.

Local Grocery Store

Residents expressed the need for an affordable grocery store in town. Currently, Earlimart has two markets that sell some produce, meat and dairy. However, the residents are not satisfied with the quality and freshness of the food and the prices charged are much higher in Earlimart than in the neighboring community of Delano. Over the last ten years Earlimart has grown to a population of over 10,000 and because Earlimart is located directly off of SR 99, residents feels a large grocery store would be sustainable. Earlimart residents regularly travel to Delano (7 miles) or Tulare (25 miles) to buy their groceries and travel would be greatly reduced if there was a large grocery store in Earlimart.

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Affordable Housing

Earlimart residents want new affordable housing to be built in town and many people expressed that they would buy a new home in Earlimart if it were available to them. For several years, the Earlimart Public Utility District has not allowed new sewer connections to the sewer system due to capacity issues. This has limited new home development in Earlimart. Residents would like to see growth in town and want to work with the Earlimart PUD to allow for new sewer connections for new homes. Residents feel that this is a priority and necessary for the advancement of the community, without new home development the community will lose residents to neighboring communities such as Delano and Tulare.

Complete Streets Outreach Efforts

The purpose public workshops or community meetings is to engage in discussions with local residents and business owners regarding specific topics, e.g., transportation related improvements. Public outreach efforts were held in several formats including formally and informally. Formal community meetings were held at local schools, community service districts/public utility districts (CSDs/PUDs), town council forums and other well-known locations. Informal meetings were conducted with individual business or property owners associated to specific access concerns or other issues. Publicity for meeting times and locations generally consisted of newspaper releases, local newsletter informational items, citizens distributing fliers, handing out bi-lingual fliers to school children to be given to the student's guardian, posting fliers at local community businesses, local school board meeting agendas, area congressional office and non-profit agency assistance, local senior centers and health clinics (if applicable), email and other forms of communication. Formal public meetings were held in the various communities shown below. A summary of additional information – Tulare County Resource Management Agency Complete Streets and Community Plan Outreach (2015) – is located in Appendix G of the Complete Streets Program in Appendix 6. Earlimart Public Meetings 15 meetings to date, 4 for Complete Streets:

- Complete Streets Meeting February 5, 2015
- Complete Streets Meeting March 5, 2015
- Complete Streets Meeting April 9, 2015
- Complete Streets Meeting April 15, 2016

EARLIMART COMMUNITY PLAN UPDATE

SUSTAINABILITY

Tulare County Climate Action Plan

Tulare County adopted a Climate Action Plan (CAP) on August 28, 2012. The CAP is an implementation measure of the 2030 General Plan Update. The policies, regulations, and programs considered in the CAP include those by federal, state, and local governments.

“The Tulare County Climate Action Plan (CAP) serves as a guiding document for County of Tulare (“County”) actions to reduce greenhouse gas emissions and adapt to the potential effects of climate change. The CAP is an implementation measure of the 2030 General Plan Update. The General Plan provides the supporting framework for development in the County to produce fewer greenhouse gas emissions during Plan buildout. The CAP builds on the General Plan’s framework with more specific actions that will be applied to achieve emission reduction targets consistent with California legislation.”⁴⁸

Tulare County General Plan Policies (Sustainability)

The Tulare County General Plan has a number of policies that apply to projects within County of Tulare. General Plan policies that relate to Sustainability include the following.

LU-1.1 Smart Growth and Healthy Communities

The County shall promote the principles of smart growth and healthy communities in UDBs and HDBs, including:

1. Creating walkable neighborhoods,
2. Providing a mix of residential densities,
3. Creating a strong sense of place,
4. Mixing land uses,
5. Directing growth toward existing communities,
6. Building compactly,
7. Discouraging sprawl,
8. Encouraging infill,
9. Preserving open space,
10. Creating a range of housing opportunities and choices,
11. Utilizing planned community zoning to provide for the orderly pre-planning and long term development of large tracks of land which may contain a variety of land uses, but are under unified ownership or development control, and
12. Encouraging connectivity between new and existing development.

LU-1.8 Encourage Infill Development

The County shall encourage and provide incentives for infill development to occur in communities and hamlets within or adjacent to existing development in order to maximize the use of land within existing urban areas, minimize the conversion of existing agricultural land, and minimize environmental concerns associated with new development.

⁴⁸ Tulare County Climate Action Plan, page 1

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LU-7.15 Energy Conservation

The County shall encourage the use of solar power and energy conservation building techniques in all new development.

LU-7.16 Water Conservation

The County shall encourage the inclusion of “extra-ordinary” water conservation and demand management measures for residential, commercial, and industrial indoor and outdoor water uses in all new urban development.

LU-7.17 Shared Parking Facilities

The County shall encourage, where feasible, the use of shared parking facilities. Such areas could include developments with different day/night uses.

AQ-3.3 Street Design

The County shall promote street design that provides an environment which encourages transit use, biking, and pedestrian movements.

AQ-3.5 Alternative Energy Design

The County shall encourage all new development, including rehabilitation, renovation, and redevelopment, to incorporate energy conservation and green building practices to maximum extent feasible. Such practices include, but are not limited to: building orientation and shading, landscaping, and the use of active and passive solar heating and water systems.

AQ-3.6 Mixed Land Uses

The County shall encourage the clustering of land uses that generate high trip volumes, especially when such uses can be mixed with support services and where they can be served by public transportation.

TCAG Sustainable Communities Strategy (2014 Regional Transportation Plan)

AB 32 set emission targets for the State of California. SB375 requires the California Air Resources Board to set greenhouse gas emission targets for different regions in California. Under SB 375 Metropolitan Planning Organizations like TCAG are required to create a Sustainable Communities Strategy. TCAG included this strategy in the 2014 Regional Transportation Plan. Highlights of the implementation strategies include:

- Encourage jurisdictions in Tulare County to consider bicycle lanes, public transit, transit-oriented and mixed-use development, pedestrian networks, rain and other complete streets development during updates of general plan or other local plans.
- Implement a Complete Streets Program whereby agencies will prepare plans to accommodate all transportation users, including pedestrians, bicyclists, transit riders, and motor vehicle operators and riders, and implement those plans as aggressively as feasible.
- Provide for continued coordination and evaluation of the planned circulation system among cities and the county.
- Fund the development of capital improvement programs for complete streets and active transportation-type plans, as funds are available.

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- Evaluate intersections, bridges, interchanges, and rail grade crossings for needed safety improvements.
- Develop funding strategies for safety projects in cooperation with Caltrans and member agencies.
- Examine alternative funding sources for streets, roads, state highways, rail systems, transit, bicycle, pedestrian, and other transportation mode improvements.
- Utilize Cap and Trade funds available for transit, if available, for projects in Tulare County.
- Encourage local agencies to support implementation of bicycle support facilities such as bike racks, showers, and other facilities during the project review process.
- Utilize Cap and Trade funds available for bicycle and pedestrian projects, if available, for projects in Tulare County.
- Encourage mixed-use developments in urbanized areas.
- Encourage provision of an adequate supply of housing for the region's workforce and adequate sites to accommodate business expansion to minimize interregional trips and long-distance commuting.
- Support and participate in efforts and coalitions promoting use of Cap and Trade funding for projects that help reduce greenhouse gas emissions in Tulare County.
- Support investment in bicycle and pedestrian systems, giving attention to projects and networks that will allow residents to walk and bicycle to frequented destinations, including schools, parks, healthcare institutions and transit stops.
- Provide environmental justice communities opportunities for input into transportation plans, programs, and projects in a manner consistent with Title VI of the 1964 Civil Rights Act and Executive Order 12898 on Environmental Justice, including the prohibition of intentional discrimination and adverse disparate impact with regard to race, ethnicity or national origin.

These implementation strategies are compatible with the Tulare County General Plan policies.

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URBAN DEVELOPMENT BOUNDARY

Urban Development Boundaries (UDBs) are officially adopted and mapped County lines delineating the area expected for urban growth in cities and unincorporated communities over a 20-year period. Within UDB boundaries, the County and cities will coordinate plans, policies and standards related to building construction, subdivision development, land use and zoning regulations, street and highway construction, public utility systems, environmental studies, and other closely related matters affecting the orderly development of urban fringe areas. These boundaries provide an official definition of the interface between future urban and agricultural land uses.⁴⁹

“For unincorporated communities, the UDB is a County adopted line dividing land to be developed from land to be protected for agricultural, natural, open space, or rural uses. It serves as the official planning area for communities over a 20-year period. Land within an unincorporated UDB is assumed appropriate for development and is not subject to the Rural Valley Lands Plan or Foothill Growth Management Plan (RVLP Policy 1-1)”⁵⁰

The purpose of this chapter is to review the adequacy of the adopted Urban Development Boundary (UDB) and determine through analysis contained in this chapter whether modifications may be required. The proposed UDB will also function as the planning area boundary of the Earlimart Community Plan. The UDB line establishes a twenty-year growth boundary for the community of Earlimart. Over the years, services will be extended into this area which will allow new growth to occur. Since the UDB line defines the area where growth will occur, it is logical that it also serves as the planning area boundary for this plan.

In addition to defining the area in which future development of the community of Earlimart will occur, designating an urban boundary can provide local agencies and citizens with other benefits, including:

- a) Encouraging coordination between land use planning and the provision of governmental services.
- b) Identifying and resolving potential interagency conflicts regarding service areas.
- c) Encouraging efficient, economical and effective delivery of public services.
- d) Allowing property owners to identify the type and level of service their lands presently receive or may receive in the future.
- e) Assisting in the County's efforts to preserve open space and productive agricultural land.

⁴⁹ TCGPU Part 1, Goals and Policies Report, Planning Framework, page 2-3

⁵⁰ TCGPU Part 1, Goals and Policies Report, Planning Framework, page 2-3

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POLICY FRAMEWORK

In determining a UDB and planning area for the Earlimart Community Plan, it is important to not only fulfill the requirements of State planning law, but to the greatest degree possible, fulfill the local goals and policies that regulate land uses in the area. As discussed earlier, there are many County policies that guide development in Earlimart area. However, those which have direct effect on the establishment of the community's urban boundary include the following policies in the Tulare County General Plan Planning Framework Element:

PF-2.1 Urban Development Boundaries – Communities

The County shall limit urban development to the area within the designated UDB for each community.

PF-2.2 Modification of Community UDB

1. The County may consider modification to a community UDB under any of, but not limited to the following circumstances:
 - The location of the UDB shall be evaluated during preparation or update of a community plan.
 - All community UDBs should be reviewed on a five-year cycle to reflect changes in growth and development patterns.
 - A request for expansion of the UDB boundary can be applied for as part of a General Plan Amendment to the Land Use Diagram.
 - At the request of a special district or the community.
 - A UDB should be considered for expansion at such time as land for infill becomes limited. This condition is considered satisfied when 80 percent of the non-Williamson Act land within the UDB is developed for urban uses.
 - UDBs should not be expanded onto Prime Farmland if Farmland of Statewide Importance or of lesser quality is available and suitable for expansion.
2. Prior to approval of a UDB boundary expansion, the County shall ensure that infrastructure can be provided to serve the new areas added to the UDB and that sufficient water supplies are also available. This may require preparation of an infrastructure master plan that includes methods of financing of improvements and maintenance, as well as representation/documentation of availability and sufficiency of long-term water supplies.
3. Preservation of productive agricultural lands shall be the highest priority when considering modifications. Expansion of a UDB to include additional agricultural land shall only be allowed when other non-agricultural lands are not reasonably available to the community or are not suitable for expansion.

PF-2.3 UDB and Other Boundaries

The County shall provide notice and opportunity for special districts, school districts, and other service providers to comment when evaluating the expansion of a Community's UDB.

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PF-2.8 Inappropriate Land Use

Areas within UDBs are hereby set aside for those types of urban land uses which benefit from urban services. Permanent uses which do not benefit from such urban services shall be discouraged within the UDBs. This is not intended to apply to agricultural or agricultural supported uses, including the cultivation of land or other uses accessory to the cultivation of land, provided that such accessory uses are time-limited through special use permit procedures.

Earlimart's Adopted UDB:

Tulare County is proposing new land use and zoning designations within an expanded UDB. The proposed Community Plan Update, if adopted, will update these designations to be consistent with the General Plan, and will bring existing non-compliant properties into conformity with the Tulare County Zoning Ordinance. The existing UDB project also includes the Complete Streets and Road Maintenance programs. The project includes the community's anticipated growth through year 2030 based on the existing land uses, census population data, and the projected 1.3% annual growth rate in unincorporated areas of Tulare County.

The community of Earlimart is primarily rectangular in shape. Earlimart is bisected in a northwest-southeasterly direction by SR 99 and the Union Pacific Railroad which divides the community. The larger area of Earlimart is located on the East side of SR 99. The central commercial and industrial area is located between SR 99 and the Union Pacific Railroad on the west. The existing Earlimart Urban Development Boundary (UDB) area consists of approximately 1,400 acres.

Based on a 1.3% annual growth rate, due to limitations on much of the undeveloped land area within the existing UDB (such as Williamson Act properties, limitations set by existing land uses and zoning, and lack of interest or inability of property owners to develop) an expansion is necessary to accommodate the projected growth of the community. The expansion is approximately 541 acres to capture pass through traffic along the SR 99 Corridor. The proposed UDB expansion area boundaries are necessary to place the UDB boundary lines along logical alignments (such as property lines and roadways) and to include existing facilities and/or residences that are currently outside of the UDB. Future developments within the northern expansion area requires additional land use entitlements and infrastructure planning; and therefore, will be required to undergo additional CEQA evaluation on a project-by-project basis at such time as entitlements and/or developments are proposed.

Existing Land Use Plan:

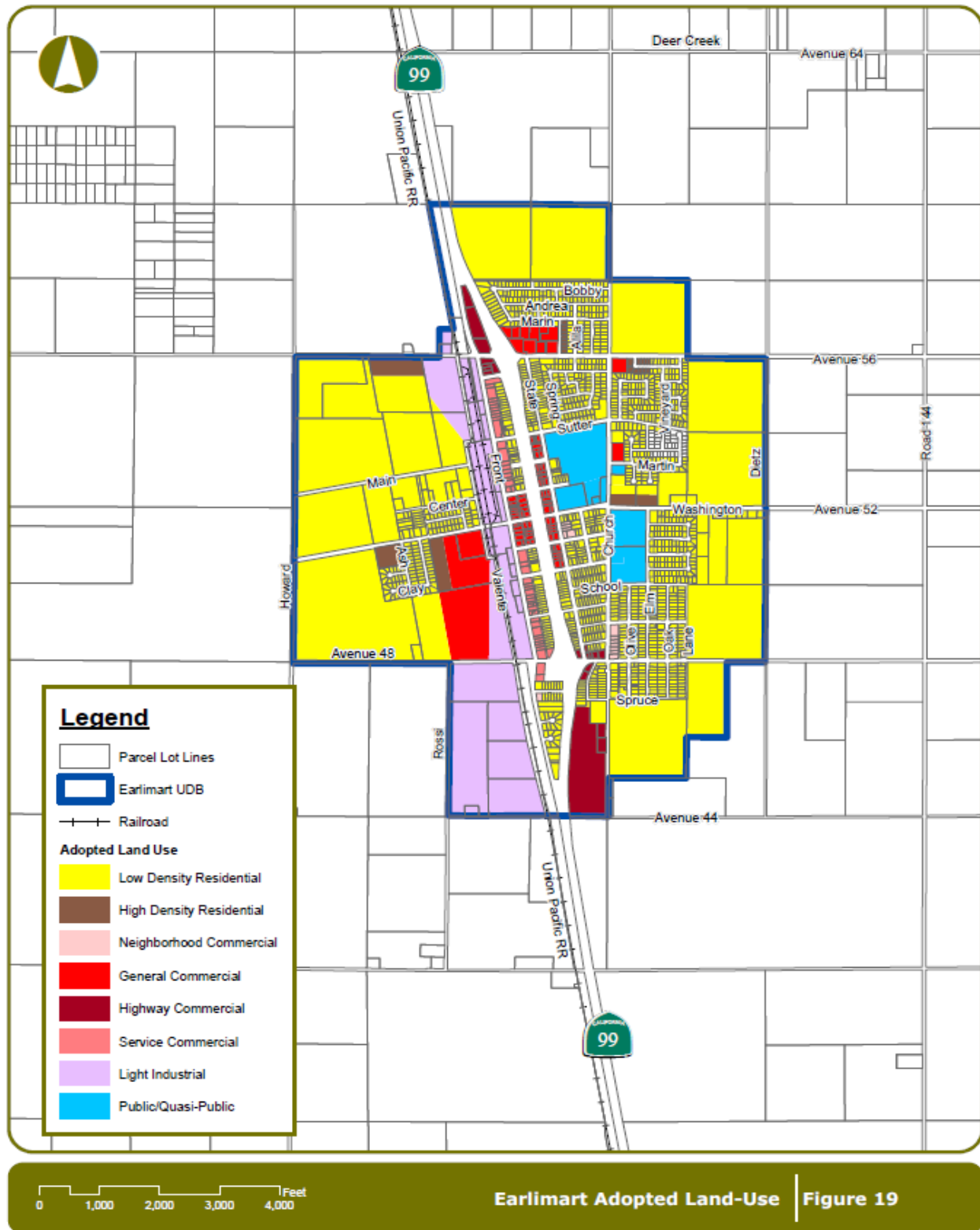
The land use designations within the existing and the proposed Earlimart Community Plan UDB are shown in **Figures 19 & Figure 21**, respectively. As demonstrated in **Table 20**, residential land use designations constitute approximately 60.5%, commercial 7.8%, industrial 10.2%, public/quasi-public and parks 3.6%, rights-of-way 16.3%, and no designation 1.5% of the existing UDB area.

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Table 20 - Existing Land Use Plan		
Designation	Existing Acres	Percent*
General Commercial	60.17	4.30
High Density Residential	21.62	1.54
Highway Commercial	32.69	2.33
Light Industrial	142.6	10.19
Low Density Residential	826.3	59.03
N/A (Blank Land Use Designation	21.07	1.5
Neighborhood Commercial	3.32	0.24
Park	5.15	0.36
Public/Quasi-Public	44.72	3.19
Service Commercial	14.15	1.01
Rights-of-Way	228.0	16.29
Total	1,399.7	100%
* Percentages have been rounded.		
Source: Tulare County GIS		

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Figure 19 - Existing Land Use Plan



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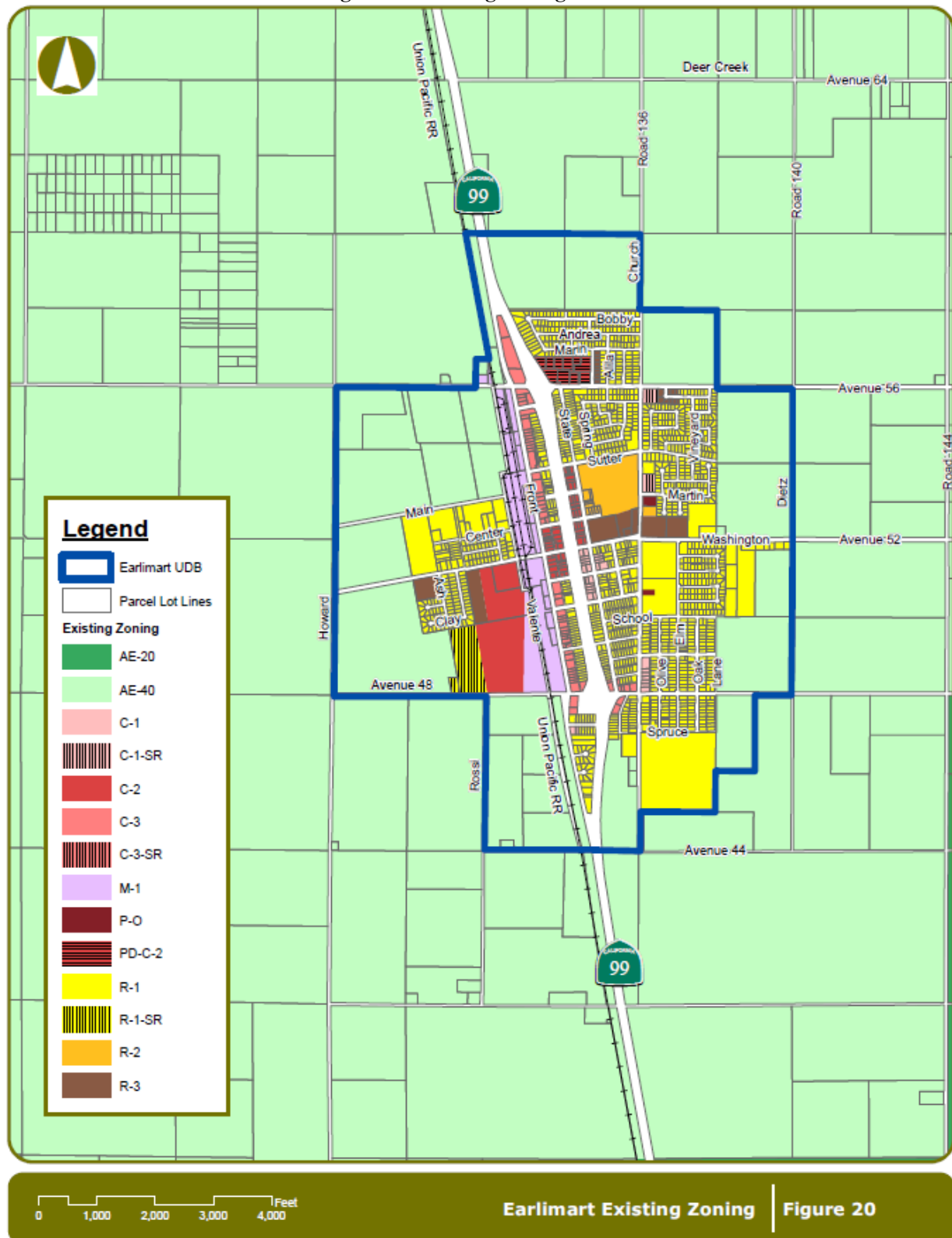
Existing Zoning Districts

The zoning districts within the existing Earlimart Community Plan UDB are shown in **Figure 20**. As demonstrated in **Table 21**, residential zones constitute 27.7%, commercial and professional office zones comprise 6.2%, industrial 3.46%, agricultural 44.66%, and rights-of-way 16.28% in the existing UDB area.

Table 21 - Existing Zoning Districts		
Zoning Designations	Existing Acres	Percent*
AE-40	625.1	44.66
C-1	6.47	0.46
C-1-SR	3.052	0.22
C-2	45.27	3.23
C-3	22.73	1.62
C-3-SR	0.13	0.0
M-1	48.5	3.46
PD-C-2	8.87	0.63
P-O	1.49	0.10
R-1	349.6	24.97
R-1-SR	12.97	0.92
R-2	21.62	1.54
R-3	26.05	1.86
Rights-of-Way	228.0	16.28
Total	1399.7	100.0
Source: Tulare County GIS		

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Figure 20 - Existing Zoning Districts



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Proposed Planning Areas

To develop additional economic development opportunities and respond to market forces, the New High School site within the existing UDB (**see Figure 4**) is proposed for revision. The revision of the UDB includes the proposed Northern Earlimart Rezone Project. The Community Plan Update proposes a northern expansion of the existing UDB (which includes the northern rezone project area), various amendments to land use and zoning designations (in both existing and proposed UDB expansion areas), and the Complete Streets and Road Maintenance programs (within the existing UDB area).

The northern expansion area includes 13 parcels totaling approximately 541 acres immediately north of the existing UDB area. As such, the proposed Community Plan Update will expand the existing 1,399.7 acre UDB by approximately 39%, for a total UDB area of approximately 1,945 acres.

Proposed residential land use designations (**see Figure 21**) increase to 63.69%, commercial increases to 10.27%, industrial decreases to 8.54%, public/quasi-public increases to 4.99%, and rights-of-way decreases to 12.44%, of the proposed UDB area (**see Table 22**).

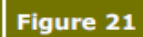
Table 22 - Proposed Land Use		
Land Use	Proposed Acres	Percent*
General Commercial	98.99	5.08
High Density Residential	65.09	3.34
Highway Commercial	31.95	1.64
Light Industrial	166.4	8.53
Low Density Residential	999.4	51.36
Low-Medium Density Residential	1.85	0.09
Neighborhood Commercial	5.88	0.30
Public/Quasi-Public	97.23	4.99
Service Commercial	14.45	0.74
Urban Reserve-Commercial	49.09	2.52
Urban Reserve-Residential	173	8.89
Rights-of-Way	242.2	12.44
Total	1,945.6	100%

The proposed residential zones (**see Figure 22**) increase to 32.4%, commercial and professional office increase to 9.27%, industrial increases to 3.87%, agricultural decreases to 40.81%, and rights-of-way decrease to 12.44%, of the proposed UDB area (**see Table 23**).

Table 23 - Proposed Zoning Districts		
Zoning Districts	Proposed Acres	Percent
AE-40	794.1	40.81
C-1	5.84	0.3
C-1-MU	0.46	0.02
C-2	43.41	2.23
C-2-MU	46.28	2.37
C-3	0.29	0.01
C-3-MU	22.87	1.17
M-1	75.38	3.87
P-O	84.1	4.32
R-1	544.9	28.0
R-1-MU	23.81	1.22
R-2	1.85	0.09
R-3	59.69	3.06
R-3-MU	0.31	0.01
Rights-of-Way	242.2	12.44
Total	1,945.6	100%

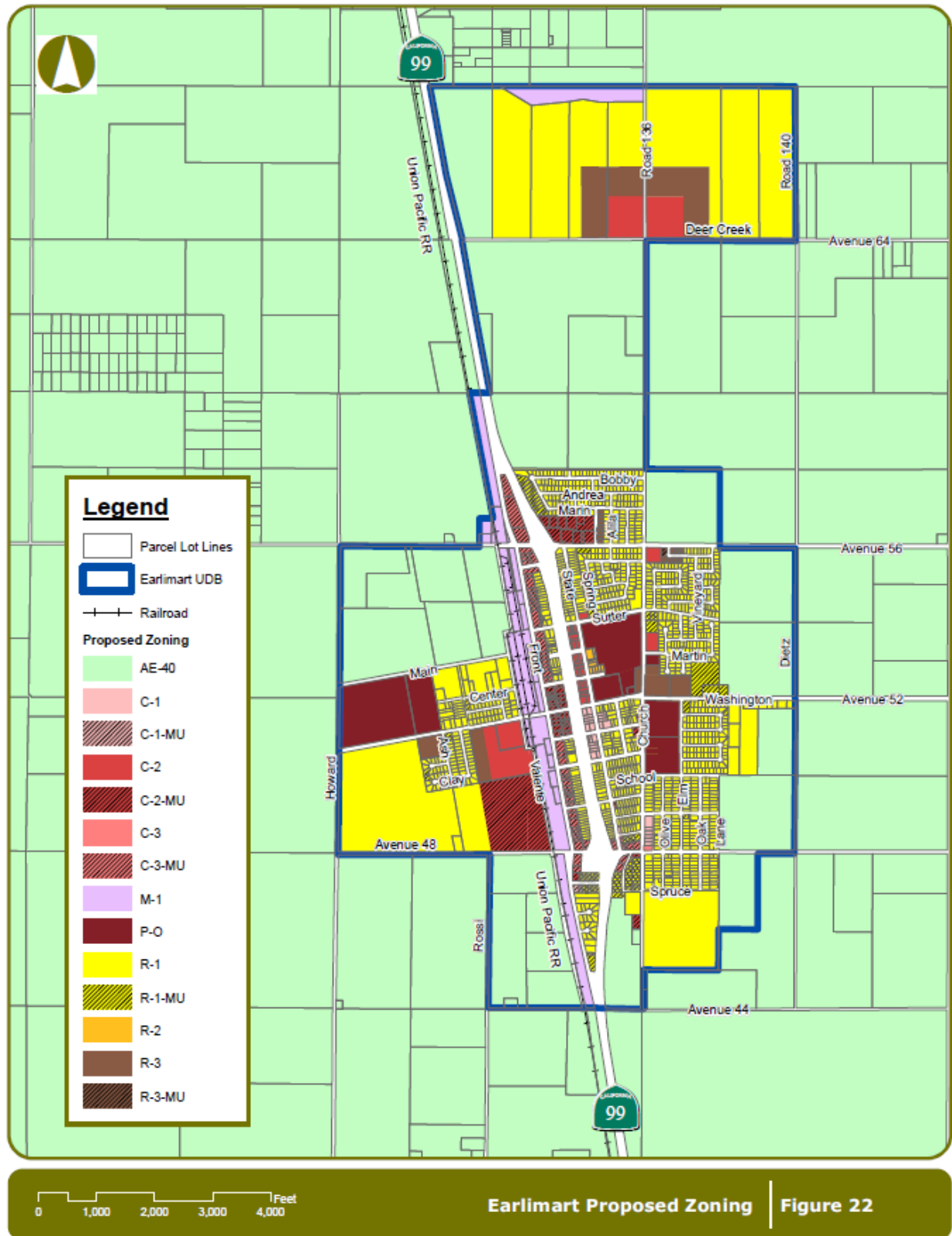
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Figure 22 - Proposed Zoning Districts



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POLICY PLAN

This chapter of the Earlimart Community Plan prescribes the policy framework which will govern the development of the community over the term of the planning period (through the year 2030). It includes text which sets out explicit policy statements about the quality, character, and manner in which development in the community will take place

The plan, although long-range in scope, is to be used on a day to day basis to guide the decisions of County staff, the Planning Commission, and the Board of Supervisors as they affect community development. Further, it will provide residents and property owners in the community with direction and guidelines regarding the evolution and growth of their town and its resources. And importantly, this plan will aid other public agencies and entities, such as the school district and the water company, in their own long-range planning and capital expenditure programming. Each subsequent section of this chapter addresses a topical aspect of the community planning environment. For each aspect, background discussion of relevant issues is included, policies are stated, and implementation programs and activities are outlined.

Policy Relationship to the General Plan

The Earlimart Community Plan is a component in Part III of the Tulare County General Plan and, as such, has the same force and effect as any other adopted element of the general plan. Structurally, the Earlimart Community Plan is part of the Land Use and Circulation Element of the overall general plan. The principal emphasis of the community plan is on establishing local land use and circulation system patterns and prescribing associated standards and policies. In addition to the specific prescriptions of the community plan, the broader policies and standards of the overall Land Use and Circulation Element apply to Earlimart.

Also applicable to Earlimart, and governing all future development in the community, are the other elements (e.g. Planning Framework, Environmental Resources Management, Air Quality, Health and Safety, Transportation and Circulation, etc.) of the Tulare County General Plan. In instances where the policies and/or standards of the Earlimart Community Plan are more specific or more restrictive than those in other elements of the general plan, the community plan shall take precedence and prevail.

Land Use Plan

One of the most important purposes of the Earlimart Community Plan is to establish land use patterns and development policies and standards for the community for the planning period, through the year 2030. The general intent of the land use plan for Earlimart is to identify the most appropriate types and distribution of land uses for the community, based on environmental, circulation, infrastructure, services, opportunities and constraints, urban development boundary suitability analysis and other economic capacities and concerns discussed in the previous chapters of the plan.

The County of Tulare, through existing policies, has encouraged both incorporated and unincorporated communities to establish urban development and land use patterns which are compact and contiguous. This policy position has reduced so-called “leap frog” development County-wide, has helped preserve agricultural lands, and has minimized land use conflicts between urban and agricultural areas. This boundary is sensitive to and consistent with the County-wide policy of encouraging

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compact and contiguous urban growth, and at the same time enables the community to accommodate any foreseen demand for economic and population expansion. **Figure 21**, on page 112, depicts and prescribes the proposed land use pattern development recommended for Earlimart.

As illustrated in this proposed land use diagram, future urban development within the existing UDB community will be concentrated on the west side of SR 99 adjacent to the new high school. The land use plan for the community provides for substantial expansion north of the present UDB and east of SR 99 allows for additional light industrial development with good highway access; provides for the development of additional commercial uses potentially serving community-oriented demand; and enables the development of additional residential uses to meet demand generated by forecasted population growth.

New High School Project

The proposed Land Use Designation for the High School Project area is Public/Quasi-Public (33 acres). This project proposes the construction of a new 350-student high school on approximately 17 acres. This project is located within the boundaries of the existing Earlimart UDB, approximately one half mile west of SR 99. The majority of the residential and commercial properties within the Earlimart UDB sit east of SR 99. This project is intended to accommodate both the existing and projected student population through horizon year 2030. The proposed technical high school has been provisioned to serve an agricultural and agronomist-based demographic with educational services requisite for preparing future generations to work in geographic locales such as the Central Valley.

Northern Earlimart Rezone Project

The proposed Land Use Designation for the Northern Earlimart Rezone Project are as follows: Urban Reserve-Commercial (49.09 acres), Urban Reserve-Residential (172.97 acres), Low Density Residential (234.76 acres), High Density-Residential 39.88 acres, General Commercial (20.57 acres), and Light Industrial (14.28 acres). The Northern Earlimart Rezone Project is the planned conversion of agriculturally-designated and zoned parcels to urban-type uses consistent with long-term developmental goals as outlined in the Tulare County General Plan 2030 Update. There are no development projects currently proposed with the rezone project, however, the rezoning of approximately 316 acres of approximately a 541-acre project area would accommodate the eventual urbanization of the site. Construction and development of the project area would occur north of the present UDB and east of SR 99.

Land Use Designations

The land use designations within the existing Earlimart Community Plan UDB are shown in **Figure 19**. As demonstrated in **Table 20**, residential land use constitute 61.3%, commercial 7.8%, industrial 10.4%, public/quasi-public 3.6%, and rights-of-way 17.0% of the existing UDB area.

The following land use designations along with descriptions including density and intensity are recommended for Earlimart to address land demand needs through the 2030 planning horizon year.

Low Density Residential (LDR)

This designation establishes areas for single-family residences with individual homes on lots generally ranging from 12,500 square feet to one acre. Uses typically allowed include: detached single-family homes; secondary dwellings; and residential support uses such as churches, schools, and other

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necessary public utility and safety facilities. This designation is typically found inside communities or on the outside edge of UDBs.

Maximum Density: 1-4 Dwelling Units/Acre

- When areas in this designation are identified as primary recharge areas for a community's water system, acreage minimums should not be below 2 acres.
- Areas with 30 percent or higher average slopes should have acreage minimums in excess of 3 acres.

High Density Residential (HDR): This designation established areas for multi-family dwellings in urbanized areas. Uses typically allowed include: duplexes, townhouses, and apartments located near schools, parks, and other public services. This designation is used only within UDBs. Dwelling Units are based on Gross Acreage and development shall be no less than that identified as the intensity per gross acreage High Density Residential designated lands.

Maximum Density: 14-30 Dwelling Units/Acre

Neighborhood Commercial (NC): This designation establishes areas for small-scale, general retail, and service businesses that provide goods to the immediate surrounding area. Uses typically allowed include: food and beverage retail sales; limited personal, medical, professional, and repair services; and retail sales. This designation is found primarily within UDBs.

Maximum Intensity: 0.5 FAR

General Commercial (GC): This designation establishes areas for small, localized retail, recreational, and service businesses that provide goods and services to the surrounding community. Uses typically allowed include: eating and drinking establishments; food and beverage retail sales; limited personal, medical, professional services; repair services; and retail sales. Such facilities may range from a single use to a cluster of uses such as a shopping center. This designation is found primarily within UDBs.

Maximum Intensity: 0.5 FAR

Highway Commercial (HC): This designation establishes areas for retail, recreational, and service-based businesses which provide goods and services to tourists and commuters along major highways. Uses typically allowed include: big box retail; eating and drinking establishments; food and beverage retail sales; limited repair services; lodging (hotels and motels); and retail sales. Such facilities may range from a single use to a cluster of uses located at a freeway off ramp or major highway intersection. This designation is located primarily within UDBs and pursuant to regional growth corridor plans and policies.

Maximum Intensity: 0.5 FAR

Light Industrial (LI): This designation establishes areas for a range of non-intensive business park, industrial park, and storage uses that do not have detrimental noise or odor impacts on surrounding urban uses. Uses typically allowed include: warehousing, welding & fabrication shops, manufacturing & processing, and business support uses such as retail or eating establishments that serve adjacent

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light industrial uses and employees. This designation is found primarily within UDBs and pursuant to regional growth corridor plans and policies.

Maximum Intensity: 0.5 FAR

Public/Quasi-Public (P/QP): This designation establishes areas for public and quasi-public services and facilities that are necessary to maintain the welfare of County residents and businesses. Uses typically allowed include: churches; schools; civic centers; hospitals; fire stations; sheriff stations; liquid and solid waste disposal sites; cemeteries; airports; and public utility and safety facilities. This designation is found primarily within UDBs and pursuant to regional growth corridor plans and policies.

Density/Intensity: None Specific

Urban Reserve (UR): This designation establishes a holding zone whereby properties shall remain zoned for agriculture or open space use until such a time as conversion to urban uses is deemed appropriate. The UR designation shall be appended by the intended future land use designation, for example, **Urban Reserve Commercial (UR)** and **Urban Reserve Residential (UR)**. When a rezoning occurs without a General Plan amendment, the UR designation shall be removed from the parcel. This designation applies primarily within UDBs.

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GENERAL PLAN POLICIES

The following adopted policies from the Tulare County General Plan are applicable to the Earlimart Community Plan. Policies throughout the General Plan use the terminology “shall” and “should.” For the purposes of interpreting the policies in the General Plan, the term “shall” indicates a mandatory or required action or a duty to undertake an action unless the context indicates otherwise, in which case the term is synonymous with “should.” The term “should” indicates a directive subject to discretion and requires at least review or consideration and, in that context, substantial compliance with the spirit or purpose of these General Plan policies. The term “may” indicates at the sole discretion of the County.

Land Use Policies

PF-1.3 Land Uses in UDBs/HDBs

The County shall encourage those types of urban land uses that benefit from urban services to develop within UDBs and HDBs. Permanent uses which do not benefit from urban services shall be discouraged within these areas. This shall not apply to agricultural or agricultural support uses, including the cultivation of land or other uses accessory to the cultivation of land provided that such accessory uses are time-limited through Special Use Permit procedures.

PF-2.6 Land Use Consistency

The County shall require all community plans, when updated, to use the same land use designations as used in this Countywide General Plan (See Chapter 4-Land Use). All community plans shall also utilize a similar format and content. The content may change due to the new requirements such as Global Climate Change and Livable Community Concepts, as described on the table provided (Table 2.1: Community Plan Content). Changes to this format may be considered for unique and special circumstances as determined appropriate by the County. Until such time as a Community Plan is adopted for those communities without existing Community Plans, the land use designation shall be Mixed Use, which promotes the integration of a compatible mix of residential types and densities, commercial uses, public

facilities, and services and employment opportunities.

PF-2.8 Inappropriate Land Use

Areas within UDBs are hereby set aside for those types of urban land uses which benefit from urban services. Permanent uses which do not benefit from such urban services shall be discouraged within the UDBs. This is not intended to apply to agricultural or agricultural supported uses, including the cultivation of land or other uses accessory to the cultivation of land, provided that such accessory uses are time-limited through special use permit procedures.

PF-4.7 Avoiding Isolating Unincorporated Areas

The County may oppose any annexation proposal that creates an island, peninsula, corridor, or irregular boundary. The County will also encourage the inclusion of unincorporated islands or peninsulas adjacent to proposed annexations.

Land Use Element

LU-1.1 Smart Growth and Healthy Communities

The County shall promote the principles of smart growth and healthy communities in UDBs and HDBs, including:

1. Creating walkable neighborhoods,
2. Providing a mix of residential densities,
3. Creating a strong sense of place,

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4. Mixing land uses,
5. Directing growth toward existing communities,
6. Building compactly,
7. Discouraging sprawl,
8. Encouraging infill,
9. Preserving open space,
10. Creating a range of housing opportunities and choices,
11. Utilizing planned community zoning to provide for the orderly pre-planning and long term development of large tracks of land which may contain a variety of land uses, but are under unified ownership or development control, and
12. Encouraging connectivity between new and existing development.

LU-1.2 Innovative Development

The County shall promote flexibility and innovation through the use of planned unit developments, development agreements, specific plans, Mixed Use projects, and other innovative development and planning techniques.

LU-1.3 Prevent Incompatible Uses

The County shall discourage the intrusion into existing urban areas of new incompatible land uses that produce significant noise, odors, or fumes.

LU-1.4 Compact Development

The County shall actively support the development of compact mixed use projects that reduce travel distances.

LU- 1.5 Paper Subdivision Consolidations

The County shall encourage consolidation of paper parcels/subdivisions, especially those lots that are designated Valley Agriculture (VA), Foothill Agriculture (FA), or Resource Conservation (RC), are irregular in shape, inadequate in size for proper use, or lack infrastructure.

LU-1.6 Permitting Procedures and Regulations

The County shall continue to ensure that its permitting procedures and regulations are consistent and efficient.

LU-1.7 Development on Slopes

The County shall require a preliminary soils report for development projects in areas with shallow or unstable soils or slopes in excess of 15 percent. If the preliminary soil report indicates soil conditions could be unstable, a detailed geologic/hydrologic report by a registered geologist, civil engineer, or engineering geologist shall be required demonstrating the suitability of any proposed or additional development.

LU-1.8 Encourage Infill Development

The County shall encourage and provide incentives for infill development to occur in communities and hamlets within or adjacent to existing development in order to maximize the use of land within existing urban areas, minimize the conversion of existing agricultural land, and minimize environmental concerns associated with new development.

LU-1.10 Roadway Access

The County shall require access to public roadways for all new development.

LU-3.1 Residential Developments

The County shall encourage new major residential development to locate near existing infrastructure for employment centers, services, and recreation.

LU-3.2 Cluster Development

The County shall encourage proposed residential development to be clustered onto portions of the site that are more suitable to accommodating the development, and shall require access either directly onto a public road or via a privately-maintained road designed to meet County road standards.

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LU-3.3 High-Density Residential

Locations

The County shall encourage high-density residential development (greater than 14 dwelling units per gross acre) to locate along collector roadways and transit routes, and near public facilities (e.g., schools, parks), shopping, recreation, and entertainment.

LU-3.5 Rural Residential Designations

The County shall not re-zone any new areas for residential development in the RVLDP area, unless it can be shown that other objectives, such as buffers and the relationship of the development to surrounding uses, can be achieved.

LU-3.6 Project Design

The County shall require residential project design to consider natural features, noise exposure of residents, visibility of structures, circulation, access, and the relationship of the project to surrounding uses. Residential densities and lot patterns will be determined by these and other factors. As a result, the maximum density specified by General Plan designations or zoning for a given parcel of land may not be attained.

LU-7.3 Friendly Streets

The County shall encourage new streets within UDBs to be designed and constructed to not only accommodate traffic, but also serve as comfortable pedestrian and cyclist environments. These should include, but not be limited to:

1. Street tree planting adjacent to curbs and between the street and sidewalk to provide a buffer between pedestrians and automobiles, where appropriate,
2. Minimize curb cuts along streets,
3. Sidewalks on both sides of streets, where feasible,
4. Bike lanes and walking paths, where feasible on collectors and arterials, and

5. Traffic calming devices such as roundabouts, bulb-outs at intersections, traffic tables, and other comparable techniques.

LU-7.4 Streetscape Continuity

The County shall ensure that streetscape elements (e.g., street signs, trees, and furniture) maintain visual continuity and follow a common image for each community.

LU-7.5 Crime Prevention through Design

The County shall encourage design of open space areas, bicycle and pedestrian systems, and housing projects so that there is as much informal surveillance by people as possible to deter crime.

LU-7.15 Energy Conservation

The County shall encourage the use of solar power and energy conservation building techniques in all new development.

LU-7.16 Water Conservation

The County shall encourage the inclusion of “extra-ordinary” water conservation and demand management measures for residential, commercial, and industrial indoor and outdoor water uses in all new urban development.

LU-7.13 Preservation of Historical Buildings

The County shall encourage and support efforts by local preservation groups to identify and rehabilitate historically significant buildings.

LU-7.14 Contextual and Compatible Design

The County shall ensure that new development respects Tulare County’s heritage by requiring that development respond to its context, be compatible with the traditions and character of each community, and develop in an orderly fashion which is compatible with the scale of surrounding structures.

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LU-7.15 Energy Conservation

The County shall encourage the use of solar power and energy conservation building techniques in all new development.

LU-7.16 Water Conservation

The County shall encourage the inclusion of “extra-ordinary” water conservation and demand management measures for residential, commercial, and industrial indoor and outdoor water uses in all new urban development.

Circulation Policies

Q-3.3 Street Design

The County shall promote street design that provides an environment which encourages transit use, biking, and pedestrian movements.

SL-4.1 Design of Highways

The County shall work with Caltrans and Tulare County Association of Governments (TCAG) to ensure that the design of SR 99 and other State Highways protects scenic resources and provides access to vistas of working and natural landscapes by:

1. Limiting the construction of sound walls that block views of the County’s landscapes (incorporate setbacks to sensitive land uses to avoid noise impacts whenever feasible),
2. Using regionally-appropriate trees and landscaping and incorporating existing landmark trees,
3. Preserving historic and cultural places and vistas,
4. Avoiding excessive cut and fill for roadways along State scenic highways and County scenic routes, and along areas exposed to a large viewing area, and
5. Promote highway safety by identifying appropriate areas for traffic pull-outs and rest areas.

SL-4.2 Design of County Roads

The County’s reinvestment in rural County roads outside urban areas should, in addition to meeting functional needs and safety needs, preserve the experience of traveling on the County’s “country roads” by:

1. Maintaining narrow as possible rights-of-ways,
2. Limiting the amount of curbs, paved shoulders, and other “urban” edge improvements,
3. Preserving historic bridges and signage, and
4. Promote County road safety by identifying appropriate areas for traffic pull-out.

SL-4.3 Railroads and Rail Transit

The County shall encourage rail infrastructure for freight and passenger service to be planned and designed to limit visual impacts on scenic landscapes by:

1. Concentrating infrastructure in existing railroad rights-of-ways,
2. Avoiding additional grade separated crossings in viewshed locations, and
3. Using new transit stations supporting rail transit as design features in existing and future core community areas.

TC-1.1 Provision of an Adequate Public Road Network

The County shall establish and maintain a public road network comprised of the major facilities illustrated on the Tulare County Road Systems to accommodate projected growth in traffic volume.

TC-1.2 County Improvement Standards

The County's public roadway system shall be built and maintained consistent with adopted County Improvement Standards, and the need and function of each roadway, within constraints of funding capacity.

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TC-1.6 Intermodal Connectivity

The County shall ensure that, whenever possible, roadway, highway, and public transit systems will interconnect with other modes of transportation. Specifically, the County shall encourage the interaction of truck, rail, and air-freight/passenger movements.

TC-1.7 Intermodal Freight Villages

The County shall consider the appropriate placement of intermodal freight villages in locations within the Regional Growth Corridors.

TC-1.8 Promoting Operational Efficiency

The County shall give consideration to transportation programs that improve the operational efficiency of goods movement, especially those that enhance farm-to-market connectivity

TC-1.9 Highway Completion

The County shall support State and Federal capacity improvement programs for critical segments of the State Highway System. Priority shall be given to improvements to State Routes 65, 99, and 198, including widening and interchange projects in the County.

TC-1.10 Urban Interchanges

The County shall work with TCAG to upgrade State highway interchanges from rural to urban standards within UDBs.

TC-1.11 Regional Significant Intersections

To enhance safety and efficiency, the County shall work to limit the frequency of intersections along regionally-significant corridors.

TC-1.12 Scenic Highways and Roads

The County shall work with appropriate agencies to support the designation of scenic highways and roads in the County.

TC-1.16 County Level Of Service (LOS) Standards

The County shall strive to develop and manage its roadway system (both segments and intersections) to meet a LOS of “D” or better in accordance with the LOS definitions established by the Highway Capacity Manual.

TC-1.18 Balanced System

The County shall strive to meet transportation needs and maintain LOS standards through a balanced Multimodal Transportation Network that provides alternatives to the automobile.

TC-2.1 Rail Service

The County shall support improvements to freight and expanding passenger rail service throughout the County.

TC-2.2 Rail Improvements

The County shall work with cities to support improvement, development, and expansion of passenger rail service in Tulare County.

TC-2.3 Amtrak Service

The County shall encourage Amtrak to add passenger service to the Union Pacific corridor in the County.

TC-2.5 Railroad Corridor Preservation

The County shall work with other agencies to plan railroad corridors to facilitate the preservation of important railroad rights-of-way for future rail expansion or other appropriate transportation facilities.

TC-2.7 Rail Facilities and Existing Development

The County will work with the California Public Utilities Commission (CPUC) to ensure that new railroads rights-of-ways, yards, or stations adjacent to existing residential or commercial areas are screened or buffered to reduce noise, air, and visual impacts. Similarly, the County should coordinate with the CPUC and railroad service providers to address

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railroad safety issues as part of all future new development that affects local rail lines. Specific measures to be considered and incorporated into the design of future projects affecting rail lines include, but are not limited to, the installation of grade separations, warning signage, traffic signaling improvements, vehicle parking prohibitions, installation of pedestrian-specific warning devices, and the construction of pull out lanes for buses and vehicles.

TC-4.7 Transit Ready Development

The County shall promote the reservation of transit stops in conjunction with development projects in likely or potential locations for future transit facilities.

TC-5.1 Bicycle/Pedestrian Trail System

The County shall coordinate with TCAG and other agencies to develop a Countywide integrated multi-purpose trail system that provides a linked network with access to recreational, cultural, and employment facilities, as well as offering a recreational experience apart from that available at neighborhood and community parks.

TC-5.2 Consider Non-Motorized Modes in Planning and Development

The County shall consider incorporating facilities for non-motorized users, such as bike routes, sidewalks, and trails when constructing or improving transportation facilities and when reviewing new development proposals. For developments with 50 or more dwelling units or non-residential projects with an equivalent travel demand, the feasibility of such facilities shall be evaluated.

TC-5.3 Provisions for Bicycle Use

The County shall work with TCAG to encourage local government agencies and businesses to consider including bicycle access and provide safe bicycle parking facilities at

office buildings, schools, shopping centers, and parks.

TC-5.4 Design Standards for Bicycle Routes

The County shall utilize the design standards adopted by Caltrans and as required by the Streets and Highway Code for the development, maintenance, and improvement of bicycle routes.

TC-5.5 Facilities

The County shall require the inclusion of bicycle support facilities, such as bike racks, for new major commercial or employment locations.

TC-5.7 Designated Bike Paths

The County shall support the creation and development of designated bike paths adjacent to or separate from commute corridors.

TC-5.8 Multi-Use Trails

The County shall encourage the development of multi-use corridors (such as hiking, equestrian, and mountain biking) in open space areas, along power line transmission corridors, utility easements, rivers, creeks, abandoned railways, and irrigation canals.

TC-5.9 Existing Facilities

The County shall support the maintenance of existing bicycle and pedestrian facilities.

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Housing Policies

Housing Guiding Principle 1.1

Endeavor to improve opportunities for affordable housing in a wide range of housing types in the communities throughout the unincorporated area of the County.

Housing Policy 1.11

Encourage the development of a broad range of housing types to provide an opportunity of choice in the local housing market.

Housing Policy 1.13

Encourage the utilization of modular units, prefabricated units, and manufactured homes.

Housing Policy 1.14

Pursue an equitable distribution of future regional housing needs allocations, thereby providing a greater likelihood of assuring a balance between housing development and the location of employment opportunities.

Housing Policy 1.15

Encourage housing counseling programs for low income homebuyers and homeowners.

Housing Policy 1.16

Review community plans and zoning to ensure they provide for adequate affordable residential development.

Housing Guiding Principle 1.2

Promote equal housing opportunities for all persons regardless of race, religion, sex, marital status, ancestry, national origin, color, family status, disability, or any other arbitrary basis.

Housing Guiding Principle 1.3

Strive to meet the housing needs of migrant and non-migrant farmworkers and their families with a suitable, affordable and satisfactory living environment.

Housing Policy 1.31

Encourage the provision of farmworker housing opportunities in conformance with the Employee Housing Act.

Housing Guiding Principle 1.4

Enhance and support emergency shelters and transitional and supportive housing programs that assist the homeless and others in need.

Housing Policy 1.51

Encourage the construction of new housing units for “special needs” groups, including senior citizens, large families, single heads of households, households of persons with physical and/or mental disabilities, minorities, farmworkers, and the homeless in close proximity to transit, services, and jobs.

Housing Policy 1.52

Support and encourage the development and improvement of senior citizen group housing, convalescent homes and other continuous care facilities.

Housing Policy 1.55

Encourage development of rental housing for large families, as well as providing for other housing needs and types.

Housing Guiding Principle 1.6

Assess and amend County ordinances, standards, practices and procedures considered necessary to carry out the County’s essential housing goal of the attainment of a suitable, affordable and satisfactory living environment for every present and future resident in unincorporated areas.

Housing Policy 2.14

Create and maintain a matrix of Infrastructure Development Priorities for Disadvantaged Unincorporated Communities in Tulare County thorough analysis and investigation of public infrastructure needs and deficits, pursuant to Action Program 9.

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Housing Guiding Principle 2.2

Require proposed new housing developments located within the development boundaries of unincorporated communities to have the necessary infrastructure and capacity to support the development.

Housing Policy 2.21

Require all proposed housing within the development boundaries of unincorporated communities is either (1) served by community water and sewer, or (2) that physical conditions permit safe treatment of liquid waste by septic tank systems and the use of private wells.

Housing Guiding Principle 3.1

Encourage “smart growth” designed development that serves the unincorporated communities, the environment, and the economy of Tulare County.

Housing Policy 3.11

Support and coordinate with local economic development programs to encourage a “jobs to housing balance” throughout the unincorporated area.

Housing Policy 3.23

Prepare new and/or updated community plans that provide adequate sites for a variety of types of housing within the development boundaries of community.

Conservation Policies

AG-1.1 Primary Land Use

The County shall maintain agriculture as the primary land use in the valley region of the County, not only in recognition of the economic importance of agriculture, but also in terms of agriculture’s real contribution to the conservation of open space and natural resources.

AG-1.4 Williamson Act in UDBs and HDBs

The County shall support non-renewal or cancellation processes that meet State law for lands within UDBs and HDBs.

AG-1.5 Substandard Williamson Act Parcels

The County may work to remove parcels that are less than 10 acres in Prime Farmland and less than 40 Acres in Non-Prime Farmland from Williamson Act Contracts (Williamson Act key term for Prime/Non-Prime).

AG-1.6 Conservation Easements

The County shall consider developing an Agricultural Conservation Easement Program (ACEP) to help protect and preserve agricultural lands (including “Important Farmlands”), as defined in this Element. This program may require payment of an in-lieu fee sufficient to purchase a farmland conservation easement, farmland deed restriction, or other farmland conservation mechanism as a condition of approval for conservation of important agricultural land to non-agricultural use. If available, the ACEP shall be used for replacement lands determined to be of statewide significance (Prime or other Important Farmlands), or sensitive and necessary for the preservation of agricultural land, including land that may be a part of a community separator as part of a comprehensive program to establish community separators. The in-lieu fee or other conservation mechanism shall recognize the importance of land value and shall require equivalent mitigation.

AG-1.7 Preservation of Agricultural Lands

The County shall promote the preservation of its agricultural economic base and open space resources through the implementation of resource management programs such as the Williamson Act, Rural Valley Lands Plan, Foothill Growth Management Plan or similar

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types of strategies and the identification of growth boundaries for all urban areas located in the County.

AG-1.8 Agricultural within Urban Development Boundary

The County shall not approve applications for preserves or regular Williamson Act contracts on lands located within a UDB and/or HDB unless it is demonstrated that the restriction of such land will not detrimentally affect the growth of the community involved for the succeeding 10 years, that the property in question has special public values for open space, conservation, other comparable uses, or that the contract is consistent with the publicly desirable future use and control of the land in question. If proposed within a UDB of an incorporated city, the County shall give written notice to the affected city pursuant to Government Code §51233.

AG-1.10 Extension of Infrastructure into Agricultural Areas

The County shall oppose extension of urban services, such as sewer lines, water lines, or other urban infrastructure, into areas designated for agriculture use unless necessary to resolve a public health situation. Where necessary to address a public health issue, services should be located in public rights-of-way in order to prevent interference with agricultural operations and to provide ease of access for operation and maintenance. Service capacity and length of lines should be designed to prevent the conversion of agricultural lands into urban/suburban uses.

AG-1.11 Agricultural Buffers

The County shall examine the feasibility of employing agricultural buffers between agricultural and non-agricultural uses, and along the edges of UDBs and HDBs. Considering factors include the type of operation and chemicals used for spraying, building orientation, planting of trees for

screening, location of existing and future rights-of-way (roads, railroads, canals, power lines, etc.), and unique site conditions.

AG-1.13 Agricultural Related Uses

The County shall allow agriculturally-related uses, including value-added processing facilities by discretionary approvals in areas designated Valley or Foothill Agriculture, subject to the following criteria:

1. The use shall provide a needed service to the surrounding agricultural area which cannot be provided more efficiently within urban areas or which requires location in a non-urban area because of unusual site requirements or operational characteristics;
2. The use shall not be sited on productive agricultural lands if less productive land is available in the vicinity;
3. The operational or physical characteristics of the use shall not have a significant adverse impact on water resources or the use or management of surrounding agricultural properties within at least one-quarter (1/4) mile radius;
4. A probable workforce should be located nearby or be readily available; and
5. For proposed value-added agricultural processing facilities, the evaluation under criterion “1” above shall consider the service requirements of the use and the capability and capacity of cities and unincorporated communities to provide the required services.

AG-1.16 Schools in Agricultural Zones

The County shall discourage the location of new schools in areas designated for agriculture, unless the School District agrees to the construction and maintenance of all necessary infrastructure impacted by the project.

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AG-2.6 Biotechnology and Biofuels

The County shall encourage the location of industrial and research oriented businesses specializing in biotechnologies and biofuels that can enhance agricultural productivity, enhance food processing activities in the County, provide for new agriculturally-related products and markets, or otherwise enhance the agricultural sector in the County.

Housing Policy 4.13

Promote energy efficiency and water conservation.

Housing Policy 4.21

Promote energy conservation opportunities in new residential development.

ERM-1.1 Protection of Rare and Endangered Species

The County shall ensure the protection of environmentally sensitive wildlife and plant life, including those species designated as rare, threatened, and/or endangered by State and/or Federal government, through compatible land use development.

ERM-1.2 Development in Environmentally Sensitive Areas

The County shall limit or modify proposed development within areas that contain sensitive habitat for special status species and direct development into less significant habitat areas. Development in natural habitats shall be controlled so as to minimize erosion and maximize beneficial vegetative growth.

ERM-1.3 Encourage Cluster Development

When reviewing development proposals, the County shall encourage cluster development in areas with moderate to high potential for sensitive habitat.

ERM-1.4 Protect Riparian Areas

The County shall protect riparian areas through habitat preservation, designation as open space or recreational land uses, bank stabilization, and development controls.

ERM-1.5 Riparian Management Plans and Mining Reclamation Plans

The County shall require mining reclamation plans and other management plans to include measures that protect, maintain, and restore riparian resources and habitats.

ERM-1.6 Management of Wetlands

The County shall support the preservation and management of wetland and riparian plant communities for passive recreation, groundwater recharge, and wildlife habitats.

ERM-1.8 Open Space Buffers

The County shall require buffer areas between development projects and significant watercourses, riparian vegetation, wetlands, and other sensitive habitats and natural communities. These buffers should be sufficient to assure the continued existence of the waterways and riparian habitat in their natural state.

ERM-1.12 Management of Oak Woodland Communities

The County shall support the conservation and management of oak woodland communities and their habitats.

ERM-2.1 Conserve Mineral Deposits

The County will encourage the conservation of identified and/or potential mineral deposits, recognizing the need for identifying, permitting, and maintaining a 50 year supply of locally available PCC grade aggregate.

ERM-2.2 Recognize Mineral Deposits

The County will recognize as a part of the General Plan those areas of identified and/or potential mineral deposits.

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ERM-3.2 Limited Mining in Urban Areas

Within the County UDBs and HDBs, new commercial mining operations should be limited due to environmental and compatibility concerns.

ERM-3.3 Small-Scale Oil and Gas Extraction

The County shall allow by Special Use Permit small-scale oil and gas extraction activities and facilities that can be demonstrated to not have a significant adverse effect on surrounding or adjacent land and are within an established oil and gas field outside of a UDB.

ERM-3.4 Oil and Gas Extraction

Facilities related to oil and gas extraction and processing in the County may be allowed in identified oil and gas fields subject to a special use permit. The extraction shall demonstrate that it will be compatible with surrounding land uses and land use designations.

ERM-4.1 Energy Conservation and Efficiency Measures

The County shall encourage the use of solar energy, solar hot water panels, and other energy conservation and efficiency features in new construction and renovation of existing structures in accordance with State law.

ERM-4.2 Streetscape and Parking Area Improvements for Energy Conservation

The County shall promote the planting and maintenance of shade trees along streets and within parking areas of new urban development to reduce radiation heating.

ERM-7.1 Soil Conservation

The County of Tulare shall establish the proper controls and ordinances for soil conservation.

WR-1.4 Conversion of Agricultural Water Resources

For new urban development, the County shall discourage the transfer of water used for agricultural purposes (within the prior ten years) for domestic consumption except in the following circumstances:

1. The water remaining for the agricultural operation is sufficient to maintain the land as an economically viable agricultural use,
2. The reduction in infiltration from agricultural activities as a source of groundwater recharge will not significantly impact the groundwater basin.

WR-1.5 Expand Use of Reclaimed Wastewater

To augment groundwater supplies and to conserve potable water for domestic purposes, the County shall seek opportunities to expand groundwater recharge efforts

WR-1.6 Expand Use of Reclaimed Water

The County shall encourage the use of tertiary treated wastewater and household gray water for irrigation of agricultural lands, recreation and open space areas, and large landscaped areas as a means of reducing demand for groundwater resources.

WR-3.3 Adequate Water Availability

The County shall review new development proposals to ensure the intensity and timing of growth will be consistent with the availability of adequate water supplies. Projects must submit a Will-Serve letter as part of the application process, and provide evidence of adequate and sustainable water availability prior to approval of the tentative map or other urban development entitlement.

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HS-9.2 Walkable Communities

The County shall require where feasible, the development of parks, open space, sidewalks and walking and biking paths that promote physical activity and discourage automobile dependency in all future communities.

PF-1.4 Available Infrastructure

The County shall encourage urban development to locate in existing UDBs and HDBs where infrastructure is available or may be established in conjunction with development. The County shall ensure that development does not occur unless adequate infrastructure is available, that sufficient water supplies are available or can be made available, and that there are adequate provisions for long term management and maintenance of infrastructure and identified water supplies.

Open-Space Policies

LU-2.3 Open Space Character

The County shall require that all new development requiring a County discretionary approval, including parcel and subdivision maps, be planned and designed to maintain the scenic open space character of open space resources including, but not limited to, agricultural areas, rangeland, riparian areas, etc., within the view corridors of highways. New development shall utilize natural landforms and vegetation in the least visually disruptive way possible and use design, construction and maintenance techniques that minimize the visibility of structures on hilltops, hillsides, ridgelines, steep slopes, and canyons.

SL-1.3 Watercourses

The County shall protect visual access to, and the character of, Tulare County's scenic rivers, lakes, and irrigation canals by:

1. Locating and designing new development to minimize visual impacts and obstruction of views of scenic watercourses from public lands and right-of-ways, and
2. Maintaining the rural and natural character of landscape viewed from trails and watercourses used for public recreation.

ERM-5.1 Parks as Community Focal Points

The County shall strengthen the role of County parks as community focal points by providing community center/recreation buildings to new and existing parks, where feasible.

ERM-5.2 Park Amenities

The County shall provide a broad range of active and passive recreational opportunities within community parks. When possible, this should include active sports fields and facilities, community center/recreation buildings, children's play areas, multi-use areas and trails, sitting areas, and other specialized uses as appropriate.

ERM-5.3 Park Dedication Requirements

The County shall require the dedication of land and/or payment of fees, in accordance with local authority and State law (for example the Quimby Act), to ensure funding for the acquisition and development of public recreation facilities.

ERM-5.5 Collocated Facilities

The County shall encourage the development of parks near public facilities such as schools, community halls, libraries, museums, prehistoric sites, and open space areas and shall encourage joint-use agreements whenever possible.

ERM-5.6 Location and Size Criteria for Parks

Park types used in Tulare County are defined as follows:

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- **Neighborhood Play Lots (Pocket Parks).** The smallest park type, these are typically included as part of a new development to serve the neighborhood in which they are contained. Typical size is one acre or less. If a park of this type is not accessible to the general public, it cannot be counted towards the park dedication requirements of the County. Pocket Parks can be found in communities, hamlets, and other unincorporated areas.
- **Neighborhood Parks.** Neighborhood parks typically contain a tot lot and playground for 2-5 year olds and 5-12 year olds, respectively, one basketball court or two half-courts, baseball field(s), an open grassy area for informal sports activities (for example, soccer), and meandering concrete paths that contain low-level lighting for walking or jogging. In addition, neighborhood parks typically have picnic tables and a small group picnic shelter. These park types are typically in the range of 2 to 15 acres and serve an area within a ½ mile radius. Neighborhood parks can be found in communities, hamlets, and other unincorporated areas.
- **Community Parks.** Community parks are designed to serve the needs of the community as a whole. These facilities can contain the same facilities as the neighborhood park. In addition, these parks can contain sports facilities with night lighting, community centers, swimming pools, and facilities of special interest to the community. These parks are typically 15 to 40 acres in size and serve an area within a 2 mile radius. Community parks can be found in communities, planned community areas, and large hamlets.
- **Regional Parks.** Regional parks are facilities designed to address the needs of the County as a whole. These facilities may have an active recreation component (play area, group picnic area, etc.), but the majority of their area is maintained for passive recreation (such as hiking or horseback riding), and natural resource enjoyment. Regional parks are typically over 200 acres in size, but smaller facilities may be appropriate for specific sites of regional interest.

The following guidelines should be observed in creating and locating County parks:

1. The County shall strive to maintain an overall standard of five or more acres of County-owned improved parkland per 1,000 population in the unincorporated portions of the County,
2. Neighborhood play lots (pocket parks) are encouraged as part of new subdivision applications as a project amenity, but are not included in the calculation of dedication requirements for the project,
3. Neighborhood parks at three acres per 1,000 population, if adjoining an elementary school and six acres per 1,000 population if separate [ERME IV-C; Open Space; Policy 3; Pg. 101],
4. Community parks at one-acre per 1,000 population if adjoining a high school and two acres per 1,000 population if separate [ERME IV-C; Open Space; Policy 4; Pg. 101],
5. Regional parks at one-acre per 1,000 population,
6. Only public park facilities shall be counted toward Countywide parkland standards, and
7. A quarter mile walking radius is the goal for neighborhood parks.

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ERM-5.12 Meet Changing Recreational Needs

The County shall promote the continued and expanded use of national and State forests, parks, and other recreational areas to meet the recreational needs of County residents.

ERM-5.13 Funding for Recreational Areas and Facilities

The County shall support the continued maintenance and improvement of existing recreational facilities and expansion of new recreational facilities opportunities for County, State, and Federal lands. The County shall strive to obtain adequate funding to improve and maintain existing parks, as well as construct new facilities.

ERM-5.15 Open Space Preservation

The County shall preserve natural open space resources through the concentration of development in existing communities, use of cluster development techniques, maintaining large lot sizes in agricultural areas, discouraging conversion of lands currently used for agricultural production, limiting development in areas constrained by natural hazards, and encouraging agricultural and ranching interests to maintain natural habitat in open space areas where the terrain or soil is not conducive to agricultural production.

Other

PF-2.7 Improvement Standards in Communities

The County shall require development within the designated UDBs to meet an urban standard for improvements. Typical improvements shall include curbs, gutters, sidewalks, and community sewer and water systems.

ERM-6.3 Alteration of Sites with Identified Cultural Resources

When planning any development or alteration of a site with identified cultural or archaeological resources, consideration should be given to ways of protecting the resources. Development should be permitted in these areas only after a site specific investigation has been conducted pursuant to CEQA to define the extent and value of resource, and mitigation measures proposed for any impacts the development may have on the resource.

WR-3.9 Establish Critical Water Supply Areas

The County shall designate Critical Water Supply Areas to include the specific areas used by a municipality or community for its water supply system, areas critical to groundwater recharge, and other areas possessing a vital role in the management of the water resources in the County.

PFS-6.1 Telecommunications Services

The County shall work with telecommunication providers to ensure that all residents and businesses have access to telecommunications services, including broadband internet service. To maximize access to inexpensive telecommunications services, the County shall encourage marketplace competition from multiple service providers.

PFS-8.4 Library Facilities and Services

The County shall encourage expansion of library facilities and services as necessary to meet the needs (e.g., internet access, meeting rooms, etc.) of future population growth.

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PFS-8.5 Government Facilities in Community Centers

The County shall actively support development and expansion of federal, State, County, districts, and other governmental offices and facilities where infrastructure exists within community core areas.

PFS-9.1 Expansion of Gas and Electricity Facilities

The County shall coordinate with gas and electricity service providers to plan the expansion of gas and electrical facilities to meet the future needs of County residents.

GENERAL PLAN POLICIES THAT RELATE TO HEALTH

Health and Safety Element

HS-1.4 Building and Codes

Except as otherwise allowed by State law, the County shall ensure that all new buildings intended for human habitation are designed in compliance with the latest edition of the California Building Code, California Fire Code, and other adopted standards based on risk (e.g., seismic hazards, flooding), type of occupancy, and location (e.g., floodplain, fault).

HS-1.5 Hazard Awareness and Public Education

The County shall continue to promote awareness and education among residents regarding possible natural hazards, including soil conditions, earthquakes, flooding, fire hazards, and emergency procedures.

HS-1.6 Public Safety Programs

The County shall promote public safety programs, including neighborhood watch programs, child identification and fingerprinting, public awareness and prevention of fire hazards, and other public education efforts.

HS-1.7 Safe Housing and Structures

The County shall continue to seek grant funding for the rehabilitation of deteriorated and dilapidated structures and provide

available information regarding housing programs and other public services.

HS-1.9 Emergency Access

The County shall require, where feasible, road networks (public and private) to provide for safe and ready access for emergency equipment and provide alternate routes for evacuation.

HS-1.10 Emergency Services Near Assisted Living Housing

In approving new facilities, such as nursing homes, housing for the elderly and other housing for the mentally and physically infirm, to the extent possible, the County shall ensure that such facilities are located within reasonable distance of fire and law enforcement stations.

HS-4.3 Incompatible Land Uses

The County shall prevent incompatible land uses near properties that produce or store hazardous waste.

HS-4.4 Contamination Prevention

The County shall review new development proposals to protect soils, air quality, surface water, and groundwater from hazardous materials contamination.

HS-4.5 Increase Public Awareness

The County shall work to educate the public about household hazardous waste and the proper method of disposal.

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HS-4.6 Pesticide Control

The County shall monitor studies of pesticide use and the effects of pesticide on residents and wildlife and require mitigation of the effects wherever feasible and appropriate.

HS-4.8 Hazardous Materials Studies

The County shall ensure that the proponents of new development projects address hazardous materials concerns through the preparation of Phase I or Phase II hazardous materials studies for each identified site as part of the design phase for each project. Recommendations required to satisfy federal or State cleanup standards outlined in the studies will be implemented as part of the construction phase for each project.

HS-5.1 Development Compliance with Federal, State, and Local Regulations

The County shall ensure that all development within the designated floodway or floodplain zones conforms with FEMA regulations and the Tulare County Flood Damage Prevention Ordinance.

New development and divisions of land, especially residential subdivisions, shall be developed to minimize flood risk to structures, infrastructure, and ensure safe access and evacuation during flood conditions.

HS-5.2 Development in Floodplain Zones

The County shall regulate development in the 100-year floodplain zones as designated on maps prepared by FEMA in accordance with the following:

1. Critical facilities (those facilities which should be open and accessible during emergencies) shall not be permitted.
2. Passive recreational activities (those requiring non-intensive development, such as hiking, horseback riding, picnicking) are permissible.
3. New development and divisions of land, especially residential subdivisions, shall be

developed to minimize flood risk to structures, infrastructure, and ensure safe access and evacuation during flood conditions.

HS-9.1 Healthy Communities

To the maximum extent feasible, the County shall strive through its land use decisions to promote community health and safety for all neighborhoods in the County by encouraging patterns of development that are safe and influence crime prevention, promote a high-quality physical environment and encourage physical activity by means such as sidewalks and walking and biking paths that discourage automobile dependency in existing communities.

HS-9.2 Walkable Communities

The County shall require where feasible, the development of parks, open space, sidewalks and walking and biking paths that promote physical activity and discourage automobile dependency in all future communities.

Noise Policies

HS-8.5 State Noise Standards

The County shall enforce the State Noise Insulation Standards (California Administrative Code, Title 24) and Chapter 35 of the Uniform Building Code (UBC). Title 24 requires that interior noise levels not exceed 45 dB Ldn (or CNEL) with the windows and doors closed within new developments of multi-family dwellings, condominiums, hotels, or motels. Where it is not possible to reduce exterior noise levels within an acceptable range the County shall require the application of noise reduction technology to reduce interior noise levels to an acceptable level.

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HS-8.6 Noise Level Criteria

The County shall ensure noise level criteria applied to land uses other than residential or other noise-sensitive uses are consistent with the recommendations of the California Office of Noise Control (CONC).

HS-8.7 Inside Noise

The County shall ensure that in instances where the windows and doors must remain closed to achieve the required inside acoustical isolation, mechanical ventilation or air conditioning is provided.

HS-8.8 Adjacent Uses

The County shall not permit development of new industrial, commercial, or other noise-generating land uses if resulting noise levels will exceed 60 dB Ldn (or CNEL) at the boundary of areas designated and zoned for residential or other noise-sensitive uses, unless it is determined to be necessary to promote the public health, safety and welfare of the County.

HS-8.9 County Equipment

The County shall strive to purchase equipment that complies with noise level performance standards set forth in the Health and Safety Element.

HS-8.10 Automobile Noise Enforcement

The County shall encourage the CHP, Sheriff's office, and local police departments to actively enforce existing sections of the California Vehicle Code relating to adequate vehicle mufflers, modified exhaust systems, and other amplified noise.

HS-8.11 Peak Noise Generators

The County shall limit noise generating activities, such as construction, to hours of normal business operation (7 a.m. to 7 p.m.). No peak noise generating activities shall be allowed to occur outside of normal business hours without County approval.

HS-8.14 Sound Attenuation Features

The County shall require sound attenuation features such as walls, berming, heavy landscaping, between commercial, industrial, and residential uses to reduce noise and vibration impacts.

HS-8.15 Noise Buffering

The County shall require noise buffering or insulation in new development along major streets, highways, and railroad tracks.

Safety Policies

HS-1.1 Maintain Emergency Public Services

The County shall ensure that during natural catastrophes and emergency situations, the County can continue to provide essential emergency services.

HS-1.9 Emergency Access

The County shall require, where feasible, road networks (public and private) to provide for safe and ready access for emergency equipment and provide alternate routes for evacuation.

HS-1.10 Emergency Services Near Assisted Living Housing

In approving new facilities, such as nursing homes, housing for the elderly and other housing for the mentally and physically infirm, to the extent possible, the County shall ensure that such facilities are located within reasonable distance of fire and law enforcement stations.

HS-5.2 Development in Floodplain Zones

The County shall regulate development in the 100-year floodplain zones as designated on maps prepared by FEMA in accordance with the following:

1. Critical facilities (those facilities which should be open and accessible during emergencies) shall not be permitted.

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2. Passive recreational activities (those requiring non-intensive development, such as hiking, horseback riding, picnicking) are permissible.
3. New development and divisions of land, especially residential subdivisions, shall be developed to minimize flood risk to structures, infrastructure, and ensure safe access and evacuation during flood conditions.

HS-5.8 Road Location

The County shall plan and site new roads to minimize disturbances to banks and existing channels and avoid excessive cuts and accumulations of waste soil and vegetative debris near natural drainage ways.

HS-5.9 Floodplain Development Restrictions

The County shall ensure that riparian areas and drainage areas within 100-year floodplains are free from development that may adversely impact floodway capacity or characteristics of natural/riparian areas or natural groundwater recharge areas.

HS-5.10 Flood Control Design

The County shall evaluate flood control projects involving further channeling, straightening, or lining of waterways until alternative multipurpose modes of treatment, such as wider berms and landscaped levees, in combination with recreation amenities, are studied.

HS-5.11 Natural Design

The County shall encourage flood control designs that respect natural curves and vegetation of natural waterways while retaining dynamic flow and functional integrity.

HS-7.4 Upgrading for Streets and Highways

The County shall evaluate and upgrade vital streets and highways to an acceptable level for emergency services.

PFS-7.1 Fire Protection

The County shall strive to expand fire protection service in areas that experience growth in order to maintain adequate levels of service.

PFS-7.6 Provision of Station Facilities and Equipment

The County shall strive to provide sheriff and fire station facilities, equipment (engines and other apparatus), and staffing necessary to maintain the County's service goals. The County shall continue to cooperate with mutual aid providers to provide coverage throughout the County.

PFS-7.11 Locations of Fire and Sheriff Stations/Sub-stations

The County shall strive to locate fire and sheriff sub-stations in areas that ensure the minimum response times to service calls.

PFS-7.12 Design Features for Crime Prevention and Reduction

The County shall promote the use of building and site design features as means for crime prevention and reduction.

PFS-8.3 Location of School Sites

The County shall work with school districts and land developers to locate school sites consistent with current and future land uses. The County shall also encourage siting new schools near the residential areas that they serve and with access to safe pedestrian and bike routes to school.

EARLIMART COMMUNITY PLAN UPDATE

Water Resource Element

WR-2.1 Protect Water Quality

All major land use and development plans shall be evaluated as to their potential to create surface and groundwater contamination hazards from point and non-point sources. The County shall confer with other appropriate agencies, as necessary, to assure adequate water quality review to prevent soil erosion; direct discharge of potentially harmful substances; ground leaching from storage of raw materials, petroleum products, or wastes; floating debris; and runoff from the site.

WR-2.2 National Pollutant Discharge Elimination System (NPDES) Enforcement

The County shall continue to support the State in monitoring and enforcing provisions to

control non-point source water pollution contained in the U.S. EPA NPDES program as implemented by the Water Quality Control Board.

WR-2.3 Best Management Practices (BMPs)

The County shall continue to require the use of feasible BMPs and other mitigation measures designed to protect surface water and groundwater from the adverse effects of construction activities, agricultural operations requiring a County Permit and urban runoff in coordination with the Water Quality Control Board.

EARLIMART COMMUNITY PLAN UPDATE

GOALS, OBJECTIVES AND POLICIES SPECIFIC TO EARLIMART

Goals, objectives and policies are the fundamental building blocks of the planning process. Goals describe the desirable results to which the plan is committed while objectives describe the intermediate steps or achievements which must be taken to reach the goals. Policies describe more specific actions or processes which must be undertaken in order to achieve objectives.

Taken as a whole, goals, objectives and policies provide the guidelines as to how the community is to grow in terms of type, quantity and quality of development. The goals, objectives and policies are an integral part of the plan itself and the final land use map and plan description must reflect the goals, objectives and policies of the community. The plan map then is a graphic portrayal of what goals, objectives and policies are intended to accomplish.

The goals, objectives and policies of this community plan are based on those contained in the Tulare County 2030 General Plan and the 1988 Earlimart Community Plan, and input received from Earlimart citizens during the public outreach process. The goals, objectives and policies are divided into four categories: Community Development, Housing, Economic Base, and Environmental Quality.

Community Development

GOAL I: Foster a cohesive community with easy access to necessary services and support facilities

Objective: Focus urban type development and services within the Community of Earlimart with the broadest mix of uses to reduce vehicle miles traveled

Policies:

1. Encourage in-filling of vacant land and compatible development on underdeveloped land as a priority before development of agriculturally productive lands.
2. The County shall carefully coordinate the extension of water and sewer services in the Plan Area with the EPUD to promote orderly and efficient development patterns
3. Locate high density residential uses in close proximity to planned shopping areas.
4. Require public, quasi-public and high density residential uses to locate where direct access to major streets is available.
5. The County shall ensure that solid waste facility sites (for example, landfills) are protected from the encroachment by sensitive and/or incompatible land uses.
6. The County shall cooperate with all affected school districts to provide the highest quality educational services and school facilities possible.
7. The County shall work with the Earlimart School District in facilitating the location and establishment of new school sites, or expansion of existing sites, as needed.
8. The County shall work with the Earlimart school district to provide safe routes to school.
9. The County of Tulare will solicit recommendations from all interested public agencies on matters regarding the Earlimart Community Plan.
10. The Earlimart Community Plan should be reviewed every five years to determine if amendments are appropriate.

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11. When considering any land use proposal, capital expenditure or other matters of community importance, the County of Tulare will request input from the local service district and other affected agencies.

Objective: Encourage land uses adjacent to State Route 99 which are consistent with noise impacts.

Policies:

1. Encourage commercial and/or industrial development to locate adjacent to State Route 99 where access is appropriate for such development.
2. Require installation of walls, berms or heavy planting along State Route 99 in conjunction with any new residential development.
3. Discourage new residential development on vacant lots within areas proposed for commercial and industrial development.
4. Encourage the eventual conversion of existing residential uses within areas proposed for commercial and industrial development to nonresidential uses; excepting living quarters used in conjunction with a business.
5. Phase-out existing non-conforming commercial and industrial uses within planned residential areas by zoning such areas residential, and by enforcement of local zoning regulations pertaining to illegal buildings and uses.
6. Tulare County shall designate and zone sufficient amounts of land to accommodate existing and projected industrial, commercial, residential, and public (e.g., parks and recreational) needs of the community.
7. Provide for appropriate buffers between areas set aside for commercial activities and single family residential uses.

GOAL II: Achieve development densities consistent with levels of available service.

Objective: Encourage merger of existing vacant substandard lots within the townsite of Earlimart.

Policies:

1. Conduct a study of the Earlimart townsite area to determine the impact of a comprehensive vacant lot merger action and undertake such a merger, if feasible.

GOAL III: Coordinate Community Development Decisions with the Earlimart Public Utility District.

Objective: Ensure that all development can or cannot be served by the Earlimart Public Utility District (EPUD) during the planning period.

Policies:

1. Coordinate zoning with availability of utilities and community services.
2. Promote commercial and industrial development with wastewater discharge characteristics which can be accommodated by the EPUD.
3. Encourage industries with excessive effluent to pre-treat wastewater prior to disposal to the EPUD wastewater system.
4. Encourage coordination between developers and the EPUD throughout the application and development process to prevent time delays and to assure that the EPUD can accommodate the needs of any proposed development.
5. Before the issuance of any land use permit, the Tulare County Resource Management Agency must receive confirmation from the EPUD that water and sewer service requirements can be accommodated or that they cannot.

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6. Assist the EPUD in applications for grant funds to carry out their capital improvement program for providing, maintaining and improving their sewer and water systems to serve new and existing developments which implement the goals and objectives of this Plan and of the Tulare County General Plan.
7. Prohibit to the extent allowed by law all development from holding, diverting and/or disposing of storm water run-off at locations, or in such a manner, as to cause groundwater recharge contributable to raising the groundwater to an unsafe level in the vicinity of the EPUD wastewater treatment facilities.
8. Investigate the necessity of preparing a drainage plan, within five years of adoption of the Community Plan, for diverting and disposing of storm water runoff and excess irrigation water at a location, or locations, where the retention or disposition of such water will not contribute to raising the groundwater level in the vicinity of the EPUD wastewater treatment facilities.
9. Before the issuance of any land use permit, the Tulare County Resource Management Agency will require all project applications for new development or redevelopment to include storm water disposal plans in accordance with the recommendations of the Tulare County RMA and Caltrans to prevent runoff flows into the State highway right-of-way.

Housing

GOAL I: Provide safer and adequate housing for all citizens within the community.

Objective: Reduce deficiencies in existing housing stock.

Policies:

1. Apply the health, safety and welfare standards of the Tulare County Ordinance Code, which may require demolition of vacant substandard housing units.
2. Encourage relocation of families from substandard housing units by expanding affordable housing opportunities within the community.
3. Inform potential rehabilitators of substandard housing that incentives such as reduced building permit fees are available.

Objective: Encourage new housing construction within the community to meet the needs of low and moderate income residents.

Policies:

1. Enable the housing industry to proceed with construction in a timely and cost-efficient fashion by providing adequate amounts of residential zoning.
2. Assure that the housing industry is made aware of residential development potentials in Earlimart.
3. Coordinate residential zoning with availability of utilities and community services.
4. Provide adequate amounts of residential zoning to encourage the housing industry to proceed with construction of residential development in a timely and cost-efficient fashion.

Objective: Provide a role for mobile homes in satisfying community housing needs.

Policies:

1. Allow for development of mobilehome parks in appropriate locations.
2. Permit mobilehomes to be installed on residentially designated lots within the original townsite; however, mobilehomes shall not be allowed to occupy more than 25% of such lots.

EARLIMART COMMUNITY PLAN UPDATE

3. Discourage mobilehomes on individual lots outside the original townsite, except when necessary for caretaker use in conjunction with commercial and industrial activities.
4. Require skirting or some other type of architectural screening to improve mobilehome appearance and safety.
5. Provide a role for mobilehomes and travel trailers in satisfying the seasonal housing needs of migrant populations.

Economic Base

GOAL I: Develop a strong and diversified economy.

Objective: Provide sufficient land for industrial and commercial development to meet the needs of the community and region and strengthen and maintain a viable community economy.

Policies:

1. Promote a concentration of industrial and commercial activities within selected areas to allow for cost efficient provision of necessary services and to protect residential neighborhoods.
2. Zone an area for a community shopping center in the northeastern portion of the community to meet local consumer needs.
3. Reserve areas with convenient highway access for highway-oriented commercial development, thereby encouraging outside cash flow into the community.

Objective: Provide the services necessary to support new industrial and commercial development.

Policies:

1. Encourage the Earlimart Public Utility District to give priority to community service development in the areas reserved for commercial and industrial growth on the plan.
2. Place emphasis on development and upgrading of water supply facilities to meet fire protection standards in planned commercial and industrial areas.

Objective: Provide the necessary safe guards to attract quality industrial and commercial development to the community.

Policies:

1. Assure that commercial and industrial developments are designed so that traffic will not impact upon residential areas.
2. Develop standards for signs, landscaping, and fencing to improve the attractiveness of industrial and commercial areas.

Environmental Quality and Public Safety

GOAL I: Preserve and enhance the quality of life for present and future generation of Earlimart citizens.

Objective: Upgrade the level of community health, sanitation and safety.

Policies:

1. Encourage capital improvements (curbs, gutters, streets paving, lighting, etc.) within existing developed areas which will upgrade the community image and improve safety.
2. Tulare County shall, within its authority, protect the public from danger to life and property caused by fire.
3. Tulare County shall, within its authority, protect the public against crime against people and property.

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Objective: Provide sufficient open space for community recreation needs.

Policies:

1. Encourage reservation of open space for recreational purposes in conjunction with future residential developments.
2. Facilitate innovation in housing and subdivision design so that private recreation and open space areas can be accommodated.
3. Assist in the development of a community park in conjunction with future site of Earlimart Schools.

Objective: Protect Agricultural Lands:

1. Land within the respective Urban Development Boundary of Earlimart, which is designated as residential reserve, commercial reserve, or industrial reserve shall be retained in agricultural use until such time as conversion to urban use (as defined in the Tulare County General Plan) is appropriate. When a rezoning occurs without a general plan amendment, the reserve designation shall be removed from the parcel.
2. The following criteria shall be used to determine when conversion to urban use is appropriate:
 - a. The property is not subject to an agricultural preserve contract;
 - b. Full urban services, schools, and infrastructure sufficient to serve urban development either are available or can be made available; and
 - c. At least 30 % the property boundaries are contiguous on at least one side to existing urban development.
3. Until productive agricultural lands are ready to be developed they shall be retained in parcels of sufficient size to allow agricultural uses.

4. Agricultural uses outside the UDB shall be protected from conflicting urban uses by aligning the UDB along streets, canals or other man-made or natural features in order to buffer the two uses to the extent possible.
5. The County (and developers) shall carefully coordinate the extension of public water and sewer services in the planning area with Earlimart Community Service District, to promote logical and orderly development patterns.
6. New agricultural preserves and contracts shall not be approved for properties within Earlimart's UDB.
7. Commercial and residential uses will be required to connect to public services provided by the Earlimart Public Utility District.
8. Large lot agricultural zoning such as AE-20 shall be applied as a holding zone to properties which do not meet the criteria set forth in policy Agriculture Policy 2 above.
9. Promote growth along SR 99 for industrial and commercial uses to preserve adjacent agricultural lands consistent with the Corridor Framework Policies in the Tulare County General Plan.

Objective: Prohibit to the extent allowed by law activities that will have a significant adverse effect on the environmental quality of Earlimart.

Policies:

1. Prohibit to the extent allowed by law residential development in excess of seven families per acre, until a sewage collection system is constructed.
2. Require a sufficient lot area for all new residential development to ensure an adequate area for on-site sewage disposal until a sewage collection system is constructed.

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3. Prohibit to the extent allowed by law new intensive animal raising operations within the "windshed" area of Earlimart.
4. Carefully evaluate proposed heavy industrial uses to be located east of SR 99 to assure that such uses will not have an adverse impact on the community.

EARLIMART IMPLEMENTING POLICIES

Housing Policies

- 1.1 The County will strive to ensure that an adequate number of housing units are constructed to meet the housing needs Earlimart.
- 1.2 The County will strive to ensure that housing is affordable to all economic segments of Earlimart.
- 1.3 The County will ensure that there are adequate sites and will work with the EPUD and other agencies to ensure that there are adequate public facilities to support future housing needs in Earlimart.
- 1.4 The County will work diligently towards the rehabilitation of the Earlimart housing stock.
- 1.5 Mobile home parks and subdivisions, trailer parks and multifamily development shall be encouraged in Earlimart in order to provide affordable housing opportunities.
- 1.6 The County will attempt to maintain a balance between owner and renter housing stock in Earlimart.
- 1.7 Sites for multi-family development shall be identified which do not overburden any one area of the community or neighborhood. Large developments shall be located on collector streets.
- 1.8 Single family development shall be located in areas free of excessive noise and traffic, adequately buffered from incompatible land uses, and serviceable with sewer and water.

- 1.9 The Tulare County Planning and Development Department will actively enforce all planning and zoning laws in order to abate illegal, non-conforming non-residential uses.

Implementation

- 1.11 The County will periodically review zoning in Earlimart and will maintain enough residentially zoned land so that the lack of adequately zoned residential sites does not become a constraint to housing production.
- 1.21 The County will work with Self-Help Enterprises, the Tulare County Housing Authority, and other entities interested in constructing or replacing low income housing in Earlimart.
- 1.32 The County will investigate or be aware of service needs, police and fire protection, and other services required to support new residential development. In addition, it will coordinate with the EPUD and the Earlimart School District when they expand or enlarge their services to support future population growth.
- 1.41 The County will encourage and assist non-profit and for-profit groups who demonstrate the ability and skill to undertake rehabilitation programs, to apply for State and federal funds for rehabilitation programs.

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Commerce Policies

- 2.1 Tulare County shall designate and zone enough land for commercial uses to meet the existing and future needs of the area's population.
- 2.2 All new commercial uses shall be designed in such a manner that will minimize land use compatibility problems with surrounding and planned land uses.
- 2.3 The County shall work towards marketing Earlimart's highway commercial sites by improving signage, access and infrastructure services.
- 2.4 New commercial uses should be located away from existing or planned residential areas or mitigation measures should be incorporated into the design of the project that will eliminate any undesirable conditions.

Implementation

- 2.11 The County will designate and zone an area north of Sierra Avenue near the highway and a site on Washington Street west of the railroad for general commercial uses. The area shall be of sufficient size to permit the construction of a shopping center which can serve the commercial needs of Earlimart residents.
- 2.12 The County will designate and zone areas east of SR 99 on the south side of Earlimart and west of SR 99 on the north side of the community for highway commercial uses.
- 2.13 The County will designate and zone areas between SR 99 and Front Road for service commercial uses. These areas are located in areas which are inappropriate for residential development due to noise, traffic and appearance.

Industry Policies

- 1.1 The County shall encourage industrialization of Earlimart, especially to industries that provide non-seasonal employment.
- 1.2 Land well suited for industrial development because of access, availability of infrastructure and proximity to similar land uses should be designated for industry and protected from the encroachment of incompatible uses.
- 1.3 When the County reviews permit applications for prospective industries that wish to locate in Earlimart several criteria should be applied which include:
 - The water and sewer requirements of new industry should be considered with respect to the limitations of EPUD's water and sewer systems.
 - Adequate access must be provided so local streets do not operate beyond a "D" level of service rating.
 - Industry shall be generally downwind of residential dwellings.
 - Industry which requires lift stations should work with the County and the EPUD to seek ways to finance the installation, maintenance and replacement of these facilities.
 - Industry shall be encouraged to pretreat effluent which is difficult to process prior to disposal at the treatment plant.

Implementation

- 3.11 The County should encourage the Tulare County Economic Development Corporation (EDC) to seek out industries which require a rural community with rail and SR 99 access and to locate within the Earlimart Industrial Park.

EARLIMART COMMUNITY PLAN UPDATE

Flooding Policies

- 4.0 New development shall be protected from flooding resulting from overflow from the White River and Deer Creek.
- 4.2 The Tulare County Flood Control District shall work with irrigation districts, CALTRANS, and other responsible agencies to reduce flooding potential in Earlimart.

Implementation

- 4.11 The ground floor level of new buildings and accessory structures shall be higher than the flood elevations noted on the National Flood Insurance Rate Map for this portion of the County.

Agriculture Policies

- 5.1 Properties within the UDB of Earlimart which meet the following criteria shall be classified in an agricultural holding zone prior to conversion to urban use (as defined in the Urban Boundaries Element of the General Plan):
 - (1) The property is subject to an agricultural preserve contract;
 - (2) full urban services and infrastructure sufficient to serve urban development either are not available or cannot be made available;
 - (3) the property is not contiguous on at least one side to existing urban development.
- 5.1 Until productive agricultural lands are ready to be developed they shall be retained in parcels of sufficient size to allow agricultural uses.
- 5.3 Agricultural uses outside the UDB shall be protected from conflicting urban uses by aligning the UDB along streets, railroads or other man-made or natural features in order to buffer the two uses.

- 5.4 The county shall carefully coordinate the extension of public water and sewer services in the planning area with the EPUD, to promote logical and orderly development patterns.

- 5.5 New agricultural preserves shall not be approved for properties within the UDB of Earlimart.

Implementation

- 5.11 Large lot agricultural zoning as AE-40 shall be applied as a holding zone to properties which meet the criteria set forth in policy 5.1 above.
- 5.51 The county should consider filing a notice of nonrenewal on lands in an agricultural preserve which are designated for an urban use under the Earlimart Community Plan.

Community Involvement

- 6.1 The County of Tulare will solicit recommendations from the Earlimart Town Council, the Earlimart School and Public Utility Districts and other interested public agencies on matters regarding the Earlimart Community Plan.
- 6.2 The Earlimart Community Plan will be reviewed every five years to determine if amendments are appropriate.

Implementation

- 6.11 When considering any land use proposal, capital expenditure or other matters of community importance, the County of Tulare will request input from the Earlimart School and Public Utility Districts, and the Earlimart Town Council.

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School Policies

- 7.1 The County shall cooperate with the Earlimart School District to provide the highest quality educational services and school facilities possible.
- 7.2 Any new site selected shall conform to the following locational criteria: Located near residential development.
- Located along an existing or future street system that will provide easy access.
 - Located away from land uses that generate high noise levels or that create excess dust.
 - Located away from or buffered from areas in which agricultural sprays are routinely applied.

Implementation

- 1.0 The Land Use Map of the Earlimart Community Plan will designate the general area where a new school site on the western side of the community should be located.
- 7.12 Investigate multiple use of the school facilities such as day care, adult school and community center.

Park Policies

- 8.1 The County, in conjunction with the EPUD and Earlimart School District, shall investigate the funding sources to provide sufficient park facilities to fulfill the recreational needs of Earlimart residents.
- 8.2 In conjunction with the new school sites, a new park should be considered for construction.

Implementation

- 8.11 The County, EPUD and the Earlimart School District should meet to determine how each agency could cooperate to meet the recreational needs of the community. The County Building Services and Parks Department shall

report the findings of this meeting to the County Board of Supervisors.

Fire Protection Policies

- 9.1 Tulare County shall, within its authority, protect the public from danger to life and property caused by fire.

Implementation

- 9.11 The Tulare County Fire Warden, through the countywide fire study, shall assess the requirements for future fire protection services in Earlimart. The study shall include recommendations regarding staffing, equipment and need for an additional fire station. If a new station is needed, it should be constructed west of the Southern Pacific Railroad tracks.

Sewer and Water Facility Policies

- 10.1 The extension of water and sewer facilities into the planning area shall implement the policies of this Plan and the goals and policies of the Tulare County General Plan.
- 10.2 Developers of land served by new lift stations shall form an assessment district, or devise other means to finance the long-term costs of operation and maintenance of these stations.
- 10.3 The County should encourage industry to locate in Earlimart which is clean and does not generate effluent which is difficult and expensive for the EPUD to treat.

Implementation

- 10.31 The discharge and collection requirements of all new development in Earlimart shall be reviewed by the EPUD. Recommendations and comments on new development shall be forwarded to the County by the EPUD prior to final project approval.

EARLIMART COMMUNITY PLAN UPDATE

Circulation Policies

- 11.1 Within the planning area, the County shall provide a transportation system that provides safe and efficient service for the travel needs of all persons, the movement of goods and as a means to implement the other policies and implementation measures of this Plan.
- 11.2 The Circulation Plan shall identify a functional street classification system consistent with the Tulare County Circulation Element.
- 11.3 The County shall explore with CALTRANS the feasibility of constructing a new SR 99 off-ramp on the southern end of Earlimart.
- 11.4 The County shall require applicants for new development projects to construct or upgrade the roads which will serve their projects to County standards.
- 11.5 The Tulare County RMA shall determine the feasibility of the realignment and construction of Valente Road and the construction of Armstrong Avenue at the Union Pacific Railroad. These improvements are meant to enhance industrialization and improve access to SR 99.
- 11.6 The Tulare County RMA shall establish engineering design standards for the streets identified in the Plan.

Implementation

- 11.61 Improvement standards for collector streets shall include perpendicular curbs, gutters, and sidewalks, when the collector street serves as a major pedestrian route for school children.
- 11.62 Improvement standards for local and minor streets shall include perpendicular curbs, gutters, and street lights.

Noise Policies

- 12.0 The County shall protect noise sensitive land uses from existing or future noise generators by locating them within compatible noise environments or by requiring noise mitigation measures as provided in the Noise Element of the Tulare County General Plan.
- 12.2 The Tulare County RMA shall request the Transportation Planning Agency to construct sound walls along both sides of SR 99 in Earlimart in order to reduce noise levels in neighborhoods adjoining the highway as long as it does not impact construction funds.

Implementation

- 12.12 Through the project review process, the County will insure that new development is located and designed in a manner consistent with the Tulare County Noise Element.

EARLIMART COMMUNITY PLAN UPDATE

DEMOGRAPHICS

Historic Population Growth

From 1960 to 1980 Earlimart's population grew from 2,897 to 4,578 persons, an average annual growth rate of 2.9 percent. This exceeded the county's growth rate of 2.25 percent over that same period of time. Since 1980, 166 single family residential and 40 multiple family units have been constructed in Earlimart. This residential development encourages immigration, and when coupled with the community's natural growth, at the least, should maintain the historical growth rate of 2.9 percent. Based on this growth rate, it is estimated that Earlimart's 1987 population was 5,600.

Recent Population Growth

The population in Earlimart increased from 6,583 in 2000 to 8,537 in 2010. Earlimart continues to have more men than women. The female population increased from 3,710 in 2000 to 4,158 in 2010. The male population increased from 3,413 in 2000 to 4,379 in 2010 (see Table 24).

Table 24 - Population (2000 & 2010)						
Geography	2000			2010		
	Total Population (2000)	Male (2000)	Female (2000)	Total Population (2010)	Male (2010)	Female (2010)
California	33,871,648	16,874,892	16,996,756	37,253,956	18,517,830	18,736,126
Tulare County	368,021	184,010	184,011	442,179	221,442	220,737
Earlimart CDP	6,583	3,413	3,710	8,537	4,379	4,158
Earlimart Percentage	-	51.85 %	48.15%	-	51.29%	48.17%
Source: California Department of Finance						

Projected Population

"The San Joaquin Valley faces major challenges. One concerns how to handle future growth. Population in the Valley is expected to nearly triple by 2050, from 3.6 million to 9.4 million people, the equivalent of adding 11 new towns the size of Fresno to the area. Tulare County is expected to grow to over 1,000,000 residents by 2050, well over doubling its current population."⁵¹

Growth Rate

As noted in the 2010 General Plan Background Report, the unincorporated areas of Tulare County have a 1.3% projected annual growth rate from 2007 to 2030. This 1.3% annual growth rate can be applied to Earlimart (see Table 25).

Table 25 - Projected Annual Growth Rate		
	Historic Growth Rates 1990-2007	Projected Growth Rates 2007-2030
County Total	1.9%	2.4%
Incorporated	2.8%	2.9%
Unincorporated	0.46%	1.3%

Source: DOF, 2007; TCAG, 2008, 2010 General Plan Background Report

⁵¹ Tulare County Regional Blueprint, page 7

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Table 26 provides the population projections for the Earlimart community based on an annual average growth rate of 1.3% consistent with the Tulare County General Plan.

Table 26 - Earlimart Population Projections					
Year	2010	2015	2020	2025	2030
Population	8,537	9,107	9,714	10,362	11,053
Note: Projections based on the Tulare County General Plan 1.3% annual growth rate.					

Median Age

The median age in Earlimart has increased between 2000 and 2010. The median age of Earlimart in 2000 was 22.0 which was younger than Tulare County (29.2) and the State of California (33.3). In 2010, the median age of Earlimart was 23.5 which was still younger than Tulare County (29.6) and the State of California (35.2) (see Table 27).

Table 27 - Median Age (2000 & 2010)		
Geography	2000	2010
	Median age (years)	Median age (years)
California	33.3	35.2
Tulare County	29.2	29.6
Earlimart CDP	22.0	23.5

Source: California Department of Finance

Earlimart has a higher percentage of persons under 18 at 40.0% than Tulare County (32.6%) and the State of California (25.0%). Earlimart also has a lower elderly population. Persons 55 years old and over made up 12.5% of Earlimart's population. Comparatively, persons 55 years and older in Tulare County was 18.5% and in the State of California was 22.2% (see Table 28).

Table 28 - Age Percentage (2010)						
Geography	Persons Under 5 years	Persons Under 18 years	Persons Age 21+	Persons Age 55+	Persons Age 60+	Persons Age 65+
California	6.8%	25.0%	70.4%	22.2%	16.3%	11.4%
Tulare County	9.3%	32.6%	62.5%	18.5%	13.6%	9.4%
Earlimart CDP	11.3%	40.0%	54.3%	12.5%	8.8%	6.0%
Source: California Department of Finance						

Ethnicity and Race

In 2010, approximately 19.3% of Earlimart residents were classified as White, 0.8% Black/African American, 8.1% Asian, 1.3% American Indian/Alaska Native, and 5.2% as two or more races. Approximately 87.5% of the residents were classified as Hispanic/Latino (any race) (See Table 29).

In 2015, approximately 37.4% of Earlimart residents were classified as White, 0.8% Black/African American, 0.5% American Indian/Alaska Native, 6.3% Asian, and 4.6% as two or more races. Approximately 91.4% of the residents were classified as Hispanic (See Table 30).

As demonstrated by Table 29 and Table 30, in the 5-year period between 2010 and 2015, the proportion of the White population increased by 46.0%, the Black/African American population increased by 0.1%, and the Native Hawaiian/Pacific Islander population increased by 0.3%. The

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proportion of the American Indian/Alaska Native population decreased by 0.5% (down to none within the community), the Asian population percentage decreased by 0.4%, and the population of two or more races decreased by 4.2%. The proportion of the Hispanic/Latino (any race) population remained the same.

Table 29 - Race & Ethnicity (2010)								
Geography	Total Population	White	Black or African American	American Indian and Alaska Native	Asian	Native Hawaiian or Pacific Islander	Total Population of Two or More Races	Hispanic or Latino (of any race)
California	37,253,956	21,453,934	2,299,072	362,801	4,861,007	144,386	1,815,384	14,013,719
Tulare County	442,179	265,618	7,196	6,993	15,176	509	18,424	268,065
Earlimart CDP	8,537	3,193	67	45	536	0	393	7,805
Earlimart Percentage	-	37.4%	0.8%	0.5%	6.3%	0.0%	4.6%	91.4%
Source: 2010 American Community Survey								

Table 30 Race & Ethnicity (2015)								
Geography	Total Population	White	Black or African American	American Indian and Alaska Native	Asian	Native Hawaiian or Pacific Islander	Total Population of Two or More Races	Hispanic or Latino (of any race)
California	38,421,464	23,747,013	2,265,387	287,028	5,261,978	150,370	1,734,897	14,750,686
Tulare County	454,033	367,167	7,846	6,089	15,427	615	15,547	283,533
Earlimart CDP	8,709	7,269	79	0	514	30	31	7,964
Earlimart Percentage	-	83.4%	0.9%	0.0%	5.9%	0.3%	0.4%	91.4%
Source: 2015 American Community Survey								

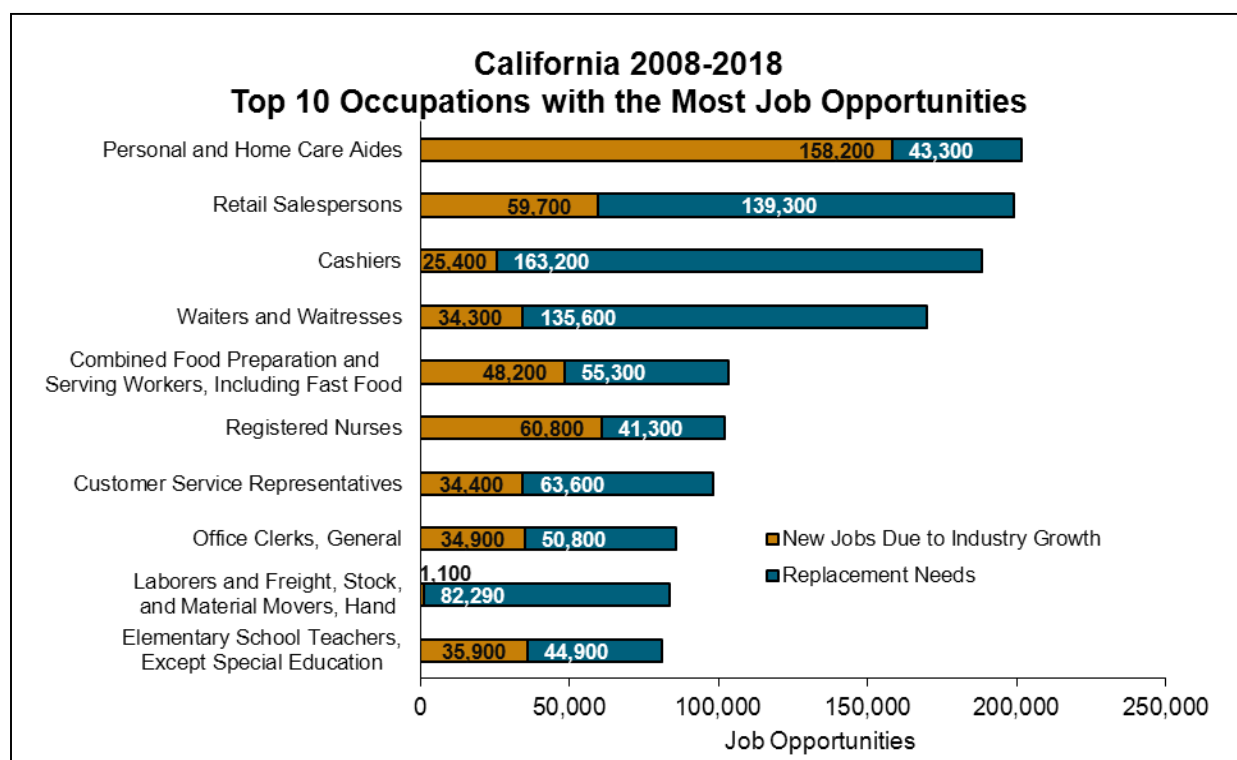
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ECONOMIC CONDITIONS

Employment Projections California

“By the end of the 2008-2018 projection period, total nonfarm employment in California is projected to grow to nearly 16.5 million jobs. This exceeds peak job level of just over 15.2 million jobs reached before the Great Recession by over 1.2 million jobs. From June 2007 to June 2009, 1.1 million jobs were lost (not seasonally adjusted). Over the 2008-to-2018 projections period, nonfarm employment is expected to rebound by 1,511,100 jobs as the economy recovers from these recessionary job losses. More than 50 percent of all projected nonfarm job growth is in education services (private), health care, and social assistance, and professional and business services. The largest number of new jobs is expected in education services, health care, and social assistance, with a gain of more than 421,000 jobs.

Factors fueling the economic recovery in California include the state’s population growth and a rise in foreign imports and exports. The state’s population increased by more than 3.3 million from 2000 to 2010 and the California Department of Finance projects the population will increase by another 4.3 million from 2010 to 2020. A steady increase in foreign imports and exports has strengthened the wholesale, retail, and transportation industry sectors.”⁵²



Source: California Employment Development Dept., California Labor and Market and Economic Analysis, 2012

⁵² California Labor and Market and Economic Analysis, 2012, page 27

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Tulare County's Local Economy

“Similar to the broader Central Valley area, Tulare County’s economy has been largely based on agriculture, food processing, and manufacturing, while professional services jobs have been limited. Tulare is the second most productive agricultural county in a State that itself is by far the most productive in the nation. The 2016 Tulare County Annual Crop and Livestock Report stated “Tulare County’s total gross production value for 2015 as \$6,980,977,800, representing a decrease of \$1,103,694,600, or 13.7% below 2014’s value of \$8,084,672,400.”⁵³ The County considered one of the largest milk producers in the United States.

Tulare County is also a major distribution hub because of its central location in the State, 200 miles north of Los Angeles and 225 miles south of San Francisco. The County’s employment base has been significantly impacted by the recent downturn with unemployment increasing to 18.3 percent in January 2010, significantly above the historic range of between 8.5 and 18.2 since 1990. In 2008, the median household income was approximately \$44,000.

The County’s major employers are Tulare County Government, Porterville Development Center, (2) Kaweah Delta Healthcare, and Ruiz Food Products. The top 20 employers combine for about 19,300 jobs, or 11 percent of the overall county employment. The major distributors include Jo-Ann Fabrics, VF Distribution, Wal-Mart, and Best Buy Electronics that combine for nearly 3.5 million square feet of distribution space. The county’s overall industrial market includes about 23 million square feet of building space.”⁵⁴

Earlimart's Economy

According to the Tulare County 2030 General Plan Update, the County’s economy has historically been driven by agriculture and has had one of the largest agricultural outputs of any county in the US. Nearly 20% of the employment in Earlimart is agriculture related, according to the Tulare County Housing Element. Despite this, the Tulare County unemployment rate has remained higher than the State average, which can be largely attributed to the seasonal nature of agricultural production.

One of the hardest factors to contend with is the lack of infrastructure. Although the County has documented the decline in available water and sewer infrastructure below in the Constraints Section the effects of it are still not totally knowable. However, there is some cursory evidence that the results of lack of water and sewer infrastructure have led to a decline in overall population in Earlimart.

As of May, 2017, the Earlimart Elementary School District Board was conducting informal surveys (phone calls to parents whose children had left), as to why there was a decline in student enrollment (-60 students year over year from 2016 to 2017) when prior to that there had been indicators of near zero growth over the previous two years (-1 and +2 students in 2015 and 2016). **(See Table 31).** In 2013 and 2014, there had been upwards growth of +16 and +72 students during 2011-12. Overall enrollment has not been this low since 2017.

The School Boards findings for the rationale behind the student population declines found in this study included: (1) Lack of housing: Rentals and Homes for Purchase (caused by lack of

⁵³ 2015 Tulare County annual Crop and Livestock Report

⁵⁴ Visalia General Plan Update: Existing Conditions Report, page 3-16

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infrastructure), (2) Political climate surrounding immigration, and (3) Work. The results are that most families moved to surrounding areas, approximately 10% moved out of the U. S. and one moved out of state. The loss to the School District would be \$607,560 in ADA (\$10,126 ADA per student year).

Table 31 - Earlimart Student Enrollment

Year	Enrollment	Change
2003	1,828	
2004	1,903	+ 75
2005	1,938	+ 35
2006	1,940	+ 2
2007	2,017	+ 77
2008	2,019	+ 2
2009	2,034	+ 15
2010	2,033	- 1
2011	1,989	- 24
2012	1,865	-124
2013	1,881	+ 16
2014	1,953	+ 72
2015	1,952	- 1
2016	1,954	+ 2
2017	1,896	- 60

Greatest enrollment period: 2009 & 2010 (2,034).

- Greatest Loss: 2011 & 2012 (-148 students).
- Lowest enrollment period: 2003 (1,828).
- Greatest difference between lowest and greatest enrollment: 206 students.
- Fifty-five (55) students were lost from the end of February to the end of March.
- Enrollment has stabilized with an overall increase from the end of March to the end of April by +3. (Source Earlimart School District Board of Trustees May 2, 1017)

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Employment in Earlimart

According to the 2013 US Census, Earlimart had 40 businesses in 2013. The size of these businesses ranged from 1 to 99 employees (see **Table 32**).

Table 32 - Number of Establishments in Earlimart by Employment –size class						
Total	Total	1-4 Employees	5-9 Employees	10-19 Employees	20-49 Employees	50-99 Employees
Total for all sectors	40	28	3	7	1	1
Manufacturing	2	1	0	0	0	1
Wholesale Trade	2	0	1	1	0	0
Retail Trade	15	11	3	0	0	0
Transportation and Warehousing	4	3	0	0	1	0
Real Estate, Rental and Leasing	4	3	1	0	0	0
Professional, Scientific, and Technical Services	1	1	0	0	0	0
Educational Services	1	1	0	0	0	0
Health Care and Social Assistance	3	2	0	1	0	0
Accommodation and Food Services	5	3	0	2	0	0
Other Services (except Public Administration)	3	3	0	0	0	0
Source: 2011 County Business Patterns, US Census, 2017						

Unemployment in Tulare County

According to the California Department of Finance, the 2011-2015 American Community indicated that the unemployment rate in Earlimart had an unemployment rate of 21.7% while Tulare County's unemployment rate was 13.5%. The State of California's unemployment rate was 10.1% (see **Table 33**).

Table 33 – 2011-2015 American Community Survey: Unemployment			
	Population	Total Civilian labor force	Unemployment Rate Percent
California	28,796,402	18,472,288	10.1
Tulare County	309,977	193,537	13.5
Earlimart CDP	1,904	1,153	26.2
Source: California Department of Finance, 2011-2015 American Community Survey 5-year Estimates			

Income

Mean and Median income in Earlimart is very low compared to Tulare County and the State of California. Earlimart's median household income was \$23,803, compared to \$42,031 for Tulare County and \$61,818 for the State of California. Earlimart's mean family income was \$ 32,644, compared to \$58,678 for Tulare County and \$87,877 for the State of California. Earlimart's per capita income was also low at \$7,959 compared to \$17,876 for Tulare County and \$30,318 for the State of California (see **Table 34**).

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Table 34 - 2011-2015 American Community Survey: Income					
Geography	Median household income (dollars)	Mean household income (dollars)	Median family income (dollars)	Mean family income (dollars)	Per capita income (dollars)
California	\$61,818	\$87,877	\$70,720	\$97,665	\$30,318
Tulare County	\$42,031	\$58,678	\$44,814	\$62,138	\$17,876
Earlimart CDP	\$23,803	\$32,644	\$22,711	\$32,260	\$7,959

Source: California Department of Finance, 2011-2015 American Community Survey 5-year Estimates

Poverty

According to the California Department of Finance, the 2011-2015 American Community Survey indicated that an estimated 54.7% of families in Earlimart lived below the poverty line. Earlimart has a higher level of poverty overall with 53.1% of the total population living in poverty compared to Tulare County at 28.1% and the State of California at 16.3%. Female-headed households with no husband present have even higher rates of poverty with 64.5% of those in Earlimart living below the poverty line (see Table 35).

Table 35 - 2011-2015 American Community Survey: Poverty					
Geography	All families	Married couple families	Families with female householder, no husband present	All people	Persons under 18 years
California	12.2%	7.3%	27.9%	16.3%	22.5%
Tulare County	23.2%	15.9%	41.6%	28.1%	37.9%
Earlimart CDP	54.7%	47.4%	64.5%	53.1%	66.7%

Source: California Department of Finance, 2011-2015 American Community Survey 5-year Estimates

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HOUSING CHARACTERISTICS

The greatest concentration of residential development in Earlimart occurs east of SR 99. The residential area west of SR 99 and east of Front Road is interspersed between industrial and commercial businesses. Residents in this area deal with highway noise as well as noise and traffic from area businesses.

Housing Characteristics

According to the 1980 Census there were 1,229 occupied housing units in Earlimart. Seventy-five percent, or 924 units, were one family dwellings; 17 percent, or 208 units, were multiple family units; and 8 percent, or 90 units, were mobile homes. These percentages are almost identical to the figures for the county for the same types of housing. Between 1980 and 1987, 166 single family and 40 multiple family residential units have been constructed.

In 1987, the number of owner-occupied and renter-occupied units in Earlimart was 844 (59 percent) and 586 (41 percent) units, respectively. When compared to the county percentages, 63 versus 37 percent, it is apparent that Earlimart was weighted towards rental units. Possibly, the second and third housing units placed on some of the single family lots are there to provide the owners with rental income rather than to house extended families.

Housing Units

During the decade between 2000 and 2010, the number of housing units in Earlimart increased from 1,603 to 2,023, which represents an increase of 26.2%. This increase was higher than the percent increase in Tulare County of 18.4% and the State of California at 12.0% (see Table 36).

Table 36 - Housing Units					
	2000	2010	2015	Percent Increase (2000-2010)	Percent Increase (2010-2015)
	Total housing units	Total housing units	Total housing units		
California	12,214,549	13,680,081	13,845,790	12.0%	1.2%
Tulare County	119,639	141,696	144,792	18.4%	2.2%
Earlimart CDP	1,603	2,023	2,001	26.2%	-1.1%
Source: California Department of Finance					

Housing Types

According to the California Department of Finance, the 2011-2015 American Community Survey data indicated that the majority (approximately 80.8%) of housing in Earlimart consists of single-unit dwellings (76.5% detached units and 4.3% attached units). Approximately 12.4% of housing consists of multi-family dwellings with less than ten units (0.8% 2-units, 7.4% 3 or 4 units, and 4.2% 5 to 9 units), while approximately 3.0% of housing consists of multi-family dwellings with 20 or more units. The remaining housing units (3.6%) are mobile homes. The percentage of single-unit detached homes in Earlimart are similar to those of Tulare County (76.0%), but is higher than California (58.1%). The percentage of multi-family dwelling (15.4% of two or more units) in Earlimart are also similar to that of Tulare County (14.3%), but is about half of that of the State of California (31.2%). The percentage

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of mobile homes in Earlimart is similar to that of California (3.8%), but is about half of that in Tulare County (7.0%) (see Table 37 and Table 38).

Table 37 - 2011-2015 American Community Survey: Percentage by Unit Type									
Geography	1-unit, detached	1-unit, attached	2 units	3 or 4 units	5 to 9 units	10 to 19 units	20 or more units	Mobile home	Other
California	58.1%	6.9%	2.5%	5.6%	6.2%	5.3%	11.6%	3.8%	0.1%
Tulare County	76.0%	2.5%	2.7%	5.2%	2.8%	1.2%	2.4%	7.0%	0.1%
Earlimart CDP	76.5%	4.3%	0.8%	7.4%	4.2%	0.0%	3.0%	3.6%	0.0%
Source: California Department of Finance, 2011-2015 American Community Survey 5-year Estimates									

Table 38 - 2011-2015 American Community Survey: Unit Types									
Geography	1-unit, detached	1-unit, attached	2 units	3 or 4 units	5 to 9 units	10 to 19 units	20 or more units	Mobile home	Other
California	8,044,831	959,528	347,451	769,437	853,934	733,003	1,602,058	519,972	15,576
Tulare County	110,070	3,590	3,961	7,560	4,124	1,729	3,436	10,139	183
Earlimart CDP	1,530	86	17	149	85	0	61	73	0
Source: California Department of Finance, 2011-2015 American Community Survey 5-year Estimates									

Tenure Owner or Renter Living in a Housing Unit

During the decade between 2000 and 2010, the home ownership percentage in California declined by approximately 1% while in Tulare County that percentage declined by approximately 3%. In Earlimart the homeownership percentage remained approximately the same. During this same timeframe the average household size increased among owner- and renter-occupied units in the State of California, Tulare County, and Earlimart, with the exception that in renter-occupied households the average size in Earlimart declined between 2000 and 2010 (see Table 39).

Table 39 - Ownership and Household Size (2000 & 2010)								
	2000				2010			
	Percent of owner-occupied units	Average household size of owner-occupied units	Percent of renter-occupied units **	Average household size of renter-occupied units	Percent of owner-occupied units	Average household size of owner-occupied units	Percent of renter-occupied units	Average household size of renter-occupied units
California	56.9%	2.93	43.1%	2.79	55.5%	2.95	44.1%	2.83
Tulare County	61.5%	3.18	38.5%	3.43	58.8%	3.24	41.2%	3.52
Earlimart CDP	52.1%	4.51	47.9%	4.26	51.9%	4.54	48.1%	4.22
Source: California Department of Finance								

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According to the California Department of Finance, the 2011-2015 American Community Survey data indicated that 39.7% of the housing units in Earlimart were owner-occupied, which is approximately 17% less than Tulare County (56.7%) and approximately 14.6% than the State of California (54.3%). The average household size in Earlimart is larger for owner-occupied units (4.87 persons per unit) than for renter-occupied units (4.20 persons per unit), and the household sizes in Earlimart for both owner- and renter-occupied units are larger than both Tulare County and the State of California (see Table 40).

Table 40 - 2011-2015 American Community Survey: Tenure							
	Total Occupied Housing Units	Owner-occupied Housing Units	%	Average household size of owner-occupied unit	Renter-occupied Housing Units	%	Average household size of renter-occupied unit
California	12,717,801	6,909,176	54.3	3.00	5,808,625	45.7	2.91
Tulare County	133,570	75,685	56.7	3.24	57,885	43.3	3.50
Earlimart CDP	1,950	775	39.7	4.87	1,175	60.3	4.20
Source: California Department of Finance, 2011-2015 American Community Survey 5-year Estimates							

Housing Conditions

According to the Tulare County Housing Element 2015 Update, approximately 52% of the housing units in Earlimart surveyed in the Housing Conditions Survey were found to be structurally sound. Approximately 41% of the housing units suffered some degree of deterioration and 7% were found to be dilapidated (see Table 41).

Table 41 - 2015 Housing Conditions Survey											
Survey Area	Sound		Deteriorated						Dilapidated		Total Units in Survey Sample
			Minor		Moderate		Substantial				
Earlimart	Units	%	Units	%	Units	%	Units	%	Units	%	
	280	52%	58	11%	127	24%	32	6%	38	7%	535
Source: Tulare County Housing Element 2015											

The percentage of substandard housing in Earlimart decreased by 12% between 1992 and 2015. The percentage was 53% in 1992, 47% in 2003, 33% in 2009 and 41% in 2015 (see Table 42).

Table 42 - Percentages of Substandard Housing Units, Unincorporated Communities in Tulare County 1992-2015				
	1992 Survey Results	2003 Survey Results	2009 Survey Results	2015 Survey Results
Earlimart	53%	47%	33%	41%
Source: Tulare County Housing Element 2015				

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Age of Structures

According to the US Census Bureau, the 2011-2015 American Community Survey data indicated that approximately 2.7% of the housing structures in Earlimart were built in 1939 or earlier. Approximately 12.4% of the housing structures were built in the 20-year period between 1940 and 1959. Approximately 23.6% of housing structures were built in the 20-year period between 1960 and 1979. Approximately 37.0% of housing structures were built in the 20-year period between 1980 and 1999. Approximately 24.3% of housing structures were built in the decade between 2000 and 2009 and no new housing structures have been built since 2009 (see Table 43).

Table 43 - 2011-2015 American Community Survey: Year Structures Built		
Age of Structures	Number	Percentage
Built 2014 or later	0	0%
Built 2010 to 2013	0	0%
Built 2000 to 2009	486	24.3%
Built 1990 to 1999	392	19.6%
Built 1980 to 1989	349	17.4%
Built 1970 to 1979	261	13.0%
Built 1960 to 1969	212	10.6%
Built 1950 to 1959	201	10.2%
Built 1940 to 1949	45	2.2%
Built 1939 or earlier	55	2.7%
Total:	2,001	100.0
Note: Percentages have been rounded		
Source: California Department of Finance, 2011-2015 American Community Survey 5-year Estimates		

Household Size (Overcrowding)

According to the US Census Bureau, the 2006-2010 American Community Survey data indicated that in 2010 approximately 80.6% of the occupied units in Earlimart are not overcrowded (having 1.00 or fewer occupants per room) and approximately 19.4% of the occupied units in Earlimart are overcrowded (having more than 1.00 occupants per room). Approximately 14.4% of the occupied housing units are overcrowded (having 1.01 to 1.50 occupants per room) and 5.0% of the units are severely overcrowded (having 1.51 or more occupants per room). These percentages are higher than those for Tulare County and the State of California.

The 2011-2015 American Community Survey data indicated that in 2015 approximately 76.3% of the occupied units in Earlimart are not overcrowded. Approximately 23.7% of the occupied housing units are overcrowded (16.8% overcrowded and 6.9% severely overcrowded), which is a 4.3% increase over the 2010 data. During this same 5-year period, Tulare County saw a 0.6% decrease in overcrowding (0.1% overcrowded and 0.5% severely overcrowded), while the State of California saw a 0.2% increase in overcrowding (0.1% overcrowded and 0.1% severely overcrowded) (see Table 44).

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Table 44 - Overcrowding: Percentage of Occupied Housing Units						
Geography	2010			2015		
	1.00 or less occupants per room	1.01 to 1.50 occupants per room	1.51 or more occupants per room	1.00 or less occupants per room	1.01 to 1.50 occupants per room	1.51 or more occupants per room
California	92.0%	5.3%	2.7%	91.8%	5.4%	2.8%
Tulare County	88.8%	8.4%	2.8%	89.4%	8.3%	2.3%
Earlimart CDP	80.6%	14.4%	5.0%	76.3%	16.8%	6.9%
Source: California Department of Finance						

Between 2000 and 2010, the average household size in Earlimart remained constant at 4.39 persons per occupied unit and was greater than the average household sizes of Tulare County and the State of California. During this decade, the average household size in Tulare County increased from 3.28 to 3.36 persons per occupied unit and the average household size in the State of California also increased from 2.87 to 2.90 persons per occupied unit. According to the US Census Bureau and the California Department of Finance, the 2011-2015 American Community Survey data indicated that over the 5-year survey period between 2011 and 2015 the average household size in Earlimart increased to 4.47 persons and in the State of California to 2.96, while the average household size in Tulare County remained constant at 3.36. The average household size in Earlimart remained higher than the average household sizes in Tulare County and the State of California (see Table 45).

Table 45 - Average Household Size Per Occupied Unit			
Geography	Average Household Size		
	2000	2010	2015
California	2.87	2.90	2.96
Tulare County	3.28	3.36	3.36
Earlimart CDP	4.39	4.39	4.47
Source: California Department of Finance			

Vacancy Rate

According to the US Census Bureau, the 2006-2010 American Community Survey data indicated that the vacancies in Earlimart were lower than the vacancies in Tulare County and the State of California. Similarly, the homeowner and rental vacancy rates in Earlimart were also lower than the vacancy rates in Tulare County and the State of California.

The 2011-2015 American Community Survey data indicated that the number of vacant housing units and the vacancy rates decreased in the 5-year period between 2010 and 2015. The number of vacant housing units in Earlimart decreased from 2.9% to 2.5%. Likewise, the vacancies in Tulare County decreased from 8.6% to 7.8% and in the State of California from 8.6% to 8.1%. While the homeowner vacancy rates decreased in Tulare County from 2.2% to 1.7% and in the State of California from 2.2% to 1.4%, the vacancy rate in Earlimart remained consistent at 0.0%. The rental vacancy rates decreased in Earlimart from 1.9% to 1.1%, in Tulare County from 4.5% to 3.7%, and in the State of California from 5.0% to 4.1% (see Table 46).

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Table 46 - Vacant Housing Units and Vacancy Rates						
	2010			2015		
	Vacant Housing Units	Homeowner vacancy rate	Rental vacancy rate	Vacant Housing Units	Homeowner vacancy rate	Rental vacancy rate
California	8.6%	2.2%	5.0%	8.1%	1.4%	4.1%
Tulare County	8.6%	2.2%	4.5%	7.8%	1.7%	3.7%
Earlimart CDP	2.9%	0.0%	1.9%	2.5%	0.0%	1.1%
Source: Department of Housing, 2011-2015 American Community Survey 5-year Estimates						

Regional Housing Needs Assessment (RHNA)

State housing element law assigns the responsibility for preparing the Regional Housing Needs Assessment (RHNA) for the Tulare County region to the Tulare County Association of Governments (TCAG). The RHNA is updated prior to each housing element cycle. The current RHNA, adopted on June 30, 2014, is for the fifth housing element cycle and covers a 9.75-year projection period (January 1, 2014 – September 30, 2023).

The growth projections applied in the Tulare County Housing Element Update are based upon growth projections developed by the State of California. A “Regional Housing Needs Assessment Plan” provides a general measure of each local jurisdiction’s responsibility in the provision of housing to meet those needs. The TCAG was responsible for allocating the State’s projections to each local jurisdiction within Tulare County including the County unincorporated area, which is reflected in the Housing Element.

“The Sustainable Communities and Climate Protection Act of 2008 (SB 375) was passed to support the State’s climate action goals...to reduce greenhouse gas (GHG) emissions through coordinated transportation and land use planning. The bill mandates each of California’s Metropolitan Planning Organizations (MPO) prepare a *sustainable communities strategy* as part of its regional transportation plan (RTP). The SCS contains land use, housing and transportation strategies that, if implemented, would allow the region to meet its GHG reduction targets.”⁵⁵

“In the past, the RHNA was undertaken independently from the RTP. SB 375 requires that the RHNA and RTP/SCS processes be undertaken together to better integrate housing, land use, and transportation planning. The law recognizes the importance of planning for housing and land use in creating sustainable communities where residents of all income levels have access to jobs, services, and housing using transit, or by walking and bicycling.”⁵⁶

⁵⁵ TCAG, *Final Regional Housing Needs Plan for Tulare County 2014-2023*, Page 5, <http://www.tularecog.org/wp-content/uploads/2015/07/Final-Regional-Housing-Needs-Plan-for-Tulare-County-2014-2023.pdf>

⁵⁶ *Ibid.*

EARLIMART COMMUNITY PLAN UPDATE

“In addition to the RHNA requirements, SB 375 requires that TCAG address the region’s housing needs in the SCS of the RTP, to include sections on state housing goals (Government Code Section 65080(b)(2)(B)(vi)); identify areas within the region sufficient to house all the population of the region (including all economic segments of the population) over the course of the planning period for the RTP (out to 2040 for the 2040 RTP/SCS); and identify areas within the region sufficient to meet the regional housing needs.”⁵⁷

Tulare County Region January 1, 2014 to September 30, 2023								
Jurisdiction	1/1/2014 Housing Unit Control Totals	2023 Housing Unit Control Totals	Estimated 9/30/2023 Housing Units		2024 Housing Unit Control Totals	Housing Units 9/30/2023 (Based on Allocation of 26,910 units)		
			Housing Units	Percent of Total		Total Housing Units	Percent of Total	Net New Housing Units 1/1/2014- 9/30/2023
	A	B	C	D	E	F	G	H
Dinuba	6,223	7,106	7,186	4.05%	7,212	7,188	4.05%	965
Exeter	3,803	4,305	4,426	2.50%	4,365	4,428	2.50%	625
Farmersville	2,878	3,253	3,343	1.89%	3,298	3,344	1.89%	466
Lindsay	3,384	3,858	3,972	2.24%	3,914	3,974	2.24%	590
Porterville	17,764	20,331	20,952	11.82%	20,639	20,960	11.82%	3,196
Tulare	20,022	22,908	23,606	13.32%	23,255	23,616	13.32%	3,594
Visalia	47,380	55,411	57,379	32.37%	56,386	57,401	32.37%	10,021
Woodlake	2,187	2,486	2,558	1.44%	2,521	2,559	1.44%	372
Unincorporated County	46,774	52,477	53,834	30.37%	53,151	53,855	30.37%	7,081
Total	150,415	172,134	177,255	100.00%	174,741	177,325	100.00%	26,910

Sources

Columns A, B, and E: TCAG 2040 Demographic Forecast (2013)

Column C: Estimated using trendline growth between Columns B and E

Column D: Column C divided by countywide total from Column C (177,225)

Column F: Proportionally scaled up from Column D to 9/30/2023 countywide total (177,325)

Column G: Column F divided by countywide total from Column F (177,325)

Column H: Column A subtracted from Column F

The RHNA housing results are summarized in **Table 47**. The Tulare County RHNA Plan recommends that the County provide land use and zoning for approximately 7,081 units per year in the unincorporated portions of the County. The County administratively agreed to a housing share of 7,081 units (726 units per year over the 9.75-year RHNA planning period). The RTP allocates 30% of population to the County. The RHNA bases the housing needs assessment on this percentage.

Affordable Housing - The community of Earlimart has a median income of \$23,803 which is considerably less than 60% of the State median income of \$61,818. Approximately 55% of the households in Earlimart spend 30% or more of their income on rent.

Moreover, 58.8% of renters spent 35% or more of their income on rent. As 64.5% of the households

⁵⁷ Op. Cit.

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include singles females with children and Earlimart's average household size of 4.39 per unit, it is very likely that many children in Earlimart share bedrooms.

There is one bus line along highway 99 which has two bus stops in Earlimart. As such, public transportation is not a viable alternative for most people living in Earlimart. Within this context, reduced parking is not a realistic strategy to reduce development costs of affordable housing. Typically, affordable housing will require more land in Earlimart than would typically be required in an area where public transit is available.

There are large vacant parcels surrounding the Alila Middle school. These parcels are ideal for affordable housing as it would make it easier for the children to attend middle school.

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EXISTING ADOPTED LAND USE PLAN

Table 48 shows that a majority of the land in the adopted Earlimart Community Plan area (see **Figure 23**) is designated Residential (826.3 acres). In total, there is about 1,399.7 acres of designated lands in the Earlimart Community Plan Area and 228.0 acres within the planning area are dedicated to Highway and Railroad ROW.

Table 47 - Existing Land Use Plan		
Land Use Designation	Existing Acres	Total Acres
General Commercial	60.17	
High Density Residential	21.62	
Highway Commercial	32.69	
Light Industrial	142.6	
Low Density Residential	826.3	
N/A (Blank Land Use Designation	21.07	
Neighborhood Commercial	3.32	
Park	5.15	
Public/Quasi-Public	44.72	
Service Commercial	14.15	
Rights-of-Way	228.0	
Total	1,399.7	
Source: Tulare County GIS		

Legend

- Parcel Lot Lines
- Earlimart UDB
- Railroad
- Adopted Land Use**
 - Low Density Residential
 - High Density Residential
 - Neighborhood Commercial
 - General Commercial
 - Highway Commercial
 - Service Commercial
 - Light Industrial
 - Public/Quasi-Public

Scale: 0 to 4,000 Feet

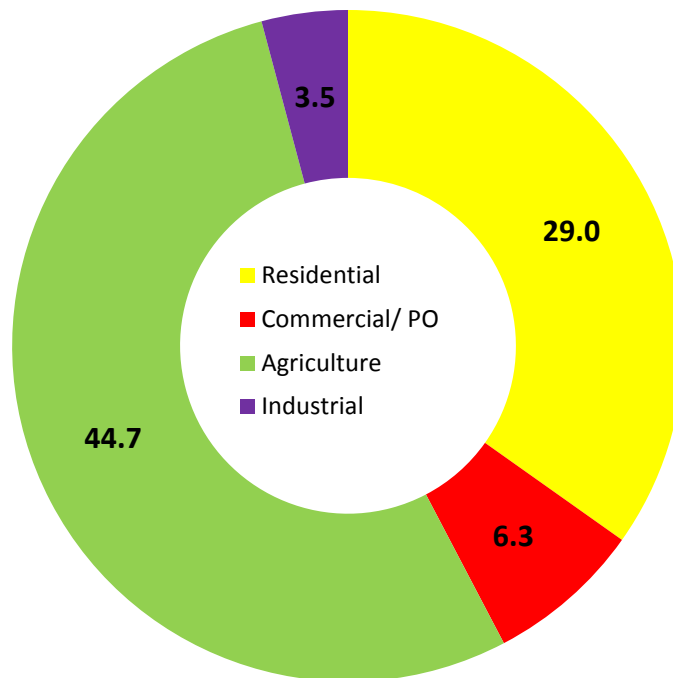
Figure 23

EARLIMART COMMUNITY PLAN UPDATE

EXISTING ZONING DISTRICTS

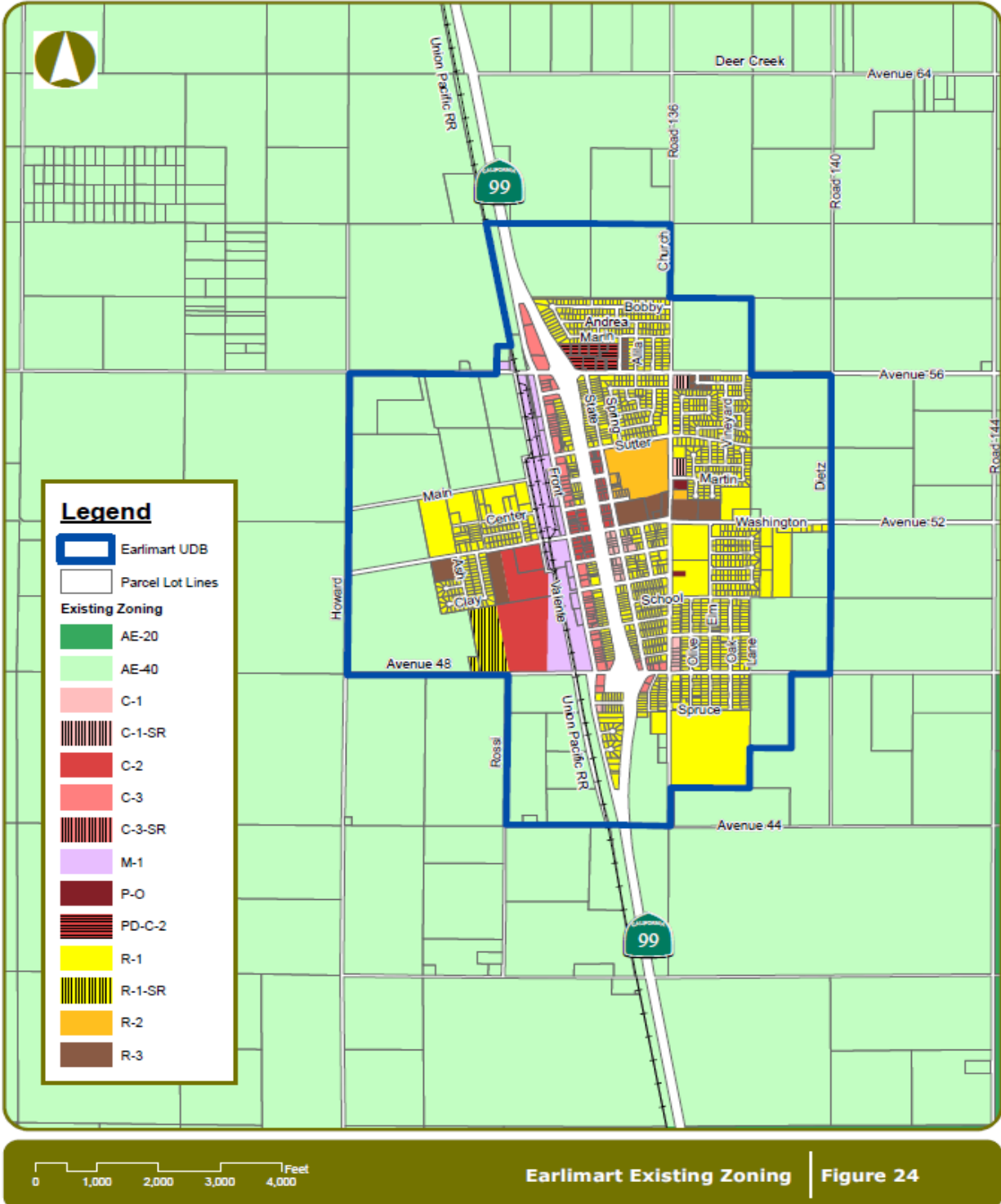
The zoning designations within the existing Earlimart Community Plan UDB are shown in **Figure 24**. As demonstrated in **Table 49**.

Table 48 - Existing Zoning Districts	
Zoning Districts	Existing Acres
AE-40	625.1
C-1	6.47
C-1-SR	3.05
C-2	45.27
C-3	22.73
C-3-SR	0.13
M-1	48.5
PD-C-2	8.87
P-O	1.49
R-1	349.6
R-1-SR	12.97
R-2	21.62
R-3	26.05
Rights-of-Way	228.0
Total	1,399.7
Source: Tulare County GIS	



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Figure 24 - Adopted Zoning Districts Map



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MARKET ANALYSIS

Rent Affordability

According to the US Census Bureau, the 2011-2015 American Community Survey data indicated that in 2015 the cost of rent in Earlimart was lower than in Tulare County and the State of California, but that rent constituted a larger percentage of household income. The median rent was \$674 in Earlimart. The median rent was \$830 in Tulare County and \$1,255 in the State of California, which is more than 1.2 times and 1.8 times, respectively, the cost of rent in Earlimart. In Earlimart, the percentage of households paying 35% or more of income on housing was 58.8% while the percentage of households paying 35% or more of income on housing was 46.8% in Tulare County and 47.5% in the State of California (see Table 50).

Table 49 - 2011-2015 American Community Survey: Renter Cost							
Geography	Median Rent	Gross Rent as a % of Household Income					
		Less than 15.0 %	15.0% to 19.9%	20.0% to 24.9%	25.0% to 29.9%	30.0% to 34.9%	35.0% or more
California	\$1,255	9.0%	10.5%	12.1%	11.5%	9.4%	47.5%
Tulare County	\$830	9.5%	11.2%	12.6%	10.7%	9.3%	46.8%
Earlimart CDP	\$674	3.6%	10.2%	9.4%	15.1%	2.9%	58.8%
Source: Department of Housing, 2011-2015 American Community Survey 5-year Estimates							

Owner Affordability

According to the US Census Bureau, the 2011-2015 American Community Survey data indicated that in 2015 the cost of a mortgage in Earlimart was substantially lower than in Tulare County and the State of California, but that the mortgage constituted a larger percentage of household income compared to Tulare County and the State of California. The median owner cost (with mortgage) was \$857 in Earlimart. The median owner cost was \$1,353 in Tulare County and \$2,155 in the State of California, more than 1.5 times and 2.5 times, respectively, the cost of a mortgage in Earlimart. In Earlimart, the percentage of households paying 35% or more of income on housing was 46.1%. The percentage of households paying 35% or more of income on housing was 33.2% in Tulare County and 33.7% in the State of California (see Table 51).

Table 50 - 2011-2015 American Community Survey: Owner Cost						
Geography	Median Owner Cost (with mortgage)	Mortgage as a % of Household Income				
		Less than 20.0%	20.0% to 24.9%	25.0% to 29.9%	30.0% to 34.9%	35.0% or more
California	\$2,155	28.9%	15.1%	12.7%	9.6%	33.7%
Tulare County	\$1,353	32.6%	14.2%	12.2%	7.8%	33.2%
Earlimart CDP	\$857	19.0%	12.3%	13.1%	9.5%	46.1%
Source: Department of Housing, 2011-2015 American Community Survey 5-year Estimates						

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Market Feasibility

Earlimart had 2,001 households in 2015. As noted in the Market Analysis Section, approximately 5,000 households are typically required to support a grocery store. Even if the number of household doubled, there still would not be enough households to support a typical grocery store. With an estimated household spending of \$218 per week on food, approximately \$21M annually is spent on food by the residents of Earlimart. As noted in **Table 52**, typically \$16M of revenue is required to support a typical grocery store. The estimated amount of money spent on food by the community of Earlimart is enough to support a grocery store like Albertsons. In 2014, a new 9,100 sq. ft. Dollar General Store opened in the community of Earlimart.

Table 51 - Estimated Current Discretionary Income in Earlimart	
Mean HH income 2011	\$32,644
Households 2010	2,001
Gross Income	\$65,320,644
Average Tax rate (estimated)	20%
Tax	\$13,064,128
After Tax Income	\$52,056,515
Rental Units	1,133
Rent (monthly)	\$674
Rent Expense	\$763,642
Units with mortgage	618
Cost with mortgage	\$857
Units without mortgage	157
Cost without mortgage	\$264
Mortgage Expense	\$571,074
Households	2,001
Spent per week (HH with kids)	218.25*
Food	\$21,653,892
Estimated Bills	20%
Other Bills	\$4,330,778
(*Source: http://calbudgetcenter.org/MakingEndsMeet/family-budget-results.php)	

Table 52 - Average annual Revenues of Business	
Business	Annualized Revenue
Convenience Store	\$ 145,000
Subway	\$ 452,000
Gas Station	\$ 700,000
Starbucks	\$ 1,060,000
Five Guys Burgers & Fries	\$ 1,139,000
Dollar General	\$ 1,530,000
Dollar Tree	\$ 1,600,000
In-N-Out Burger	\$ 1,925,000
McDonalds	\$ 2,400,000
Longhorn Steakhouse	\$ 3,000,000
Olive Garden	\$ 4,700,000
Albertsons (Grocery Store)	\$ 16,264,607
Trader Joes (estimated)	\$ 23,255,814
Whole Foods	\$ 35,464,000

Sources:

http://www.abmi.net/pages/convenience_store_division

<http://www.qsrmagazine.com/reports/top-50-sorted-average-sales-unit>

Trader Joe's vs. Whole Foods Market: A Comparison of Operational Management, MIT

Darden 2012 Annual Report

Dollar Tree 2013 10K

Whole Foods 2012 10K

Supermarket Industry Overview 2006

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Existing Businesses in Earlimart

In 2013, the following businesses were inspected by the Fire Department.

Existing Businesses in Earlimart

- Earlimart Motel
- Guzman, Isabel Family Child Care
- Rubio, Martha FCC
- Market Street CDC
- Earlimart Child Care Ctr #2
- Earlimart Child Dev Ctr
- Teresita's Family Child Care
- C J's Day Care
- Earlimart Auto Dismantling
- Eden Produce Co
- Alexander Trucking
- Etchegary Farms
- Can Am Produce
- Correia Pallet Yard
- Georgia Pacific
- Maxco (Delano Facility)
- RG Agricultural Services
- Sunrise Almond Hullers
- Treehouse California Almonds
- Vignolo Delmart Farm Shop
- Earlimart Dusters
- Earlimart Welding
- Wilbur Ellis Sulfur Plant
- Deweese Spreaders
- Earlimart Growers
- Vignolo Delmart Cold Storage
- Anchor Warehouse #3
- Campos and Sons Trucking
- Earlimart Community Swapmeet
- Lamanuzzi & Pantelo
- Maxco
- Sandrini Cold Storage
- South Creek Dairy
- Sunview Cold Storage AK Laundry
- Amigo's Market & Liquor
- BE Cell Phone & Gift
- Big B's Travel Center
- Don Coyote Tires
- Earlimart Auto Sales
- Earlimart Barber Shop
- Earlimart Chevron
- Earlimart Dollar Plus
- Earlimart Feed Store
- Earlimart Market
- Earlimart Outreach Clinic
- El Jacalito Restaurant
- El Manatial Bar
- El Mazatlan Mex. Rest.(La Chiquita)
- ET Party Supplies
- Joe's Barber Shop
- La Barca Panaderia
- La Potranca Bar
- La Princesa Bakery
- Lollipop Avenue
- Manantial De Agua Viva
- Mejia Auto Repair
- Nagis Market
- Picar Farming Labor
- Pizza & Wings
- La Potranca Bar
- Ruben's Auto Express (Discount Auto)
- SA Market and Gas
- Sam's Market
- Sandoval's Fish Market
- Saned Freeze
- Earlimart Veterans Memorial Building
- State Market
- Steve's Mini Mart
- Su Casa Market
- Taqueria El Sol de Jalisco
- Tri Valley Growers
- U.S. Mini Mart
- USA Earli Mart – GAS
- Discoteca Pris

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- Carniceria Jalisco
- Colson Auto Parts
- Country & True-Value Hardware

DEVELOPMENT HISTORY

The community of Earlimart had some development between 1992 and 2017. In this time period, 516 traditional housing units were built and 122 mobile homes were built (see Table 54). This community had about 48,211 sq. ft. of commercial development and 109,008 sq. ft. of industrial development during the 1992-2017 time period.

Table 53 – Construction in Earlimart					
	Residential (Single Family & Multifamily)	Mobile homes	Commercial	Industrial	Agriculture
	Units	Units	Sq. Ft.	Sq. Ft.	Sq. Ft.
1992	1	7	432		
1993	38	5			
1994	4	6	880	3,024	
1995	4	11		1,124	2,250
1996	2	3		16,880	
1997	37	11			4,320
1998	53	13	2,233	11,975	
1999	37	8	3,183		
2000	67	10	2,460		
2001	34	3	3,960		768
2002	28	4	448	45,400	
2003	50	3	3,981		25,554
2004	48	3			
2005	94	10	4,762	5,360	
2006	6	7		1,200	
2007	5	9	25,311	1,040	
2008	1	3			
2009	2				
2010	2		561	3,805	
2011		3		4,800	22,400
2012	3	3		14,400	
2013		1	7,381.00		
2014	1	1	9,100.00		
2015	1		2,880.00		
2016	2		3,130.00		5,232.00
2017			1,392.00		
Total	516	122	48,211	109,008	55,292

Based in information from Tulare County's Permit Tracking System

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Population Growth Forecast

As shown in **Table 55** Earlimart's population is projected to increase by a 976 persons during the planning period. It should be noted that the 2010 population baseline is based on the 2010 U.S. Census and projected to increase 1.3% annually which is consistent with the Tulare County General Plan.

Table 54 - Earlimart Population Projections					
Year	2010	2015	2020	2025	2030
Population	8,537	9,107	9,714	10,362	11,053
Note: Projections based on the Tulare County General Plan 1.3% annual growth rate.					

Demand Forecast

Based on the data and analysis contained in **Table 56**, Year 2030 residential unit demand forecast for the Earlimart planning area if the current single-family, multi-family, and mobilehome housing trends continue through the planning period.

Table 55 - Earlimart Housing Development Projections				
Housing Type	Percent of Total	YEAR		
		2007-2011	2020	2030
Single Family Homes	81.7%	646	736	837
Increase from 2007-2011	-	-	90	191
Multi Family Homes	4.1%	32	37	42
Increase from 2007-2011	-	-	5	10
Mobile Homes	14.3%	113	129	147
Increase from 2007-2011	-	-	16	34
Residential percentages from Department of Finance, 2007-2011 American Community Survey. Assume 4.18 persons per occupied unit from 2007-2011 American Community Survey; figures are rounded to nearest whole number.				

Tulare County recognizes that land use and other policies must continue to maintain and encourage a diverse and entrepreneurial economy to ensure that the community thrives. This Economic Development Chapter establishes Tulare County's vision for a long-range economic growth, sets forth the policy framework supportive of that vision, and identifies actions that Tulare County leaders will take to achieve these goals. In particular, this Chapter identifies growing economic sectors that the community looks to accommodate and outlines economic development strategies that will match local residents with the job skills required by employers.

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OPPORTUNITIES & CONSTRAINTS

OPPORTUNITIES

Complete Streets

The Complete Streets Act of 2007 (Assembly Bill 1358) requires counties when updating General Plans, to identify how the jurisdiction will provide for the routine accommodation of all users of the roadway including motorists, pedestrians, bicyclists, individuals with disabilities, seniors, and users of public transportation.

Enterprise Zones

Tulare County has a number of Enterprise Zones. Enterprise zones involve a series of statewide incentives that have recently been eliminated. Although Enterprise Zone area boundaries remain in existence, there are no long term economic benefits of these zones.

Affordable Housing

The community of Earlimart has a median income of \$23,803 which is considerably less than 60% of the State median income of \$61,818. Approximately 55% of the households in Earlimart spend 30% or more of their income on housing. As such, there is a high need for affordable housing.

Moreover, 58.8% of renters spent 35% or more of their income on rent. As 64.5% of the households include singles females with children and Earlimart's average household size of 4.39 per unit, it is very likely that many children in Earlimart share bedrooms.

There is one bus line along highway 99 which has two bus stops in Earlimart. As such, public transportation is not a viable alternative for most people living in Earlimart. Within this context, reduced parking is not a realistic strategy to reduce development costs of affordable housing. Typically, affordable housing will require more land in Earlimart than would typically be required in an area where public transit is available.

There are large vacant parcels surrounding the Alila Middle school. These parcels are ideal for affordable housing as it would make it easier for the children to attend middle school.

Urban Development Boundary

Although State planning law does not define specific requirements for establishing planning area boundaries, it is generally agreed that the planning boundaries should include the territory within a community's probable ultimate physical boundaries and service area. Urban Development Boundaries provide a planning framework that promotes the viability of communities, hamlets, and cities while protecting the agricultural, open space, scenic, cultural, historic, and natural resource heritage of the County. In the past, the County used three key planning tools to guide urban development in all unincorporated areas of the County. The first was the Urban Boundaries Element; the second are the Area Plans; the third are the General Plans for identified incorporated cities and Community Plans for unincorporated communities. In 1974, Tulare County added an Urban Boundaries Element to its General Plan. The element required the designation of an urban boundary for every "viable" unincorporated community in the county. The Urban Boundaries Element also established Urban

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Improvement Areas (20-year planning boundaries) for certain communities. The 1974 Urban Boundaries Element designated both an Urban Area Boundary and an Urban Improvement Area for Earlimart. In 1983, the Urban Boundaries Element was amended to create Urban Development Boundaries (UDBs, which are also to function as 20-year planning boundaries) and to Change the function of the Urban Area Boundary to simply a "comment line" around incorporated cities. Under the 1983 amendment, Urban Area Boundaries are no longer established around unincorporated communities - and Urban Improvement Areas are to be phased out over time (replaced with UDBs) as each community's boundaries are updated.

For unincorporated communities as per the Planning Framework Element of the General Plan, the UDB is a County adopted line dividing land to be developed from land to be protected for agricultural, natural, open space, or rural uses. It serves as the official planning area for communities over a 20-year period. Land within an unincorporated UDB is assumed appropriate for development and is not subject to the Rural Valley Lands Plan or Foothill Growth Management Plan.

RECOMMENDED BOUNDARY ADJUSTMENTS

New High School

The Delano Joint Union High School District is proposing the construction and operation of a new high school specializing in agricultural science and technology. The new 15-acre high school will be located on property owned by the School District, west of and adjacent to the existing Alila School (elementary) located at 850 W. Washington Avenue in Earlimart. The new high school will provide new educational facilities within the community that will accommodate the existing student population as well as the projected student population through 2030. A local high school within the community would reduce the number of vehicles and vehicle trips needed to transport students to school facilities, thereby reducing vehicle miles travelled and reducing impacts on air quality and greenhouse gas emissions from school-related mobile sources.

Northern UDB Expansion

Expansion of the Urban Development Boundary to include the existing Eden Produce Company property and adjacent parcels located north of the existing Urban Development Boundary, to accommodate future commercial and residential uses, and to place the Urban Development Boundary lines along logical alignments, such as property lines and roadways. Although the expansion of the Urban Development Boundary is not necessary to accommodate future growth through 2030, the expanded Urban Development Boundary does provide future developers with greater flexibility in location and project design of potential future developments.

Commercial Potential

There is limited need for an expansion of commercial zoning, it is anticipated, as much as four acres of demand (1 acre per 150 persons). As there is limited discretionary income available from the community (based on the median incomes and proportions going towards housing), new commercial uses will mostly rely on the regional and highway market for revenue potential. As such, there is limited potential for large local community serving commercial uses. New commercial uses will likely be highway oriented, and fit under the new zoning district boundaries or under the new mixed use overlay boundary.

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Commercial Reserve Areas

There is one parcel that has been designated as Commercial Reserve in the General Plan. The parcel is currently zoned AE-40. Updating the Zoning Map creates consistency between the Zoning Ordinance and the General Plan. This would also allow for future development to move forward with entitlement applications without the need for a zone change. By eliminating the need for a zone change, entitlements can be issued quicker and at a lower cost.

Residential Reserve Areas

There are a several parcels that have been designated as Residential Reserve in the General Plan Land Use map. Much of the area designated for Residential Reserve is currently zoned as AE-40 and many of these parcels are used for agricultural purposes. With a zoning designation of AE-40, there is limited potential for residential subdivisions on large agricultural parcels. By updating the zoning designation from AE-40 to R-1 or R-2, Tulare County would reduce the entitlement restrictions and allow these areas to be developed with a residential use as outlined by the General Plan. In addition, updating the Zoning Map creates consistency between the Zoning Ordinance and the General Plan. This would also allow for future development to move forward with entitlement applications without the need for a zone change. By eliminating the need for a zone change, entitlements can be issued quicker and at a lower cost.

Zoning Map Update

The current Zoning Map for Earlimart is very similar to the Land Use Map outlined in the General Plan. There are a couple of zoning changes that need to occur to allow the General Plan and Zoning Ordinance to be in conformity with each other.

AREAS FOR RECREATION

Bicycle Traffic

The Tulare County General Plan includes a map of the Regional Bicycle Transportation Plan network. A Class III Bikeway is proposed on Washington Avenue, Church and School Streets. Although not signed on many local roads in Earlimart, bicyclists are allowed to use the side of the road or share the road on all county roadway facilities excluding freeways.

Running/Walking Trails

Multiuse trails are facilities that can be used by bicycles, pedestrians, equestrians, and other recreational users. Currently there are no multiuse trails located in Earlimart.

CONSTRAINTS

There are several constraints or restrictions which will impact the nature and location of future development within the community. In particular, these constraints pertain to existing problems of limited infrastructure, public health and safety; acceptable noise level impacts of deteriorating housing, lack of a full range of community services. Following are constraints that were recognized in the preparation of this plan.

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Infrastructure Needs

Additional water and wastewater capacities are an important component to allow future growth within Earlimart. The Earlimart Public Utilities District (EPUD) serves only those residential/urban uses within the District's boundary. Not all areas within the Urban Development Boundary (UDB) are within the EPUD existing boundary, which constrains the immediate or near future development of urban uses. Therefore, those areas planned for future urban development outside of the EPUD boundary will require annexation to the district and connection to the EPUD facilities or have to create their own sources for sewer or water or connect to Teviston's CSD for water purposes and build out its own package plant.

As discussed in the Environmental Impact Report (DEIR) Utilities Section 3.18: "The Tulare County Resource Management Agency (RMA) prepared a report titled *Preliminary Earlimart Water & Sewer Capacity Analysis and Request for Information* earlier this year (see Appendix "G" of this DEIR). This report involved an analysis by RMA of information known to RMA regarding the water and sewer serviced provided by the Earlimart Public Utility District (EPUD). This analysis was conducted in part to develop the 2030 General Plan and the 2014 Housing Element, as well as for the development of the EIR for the Earlimart Technical Academy High School and for the Northern Rezone Area Project (NRA). A of the study was provided to EPUD's engineer (Mr. Dennis Keller of Keller Wegley Consulting Engineers), for comment and to request a response to the requests for information contained in the study. In response, the EPUD engineer sent RMA a letter dated June 28, 2017. The RMA reviewed and provided an initial response to the letter regarding water and sewer as follows:

Water

"It should be noted that the Northern Rezone Area Project (NRA) and other properties not currently within the boundaries of EPUD could form their own entity to govern its water and sewer services or join with another nearby utility provider. For example, the Northern Rezone Project could seek to join with the Teviston Community Services District to the north, which provides domestic water. Further, if the Northern Rezone Project were to join with EPUD, it may provide a reduction in operating and maintenance costs per customer through economies of scale.

"As indicated in the Water and Sewer Capacity Analysis prepared by Tulare County RMA, "The proposed northern subdivision is estimated to equate to 1,490 EDUs based on the land use, and require a design flow of 3,833 gpm (PHD). EPUD cannot currently meet the water demand for the Northern Rezone Project, without finding additional water capacity. There are two identified options for this capacity:

1. Construct and develop new wells.
2. Convert four existing agricultural wells located within the planned subdivision area to municipal wells, which would provide a total capacity of 4,100 gpm.

It is assumed that both options will require a local distribution system for the Northern Rezone Area (NRA), an interconnection between the NRA and EPUD, and a booster pump station for the interconnection.

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Option 1 will require the development of at least 3 new wells sites with a combined total capacity of 4,800 gpm. Assuming that there are no issues in [with] meeting a minimum pressure of 25 psi, this option will meet EPUD's existing deficit of 686 gpm and the NRA water needs. This option is estimated to cost approximately \$3,500,000.

Option 2 will require the conversion of the 4 existing agricultural wells to public drinking water wells and the addition of appurtenances like booster pumps, hydro tanks and storage tanks. Assuming that the well[s] site[s] can be converted, produce the same amount (4,100 gpm) and there are no issues in [with] meeting a minimum pressure of 25 psi, this option will meet some of the EPUD's existing deficit with only a 419 gpm deficit remaining. This option is estimated to cost approximately \$2,000,000.”⁵⁸

As indicated in the Tulare County Local Agency Formation Commission's (LAFCO's) Municipal Service Review Final Report (March 2006), Earlimart Public Utility District MSR; “The District generally requires new development projects to construct the necessary infrastructure to serve their development. A program of developer obligated infrastructure improvements provides for the installation of physical infrastructure to serve development sites and therefore relieves the financial obligation of the District. Developers are also required to pay fees for rights to water and sewer capacity, which are ultimately used by the District for capital capacity improvements including, but not limited to, additional wells, storage facilities, or capital WWTF improvements. These fees are set by the Board of Directors by resolution, and are allocated to a restricted reserve account.”⁵⁹

Another possible option would be for the NRA to consolidate with the Teviston Community Services District (TCSD), which lies north of Deer Creek, the northern extent of the Northern Area project. Similar to connection with EPUD, connection to TCSD would likely obligate a developer to pay for infrastructure improvements to serve their development.

Regardless of water supply approach, the NRA project must also comply with General Plan policies PFS-2.1 Water Supply to ensure that there is an adequate quantity and quality of water for all uses, including water for fire protection, for this project; PFS-2.3 Well Testing to provide evidence that a well site can produce the required volume of water (without impacting the ability of existing wells to meet their needs) and PFS-2.4 Water Connections which requires new development in UDBs or existing water district service areas to connect to the community water system (where such system exists) or the County may grant exceptions (in extraordinary circumstances) require new development to connect to the water system when service becomes readily available.”⁶⁰ It is also noted, future subdivisions will also have to comply with SB 610 and SB 220 depending on the amount of connections, but will be required to provide master water plans and a water supply assessment (WSA).

In his response letter, EPUD's engineer correctly notes that the Report references a variety of standards and guidelines which do not apply to the District, as the District is a self-governing entity. These guidelines, particularly where they agree with the adopted policies and rules of EPUD, were provided as a reference of a relevant practice or industry standard. Upon revision, the County's report will more clearly reflect the use of these documents as providing guidance and context. Nevertheless,

⁵⁸ Preliminary Earlimart Water & Sewer Capacity Analysis and Request for Information prepared by the Tulare County Resource Management Agency, May 2017. Pages 4 and 5.

⁵⁹ Group 1 Municipal Service review by the Tulare county Local Agency Formation Commission (LAFCO), final report March 2006, page 5-15.

⁶⁰ DEIR page 3.18-27

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the County would recommend that EPUD consider revisiting the standards and ordinances referenced in the report in advance of significant capital expenditures to the water or wastewater system.

EPUD's engineer also highlights potential issues with the Sustainable Groundwater Management Act (SGMA) and its impacts on water use in this area. As no Groundwater Sustainability Plan (GSP) has been implemented for Earlimart and the surrounding area at this time, the requirements and allocations of water to the undeveloped areas, including the NRA cannot be guaranteed. The water use of the undeveloped parcels, including the NRA, will need to comply with SGMA and any GSP properly implemented thereunder. This may require the development of an alternative source of water, such as importation, or other efforts to ensure any development complies with the sustainable yield requirements under the GSP.

EPUD's water system has limited unallocated capacity and would require additional sources of water to meet the demand of any significant new development, such as the NRA. To this end, EPUD's engineer has stated that the Board would likely be unwilling to accept the conversion of existing agricultural wells into municipal wells, and would likely need to or require the development of new municipal wells to increase water system capacity for new development. The County assumes that imported water or treated surface water would also be acceptable to EPUD. EPUD could develop these new sources, which could be financed through loans, grants, or a combination of the two.

EPUD could require that developers for any new significant developments provide new sources of water to meet their demands. Those developers outside of EPUD's current boundary could alternatively form their own governance entity or join with another system, either of which could use the newly developed sources of water.”

The Proposed New High School: “As indicated in the Water and Sewer Capacity Analysis prepared by Tulare County RMA, “EPUD has agreed to supply water service to the Earlimart Technical Academy High School on the condition that the Delano Joint Union High School District (District) develops the following onsite and offsite improvements:

- Provide new 10" PVC water main in Washington Street.
- Provide 3 new Fire Hydrants in Washington Street.
- Provide new 10" PVC water main in Howard Street.
- Provide 3 new Fire Hydrants in Howard Street.
- Develop a private onsite fire water system.
- Develop a private onsite domestic water system.

Per the 2016 California Fire Code, Section BB105 Fire-Flow Requirements for Buildings the minimum fire flow is 1,500 gpm. EPUD was only able to guarantee the Earlimart Technical Academy High School “a peak fire flow of 500 GPM with a minimum residual pressure of 20% for 2 hour duration” and therefore have to design an onsite fire water system to supplement the low fire flow.”⁶¹ Environmental impacts would be limited to temporary, short-term construction-related activities as the physical environment where any new or expanded water facilities would be constructed within

⁶¹ Preliminary Earlimart Water & Sewer Capacity Analysis and Request for Information prepared by the Tulare County Resource Management Agency, May 2017. Page 5.

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existing streets or within the high school site that does not contain or create impacts to aesthetic features; does not contain agriculturally productive lands (see Chapter 3.2 Agricultural Lands & Forestry); does not contain special status species and is devoid of habitat for special status species (see Chapter 3.4 Biological Resources); does not contain known cultural resources (see Chapter 3.5); does not contain known Tribal Cultural Resources (see Chapter 3.17), or does not contain known or contribute to hazards or hazardous materials (see Chapter 3.8).

Regardless of water supply approach, the high school project must also comply with General Plan policies PFS-2.1 Water Supply to ensure that there is an adequate quantity and quality of water for all uses, including water for fire protection, for this project; PFS-2.3 Well Testing to provide evidence that a well site can produce the required volume of water (without impacting the ability of existing wells to meet their needs) and PFS-2.4 Water Connections which requires new development in UDBs or existing water district service areas to connect to the community water system (where such system exists) or the County may grant exceptions (in extraordinary circumstances) require new development to connect to the water system when service becomes readily available.”⁶²

Wastewater

“The EIR discusses the limited [waste water facilities and connection capacity] available for Waste Water Treatment. The Tulare County Resource Management Agency, Public Works Branch, prepared the “*Preliminary Earlimart Water & Sewer Capacity Analysis and Request for Information*” (Water & Sewer Capacity Analysis) includes a recommendation that “...EPUD reanalyze their wastewater conveyance capacity with a reduced peaking factor and/or increase the d/D limit. At a peaking factor of 1.4, the same as the Sewer Study by Afinar [the school District’s consulting engineer], the current infrastructure has capacity for the addition of the school’s wastewater.”⁶³ Additionally, the Water & Sewer Capacity Analysis indicates, “Per the sewer study prepared by Afinar, the proposed [high] school is estimated to produce 35,000 gpd of wastewater on average. With EPUD’s WWTP peak average monthly flow reaches 91%. The addition of the High School’s wastewater will increase the peak average monthly flow to 95% of capacity. If the WWTP is certified by an engineer to treat 1.26 mgd there will be sufficient treatment capacity for the additional flow from the High School.”⁶⁴

[In response], EPUD’s engineer provided empirical evidence that the interceptor sewer line connecting to the wastewater treatment plant runs full. As such, significant new development within the community may require a replacement or a relief line be constructed to connect to the wastewater treatment plant. Further, the Report noted that the wastewater treatment capacity would be limited and new waste discharge requirements would be necessary to increase capacity. Significantly increasing the wastewater treatment capacity of the District’s facilities would likely require modifying their Waste Discharge Requirements with the Regional Water Quality Control Board, which may require additional forms of treatment to reflect current practices and standards. Grants and other funding may be available for the community of Earlimart to improve or replace, as necessary, their collection system and wastewater treatment plant.

⁶² DEIR, Pages 3.18-25 - 26

⁶³ *Preliminary Earlimart Water & Sewer Capacity Analysis and Request for Information prepared by the Tulare County Resource Management Agency, May 2017, Page 8.*

⁶⁴ *Ibid.* 9.

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If EPUD were to require a developer of a significant new development to pay for improvements to the sewer or wastewater treatment plant, the developer may also explore options for forming its own entity or joining with other nearby systems. In this case, the economics of developing a small wastewater treatment facility may have some significant benefits to the new development as compared to connecting to EPUD's system."⁶⁵

State Route (SR) 99

State Route 99 is a major route between cities within the Central Valley. SR 99 is the primary route between the City of Fresno to the North and the City of Bakersfield to the South. SR 99 Earlimart in north/south fashion in effect, have divided the community geographically. Most of the residential development is located on the East side to the Freeway. Most of the commercial and industrial areas are located in the west side of the Freeway. SR 99 is accessible from Armstrong Avenue (Ave 48) and West Sierra Avenue (Ave 56).

Union Pacific Railroad

The Union Pacific Railroad runs parallel to and west of SR 99. This railroad line is located between N Front/S Front Street and N Valente Road/Valenti Street

Noise Contours

There are a variety of sources that produce noise in the Earlimart Plan Area and include traffic, railroad operations, airport operations, and agricultural operations. Traffic noise is the most dominant source of ambient noise in the County, according to the Tulare County General Plan EIR. SR 99 runs through the Earlimart Plan Area and would be the largest source of traffic noise in the area due to the high volumes of traffic. Noise from SR 99 adversely impacts an area through central Earlimart making properties in close proximity to the highway less desirable for new housing construction.

Operations along the Union Pacific Railroad line are another dominant source of noise in Earlimart. According to the Tulare County General Plan EIR, there are more than twenty (20) freight train operations per day along the Southern Pacific rail line in Tulare County and may occur at any time of day or night.

The Noise Element includes performance standards for new residential or other noise-sensitive land uses which are to be located near noise-impacted areas. The Element indicates that these uses will not be permitted unless effective design measures can be integrated into the development to mitigate the impact of noise. **Table 4** summarizes the daily traffic volumes along SR 99 from Avenue 24 to Avenue 48 and Avenue 48 to Avenue 76.

Storm Drain

Storm drain facilities are sporadic throughout Earlimart. As such, new development should be required to construct storm drain facilities. This requirement could increase the cost of development.

⁶⁵ EIR pages 3.18-19 – 20. *RMA Response to EPUD's Engineer Letter* dated June 26, 2017. See Appendix "G" of the EIR.

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Designated Flood Zones

As illustrated in **Figure 10**, a portion of Earlimart, east of SR 99 is within the 100-Year Flood Zone areas. The depth of this potential flood could range from one to three feet. This potential flood condition is caused by the White River, located south of Earlimart, and Deer Creek which is located north of Earlimart, damming up against SR 99 due to conditions which restrict flow under the highway. However, a sufficient amount of land will be available in other parts of the community and current Tulare County policies applicable to flood-zones will allow for the future development of the zones to accommodate future growth needs.

Agricultural Lands within the UDB

The presence of approximately 116.4 acres of Williamson Act contract proposed within for the Earlimart Urban Development Boundary may deter the natural "grow out" pattern of urban related uses. Although a constraint, Agricultural Preserves prevent premature urban development of agricultural lands and encourage in-filling of existing vacant parcels within the immediate core of the Plan Area. But this has also caused to the need to look at increasing the Urban Development Boundary. Also, as the need arises for developable land Agricultural Preserves can be canceled by a landowner with the approval of the Tulare County Board of Supervisors, which has an added cost to the applicant of 12.5% of the land value. Another option available to landowners is nonrenewal of their ten-year contracts. This option allows their land to revert to "regular" agricultural lands over a ten-year period and, subsequently allowing the landowner an opportunity to develop his land through the regular permitting process.

ECONOMIC DEVELOPMENT STRATEGY

Tulare County's current Economic Development Strategy focuses on the agricultural industry and pursuing grants.

Agriculture

Tulare County has a booth at the World Agricultural Exposition (Ag Expo) every year. The Economic Development Office uses the event to promote Tulare County tourism and business opportunities. Partnering with the County's Purchasing Department the Ag Expo provides an excellent method to market directly to the global agriculture related businesses attending the Ag Expo and sell surplus county equipment.

Solar Projects

In Tulare County, there have been 13 Utility Scale Solar Projects that have a capacity of 198 MW. There are 9 projects in /Under Construction with a capacity of 260 MW. In terms of total solar projects (including Utility Scale, Solar on Dairies, Commercial Solar, and Residential Solar) there have been 1570 projects built that accounts for a capacity of 227.5 MW. The Corridor offers realistic potential to locate solar projects closer to the urban areas and outside of the direct line-of-sight viewshed of the Highway 99 Corridor. Table 10-4 provides a summary of solar development in Tulare County.

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Table 10-4: Solar Developments in Tulare County

Utility Scale Solar Projects		
Phase of Construction	No. of Permits	Total Capacity (MW)
Under Review	0	0.0
Pre-construction/Under Construction	1	45.0
Constructed	21	413.0
Total	22	458.0
Solar Projects on Dairies		
Phase of Construction	No. of Permits	Total Capacity (MW)
Under Review	0	0.0
Pre-construction/Under Construction	6	3.6
Constructed	44	39.7
Total	50	43.3
Other Commercial Solar Projects		
Phase of Construction	No. of Permits	Total Capacity (MW)
Under Review	0	0.0
Pre-construction/Under Construction	36	8.9
Constructed	172	44.8
Total	208	53.7
Anaerobic Digesters		
Phase of Construction	No. of Permits	Total Capacity (MW)
Under Review	0	
Pre-construction/Under Construction	16	
Constructed	4	
Total	20	
Residential Solar Projects (based on 7 kw/sfd average)		
Phase of Construction	No. of Permits	Total Capacity (MW)
Under Review	13	0.09
Pre-construction/Under Construction	127	0.89
Constructed	2092	14.64
Total	2232	15.6
Solar Project Totals		
Phase of Construction	No. of Permits	Total Capacity (MW)
Under Review	13	0.09
Pre-construction/Under Construction	186	58.39
Constructed	86	512.14
Total	2532	570.74

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Grants

- State Water Resources Control Board – State Revolving Fund: \$500,000 for Traver Community Wastewater System Improvements Planning Study and Design, once plans are near complete we will apply for construction funding between \$8 and 10 million.
- State Water Resources Control Board: have applied and received \$5 million in construction funding for Phase 1 of the Yettem Seville Water System.
- County Measure R funding \$575k for sidewalks and ADA improvements in Earlimart around the Earlimart Park.
- ATP Active Transportation Program- Statewide competitive \$2 million funding is anticipated for Safe Routes to School and ADA improvements in and around three (3) Earlimart Schools.
- Low Carbon Transit Program funding \$147,474.00.
- Prop 84 – Earlimart Neighborhood Improvement Program funding \$2,153,900.00.
- Yettem & Seville Project Phase 1 funding \$4,300,200.00
- Navigation Aids at Sequoia Field Airport funding \$340,200.00
- Transit Operations & Maintenance Facility (TOMF) funding \$10,800,000.00

Online Presence

Tulare County is currently developing and growing the Economic Development Website by offering content that is current, informative and useful for decision-makers in an effort to attract, retain and expand a diverse business community in all regions of the County. We are currently building an online industrial/commercial property inventory. We are reaching out to brokers in order to post their listings on our website.

The Tulare County Economic Development Office maintains an active presence on Facebook and Twitter. In addition, the Tulare County Film Commission Channel contains the latest videos of groundbreaking and ribbon cutting events. Content development will continue to grow over time.

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SWOT ANALYSIS	
Strengths	Weaknesses
<ul style="list-style-type: none"> ▪ Tulare County has a strong agricultural economy. ▪ There is a senior center on East Washington Avenue. ▪ Earlimart has two elementary schools (K-2 and 3-5) and a middle school (6-8). ▪ There are three crossings over SR 99 which allows Earlimart to feel like a cohesive community. ▪ There are two on- and off-ramps to and from SR 99. ▪ There is a baseball field across the street from Earlimart Middle School. ▪ The cost of living is low in Tulare County. ▪ Earlimart has a Health Care Center on East Washington Avenue. ▪ There are a number of food packing businesses along N Front/S Front Street. 	<ul style="list-style-type: none"> ▪ Earlimart is considered a severely disadvantaged community with a median income of \$23,803. ▪ The Storm drain system is inadequate. ▪ There is no High School in Earlimart. ▪ The aesthetics of existing commercial buildings are low. ▪ Sidewalks are needed. ▪ Economic growth is heavily dependent on industrial development because the limited population size will not attract large commercial uses. ▪ Earlimart has limited transportation options. There is one bus line and two bus stops in Earlimart. ▪ Industrial parcels are medium sized and there are no large industrial parcels, so large industrial developments will be want to locate in Earlimart.
Opportunities	Threats
<ul style="list-style-type: none"> ▪ Vacant land is available. ▪ Rail Line is available. ▪ Earlimart is located on SR 99. ▪ The Alila School is surrounded by areas designated as Residential Reserve. There is a potential for Medium/High Density Residential near this Middle School. ▪ A Mixed Use Overlay Zone would allow for additional development. ▪ There is growth in distribution jobs in California. ▪ Earlimart has a bus line TCaT run by Tulare County that stops at United Health Care Center in Earlimart. 	<ul style="list-style-type: none"> ▪ Earlimart competes with Visalia, Tulare, Dinuba, Porterville, Pixley, and other parts of the County. Earlimart also competes with the City of Delano. ▪ Farms are getting larger and relying more on machinery. This can reduce jobs and incomes. ▪ Tulare County has a limited budget for road improvements. Budgeting for roadway maintenance could be reduced depending on the financial health of Public Works department. ▪ Small businesses are sensitive to changes in tax rates, minimum wage, requirement of health care provisions and other nationally established policies and requirements. ▪ National and Statewide economic trends can impact Earlimart's economy.

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REDUCING BARRIERS TO ECONOMIC DEVELOPMENT

California Competes

“The California Competes Tax Credit is an income tax credit available to businesses that want to locate in California or stay and grow in California. Tax credit agreements will be negotiated by GO-Biz and approved by a newly created “California Competes Tax Credit Committee,” consisting of the State Treasurer, the Director of the Department of Finance, the Director of GO-Biz, one appointee from the Senate, and one appointee from the Assembly.”⁶⁶

“The California Competes Tax Credit only applies to state income tax owed to the Franchise Tax Board. The credit is non-refundable, and in the case where the credit allowed exceeds tax owed, the excess may be carried over to reduce the tax in the following year, and the succeeding five years if necessary, until exhausted.”⁶⁷

“The value of the credit will be based on the following factors:

- The number of jobs the business will create or retain in this state.
- The compensation paid or proposed to be paid by the business to its employees, including wages and fringe benefits.
- The amount of investment in this state by the business.
- The extent of unemployment or poverty where the business is located.
- The incentives available to the business in this state, including incentives from the state, local government, and other entities.
- The incentives available to the business in other states.
- The duration of the business’ proposed project and the duration the business commits to remain in this state.
- The overall economic impact in this state of the business.
- The strategic importance of the business to the state, region, or locality.
- The opportunity for future growth and expansion in this state by the business.
- The extent to which the anticipated benefit to the state exceeds the projected benefit to the business from the tax credit.”⁶⁸

“The tentative amount of credits that GO-Biz can allocate is as follows:

- \$30 million in fiscal year 2013/14.
- \$150 million in fiscal year 2014/15.
- \$200 million in each fiscal year 2015/16 through 2017-18.”⁶⁹

“...25 percent of the total credits available each year is expressly reserved for small businesses (gross receipts of less than \$2 million during the previous taxable year).”⁷⁰

Tulare County Strategy

In rural areas, elimination of barriers to economic development is the foundation for growth. This

⁶⁶ California Competes Tax Credit FAQ

⁶⁷ California Competes Tax Credit FAQ

⁶⁸ California Competes Tax Credit FAQ

⁶⁹ California Competes Tax Credit FAQ

⁷⁰ California Competes Tax Credit FAQ

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Plan addresses the following four potential barriers to Economic Development.

- *Infrastructure:* The water system is at capacity and the wastewater system is near limiting capacity. In order for more development to occur, service levels for water and wastewater need to be expanded. Grant funding is needed to increase service levels.”
- *Use Permits:* There are a number of uses that currently require Planning Commission approval. In many cases, these uses are beneficial for the community and do not necessarily need discretionary review. In order to reduce the cost of and length of time to obtain entitlements, use permit requirements are being reduced.

- *Education:* Tulare County has five satellite campuses for four year universities: California State University-Fresno, University of California-Davis, Fresno Pacific University, Brandman University, and the University of Phoenix. Community Colleges in Tulare County include the

Table 56 - Education Attainment			
Educational Attainment	California	Tulare County	Earlimart
Population 25 years and over	25,257,858	260,964	4,142
Less than 9th grade	10.0%	20.8%	48.9%
9th to 12th grade, no diploma	8.2%	10.9%	15.9%
High school graduate (& equivalency)	20.7%	25.2%	21.6%
Some college, no degree	21.8%	22.3%	9.6%
Associate's degree	7.8%	7.0%	2.3%
Bachelor's degree	19.8%	9.5%	1.7%
Graduate or professional degree	11.6%	4.3%	0.0%

2011-2015 American Community Survey 5-Year Estimates

College of the Sequoias, Porterville College, and San Joaquin Valley College. Workforce Development Partners include Proteus Inc., and CSET.

Based on the 2011-2015 American Community Survey (**see Table 57**), the educational barrier in Earlimart begins in grade school. Of the adults age 25 and older, 48.9% had an educational level of less than 9th grade. This limits the types of jobs that these adults are qualified for. Improving educational attainment needs to begin in elementary school. As part of the safe routes to schools, Tulare County will provide pedestrian facilities for children to walk or bike to school. This should make it easier for children to get to school.

Home Occupation Regulations

Encouraging small scale entrepreneurship is one way to foster economic development. Although some home occupations are allowed by right, these home occupations have regulations which limit the type and scope of allowed businesses. By a reduction in some of these regulations, barriers to small scale entrepreneurship will be reduced/eliminated. The following are proposed changes to the Zoning Ordinance:

Eliminate

Section 15.A.7.a.6: “No one other than residents of the dwelling shall be employed in the conduct of a home occupation.”

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Section 15.A.7.b.6 “No one other than residents of the dwelling shall be employed in the conduct of a home occupation.”

Section 15.A.7.c.5 “A rural home occupation shall be limited in employment to residents of the property and not more than one (1) additional person.”

Replace with:

“Employment in a rural occupation shall be limited to residents of the property and not more than three (3) additional non-resident persons.”

Add:

All home occupations located within the Earlimart UDB will not require a use permit.

Health Care

Health care is important for economic development as businesses need healthy employees. United Health Centers provide health care serves for the Earlimart area, located at 476 E. Washington Avenue. United Health Centers offers Community Health and Migrant Health services. Services available are dental, diagnostic, family health, immunization, Pharmacy, pregnancy, radiology, and services for women’s health. Other medical offices are located in the community of Pixley and the City of Delano, Kern County, California.

MARKETING STRATEGY

Place

In terms of real estate, location is a major factor (influence) in development and expansion of businesses. There are two locational advantages in Earlimart, access to SR 99 and industrial clusters. These locational advantages are discussed below.

- *Access to SR 99:* There are two freeway entry and exits points which provide convenient access to SR 99. W Sierra Road and Armstrong Avenue have on-ramps and off-ramps for both northbound and southbound access to SR 99. There are two gas stations located at the Armstrong Avenue exit and another located on Sierra Avenue, adjacent to SR 99.
- *Industrial clusters:* One of the goals of this community plan is to strengthen the current businesses located in Earlimart. With the establishment of industrial clusters, existing businesses can attract other types of similar businesses. There are a few food packing and cold storage plants in Earlimart. There are several



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locations on N Front Street/S Front Street that would be ideal for medium sized industrial uses. There are also a couple of parcels along N Valente Road which would allow for medium to large industrial uses. N Front Street/S Front Street and N Valente Road is an ideal location for a Food Packing/Distribution Cluster.

- *Quality of Life:* It is possible to increase the desirability of Earlimart. With physical improvements, the quality of life can be improved. There are a number of potential projects that can improve the quality of life in Earlimart. New recreational facilities and aesthetic improvements have the most visible effect(s) on the perceived improvement on the quality of life. Physical improvements could include the following:
 - 1) Streetscape improvements for N Front Street/S Front Street.
 - 2) A bike/running trail along the west side of the Union Pacific Railroad.
 - 3) Plant Street Trees on N Front/S Front Street to screen Industrial uses from commercial uses. Sidewalks on commercial side of the street.
 - 4) Plant additional trees along SR 99 to reduce noise impacts.
 - 5) The Earlimart School District has partially vacant property at School Avenue and S Elm Road. This site would be a good location for a community park.
 - 6) Development of Class III bike Facility on W Sierra Road.

Price

New businesses consider price of land and/or rent in their formation of business plan. The cost of the land and/or the cost of rents for commercial and/or industrial uses provide an estimate of the cost of development (see Table 58). See cost outlined below.

Table 57 - Cost of Vacant Land	
Zoning	Average Cost Per Acre
Agriculture	\$5,897
Commercial	\$54,800
Industrial	\$21,619
Residential	\$35,715

- *Cost of Land:* Staff reviewed assessor data and calculated the average assessed value of land per acre based on zoning district.
- *Industrial Rents:* “The county’s overall industrial market consists of about 23 million square feet of space with vacancy below 5 percent. Monthly rents range between \$0.15 and \$0.30 per square foot on a triple-net basis.”⁷¹
- *Commercial Rents:* Staff has no information on the retail rents in Earlimart; however, staff assumes that retail rents in Earlimart are lower to match the smaller market size of the commercial establishments in Earlimart.

Product

The products to be marketed in Earlimart are vacant land and available buildings.

- *Vacant Land:* Staff has prepared a vacant parcel inventory which identifies vacant parcels of various sizes and various zoning districts (see Table 59 and Figure 25).
- *Available Buildings:* There are limited opportunities to use buildings for commercial or industrial uses. When individual spaces become available, it would behoove a property owner to coordinate with a real estate agent to lease out each space.
- *Inventory:* The Tulare County Economic Development Department is reaching out to local real

⁷¹ Visalia General Plan Update: Existing Conditions Report, page 3-33

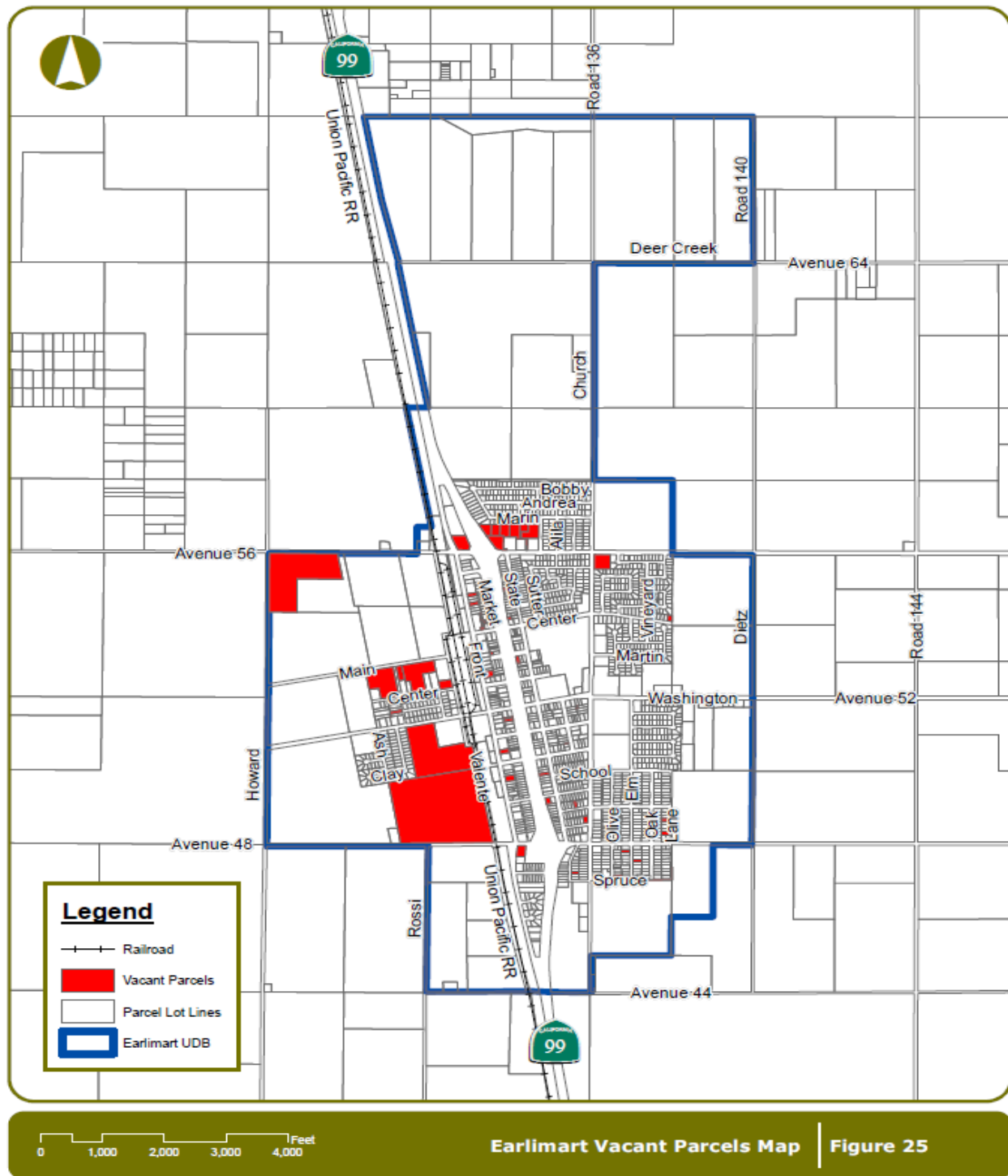
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estate brokers to develop a parcels availability database. In addition, the County will be providing links from its Economic Development Webpage to individual broker webpages which can serve to advertise the availability, location, size, etc., of parcels.

Table 58 - Vacant Parcels		
APN #	Size/Acres	Zoning
315-030-018	0.01	R-1
315-030-061	0.16	R-1-MU
315-030-060	0.06	R-1-MU
315-040-003	0.60	R-1
315-040-005	0.60	R-1
315-040-007	0.74	R-1
315-040-012	4.81	R-1
315-060-044	0.40	C-3-MU
315-072-032	0.19	C-3-MU
315-073-001	0.17	R-1
315-073-019	0.08	R-1
315-091-012	0.06	R-1
315-092-006	0.27	C-2
315-104-014	0.15	C-2
315-111-014	0.33	C-2-MU
315-111-01	0.09	C-2-MU
315-132-010	0.14	R-1
315-145-002	0.17	C-3-MU
315-030-018	0.01	R-1
318-064-003	0.18	R-1
318-064-008	0.18	R-1
318-064-017	0.18	R-1
318-064-001	0.05	R-1
318-071-012	0.19	R-1
318-081-011	0.22	R-1
318-360-062	0.65	R-1-MU
318-380-077	0.19	R-1
318-390-058	1.01	R-1
318-072-023	0.16	R-1-MU
337-082-008	0.17	R-1

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Figure 25 - Location of Vacant Parcels

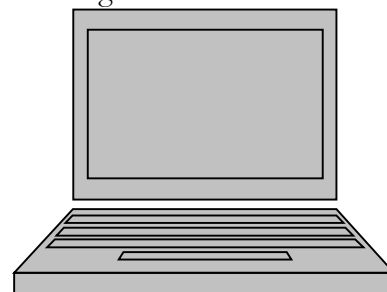


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Promotion

Promotion of Earlimart and properties in Earlimart is a continual need for effective economic development. The following tasks can be used to promote the community of Earlimart.

- *Companies to Target:* The Community of Earlimart is a small community along SR 99, economic development should be focused on enhancing existing assets. There are significant industries and existing businesses located in Earlimart. Tulare County should target the following agricultural industrial business cluster:
 1. Food production companies already in Tulare County.
 2. Food companies in California and the United States.
 3. Cold Storage companies.
- *Online Presence:* Content is one of the most important aspects of an online presence. The amount of content provides relevancy and usefulness. The following could be done to improve the County of Tulare's online presence:
 1. Constantly improve (and keep current) the Economic Development website
 2. Additional information about Tulare County could be added to this website. Pictures from Tech Fair Photo Contest are available for use by Tulare County Departments.
 3. Write and submit articles for trade magazines and bloggers.
 4. Develop a Tulare County Economic Development Blog.
 5. In addition to social media sites Facebook and Twitter, Tulare County's Economic Development Department could utilize other Social Media sites such as Instagram, Youtube, Flickr, etc.
- *Brochures:* Brochures provide specific and relevant information. Brochures should be designed for the following.
 1. Specific Site Handouts brochures with the following information: APN, address, size, price, contact information, pictures of site, pictures of the neighborhood, zoning designation, and a location map.
 2. A Food Production Industry Cluster brochure with the following information: a list of available properties, pictures of the area, prices of land, highlights of the Agricultural Industry in Tulare County.
 3. A Solar/Alternative Energy Industry Cluster brochure with the following information: a list of available properties, pictures of the area, prices of land, highlights of the Solar/Alternative Energy Industry in Tulare County.
 4. A brochure that lists the real estate brokers in Tulare County. This brochure should include contact information such as name, address, phone and email.
- *Advertising:* Tulare County's Economic Development Office and the community of Earlimart can be advertised in targeted trade journals. These trade journals could include:
 1. California Leagues of Food Processors
 2. Food Manufacturing Magazine
 3. Food Processing Magazine
 4. Food Business News Magazine
 5. Logistics Business Magazine



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6. Food and Beverage Packing Magazine
7. Packing World
8. Packing Digest

- *Events:* Conferences and other events are excellent venues for networking. Earlimart could be promoted at the following conferences.
 1. World Ag Expo
 2. Food Shows
- *Listing of properties online:* Providing property listings online allows for potential businesses to learn about opportunities in Earlimart. There are several online real estate listing sites. There are two that could be considered for property listings.
 1. Loopnet
 2. Showcase

Steps, Tasks, Methods, Techniques, Build Trust with content. Good content, components, part, Content, Content, Content

- *Videos:* Promotional videos could be available online and displayed on conferences.
 1. Powerpoint presentation video of pictures and statistics.
 2. Videos of movies filmed in Tulare County.

Development Suitability Analysis

The purpose of a development suitability analysis is to determine the areas of the community and surrounding vicinity which could most appropriately accommodate new growth. It is a means to determine the location of the Earlimart Urban Development Boundary, identifying areas free of development constraints and areas in which improvements must be made before urban development. This development suitability analysis will also be used to help establish land use patterns for the community's future growth.

To determine development suitability, factors that either encourage or constrain development were selected and mapped. Each factor was assigned a suitability rating to show the level of influence it will have on potential new development. Suitability ratings used in this study include:

<i>Very High:</i>	<i>These areas are "infill lands", or are surrounded on three sides by existing urbanized land and are, or can be easily be, serviced with urban services.</i>
<i>High:</i>	<i>These areas are lands free of development constraints and which are either adjacent to existing development and are, or can be, served with community sewer and water service. Wastewater service areas but which are near existing urban development.</i>
<i>Moderate:</i>	<i>These lands are outside existing boundaries of local water and these lands also exhibit other qualities that make them suitable for future development.</i>

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Low: These lands should be precluded from intensive development until certain constraining conditions can be changed or corrected. Examples of such constraints include flooding and Williamson Act lands.

From the process of mapping the various factors potentially influencing the developability of the community and surrounding environs, a composite analysis of the application of these factors to each segment of the planning area is possible. The patterns which emerged from this composite analysis have enabled the overall developed suitability of the planning area to be mapped, in conformance with the ratings described above.

It is recommended, based on the Market Analysis, Opportunities and Constraints analysis as contained above that the existing configuration of the Urban Development Boundary as adopted be expanded to encompass the entire planning Study area (see **Figure 21**) as this would accommodate the needs of anticipated future growth.

PROPOSED URBAN DEVELOPMENT BOUNDARY

The Community Plan Update encompasses the New High School Project, Northern Earlimart Rezone Project, and the Existing UDB Project (see **Figure 26**) as discussed below. The Community Plan Update proposes approximately 541 acre (13 parcel) expansion to the north of the existing Urban Development Boundary (UDB), amendments to land use and zoning designations, and the Complete Streets and Road Maintenance programs.

New High School Project

The Delano Joint Union High School District is proposing to construct a new approximately 350-student high school on approximately 17 acres of 25-acre site within the community of Earlimart. The proposed construction site is approximately 0.68 miles west of State Route (SR) 99, a primary north-south corridor running longitudinally through the San Joaquin Valley. Areas to the north, west and south are presently zoned for and engaged in agricultural production. These areas are flat fields under intensive agricultural use typified by row crops and orchards. The high school's area visual considerations are likely dependent on use patterns, time of day and volume of activity in addition to the eventual presence of the high school building itself. The building design of the high school has not been determined yet; however, given the rural nature of its location it is likely the aesthetics of the campus will not be significantly intrusive upon the rural setting. Alila Elementary School is located immediately adjacent to and east of the proposed high school, residential development is south and east of the elementary school.

Design features of the high school construction site include a bus drop-off, staff parking area, student/visitor parking and parent drop-off vehicular access to the south and west of the building site. These approaches will accommodate access to the high school's buildings, class rooms and laboratories. The area north of the structural facilities plans include gardens, orchards, and an agricultural area for student training. A combined soccer and baseball field would be located in the east part of the campus. The high school would be surrounded by a fence or barrier to provide separation from adjacent properties.

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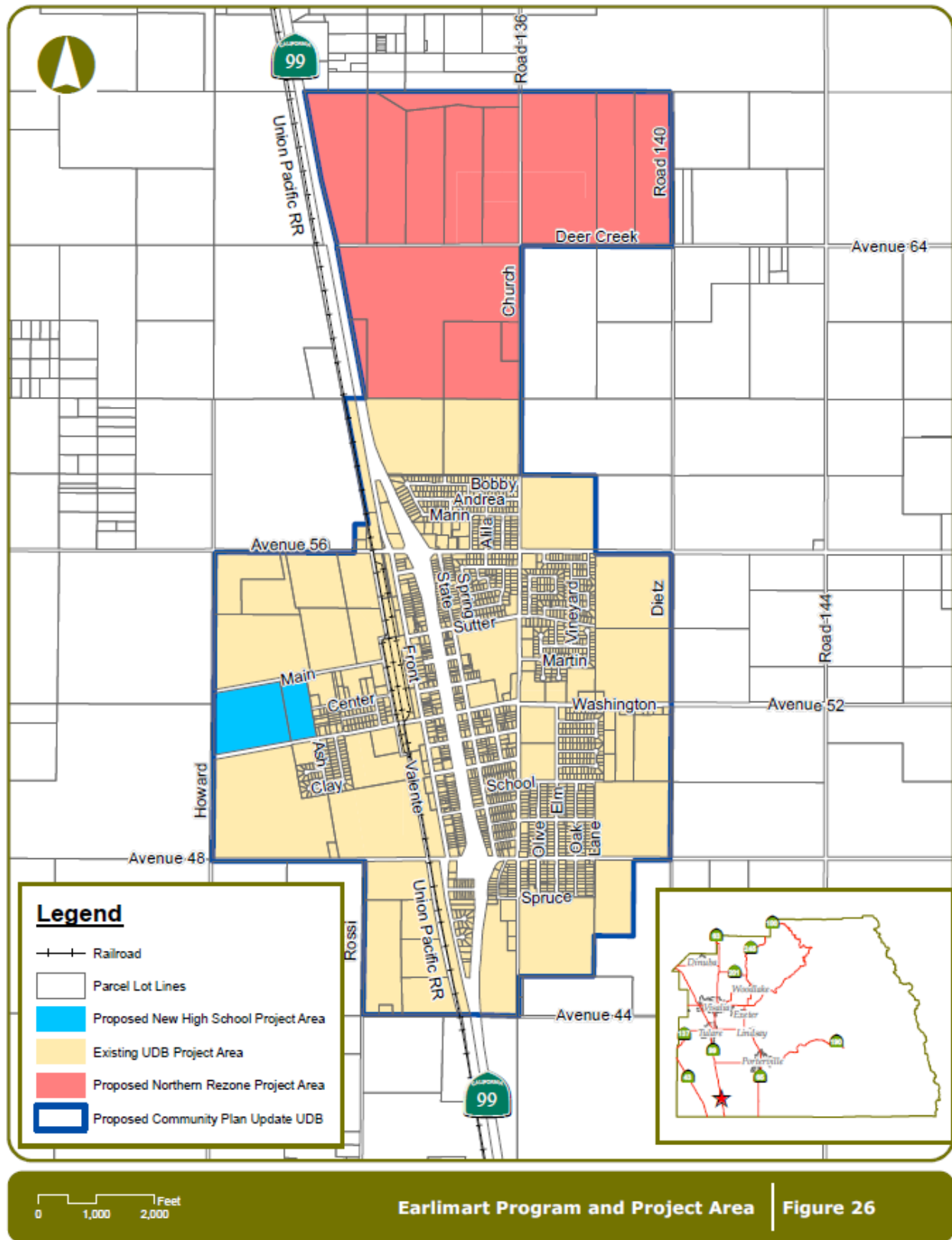
Northern Earlimart Rezone Project

The proposed Northern Rezone Project Area encompasses approximately 541 acres (on 13 parcels) and is located immediately north of the existing UDB area, south of the Teviston Hamlet Development Boundary (adjacent to Deer Creek), and between SR 99 and Road 140. Project Description, Setting, & Objectives. The proposed Northern Rezone Project will amend the existing valley agriculture land use designations of the approximately 541 acre site to Urban Reserve-Commercial (approximately 234.76 acres), Urban Reserve-Residential (approximately 172.97 acres), Low Density Residential (approximately 234.76 acres), High Density Residential (approximately 39.88 acres), General Commercial (approximately 20.57 acres), and Light Industrial (approximately 14.28 acres). The proposed project would also amend the existing agricultural zoning of an approximately 316 acre portion of the project site (8 parcels) to single-family residential (R-1, approximately 241 acres), multi-family residential (R-3, approximately 40 acres), general commercial (C-2, approximately 20 acres), and industrial (M-1, approximately 15 acres) zones.

The project site is predominantly agricultural land (vineyards and nut orchards), with scattered rural residences. The surrounding area is predominantly productive agricultural land (e.g., vineyards, orchards, and other commodities) in all directions, with scattered rural residences to the north and a private airstrip east of the Project site. Deer Creek is the northern boundary of the proposed project while Howard Road is the western boundary, Road 142 (Dietz Road) is the eastern boundary, and the existing Earlimart UDB (Sierra Avenue) as the southern boundary. The unincorporated hamlet of Teviston lies directly north of Deer Creek.

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Figure 26 – Earlimart Program and Project Area



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PROPOSED LAND USE AND ZONING DISTRICTS

Proposed Planning Areas

To develop additional economic development opportunities and respond to market forces, the UDB is proposed for revision. The revision of the UDB includes the New High School Project and the Northern Earlimart Rezone Project. The Community Plan Update proposes a northern expansion of the existing UDB (which includes the northern rezone project area), various amendments to land use and zoning designations (in both existing and proposed UDB expansion areas), and the Complete Streets and Road Maintenance programs (within the existing UDB area).

The northern expansion area includes 13 parcels totaling approximately 541 acres immediately north of the existing UDB area. As such, the proposed Community Plan Update will expand the existing 1,399.7 acre UDB by approximately 39%, for a total UDB area of approximately 1,945.6 acres. The proposed Community Plan Update will also amend the land use designations of the entire 541 acre northern expansion area, but will rezone only a 316 acre portion of that area.

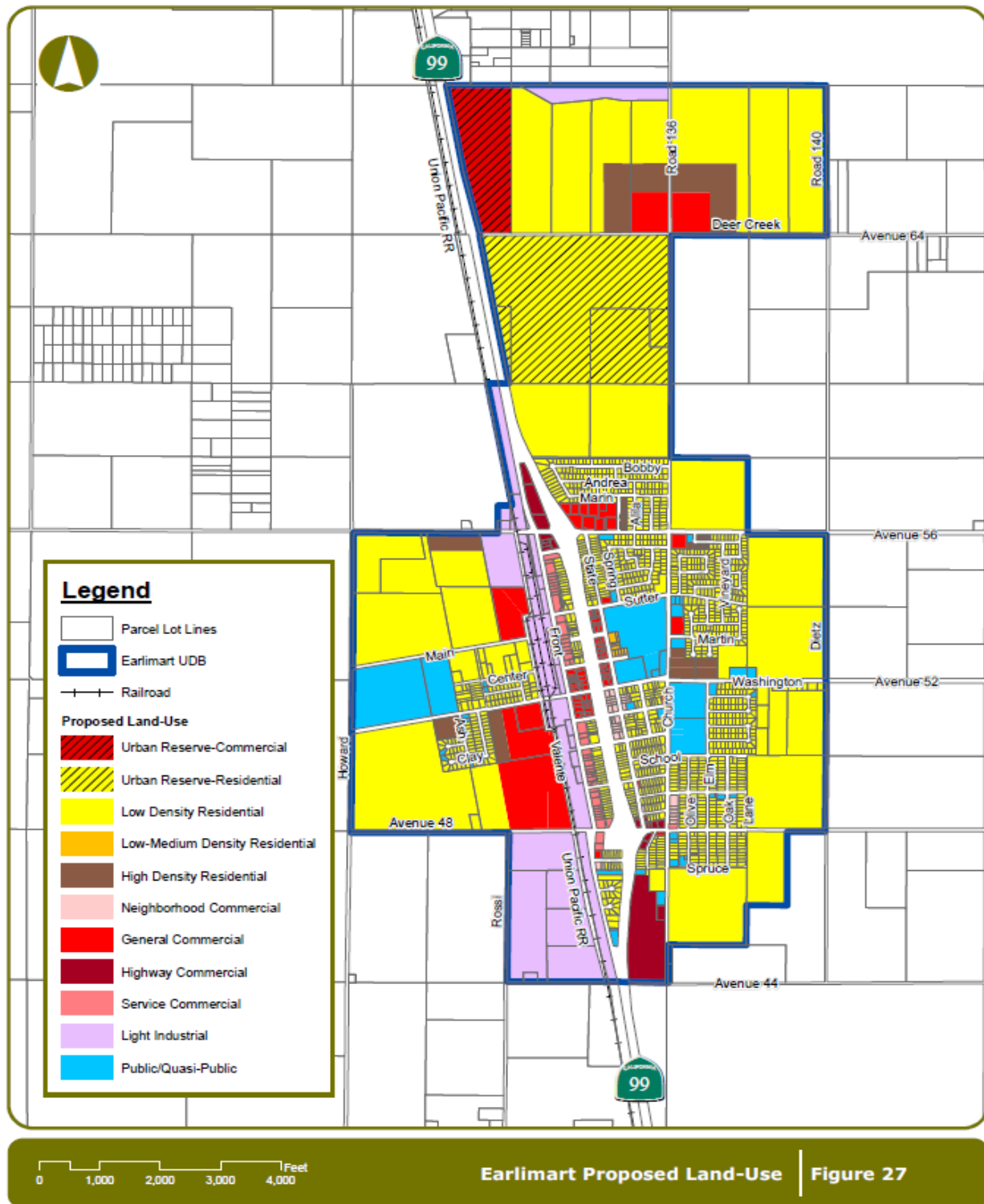
As suggested above and based on the forecasted growth and the recommended Urban Development Boundary, on the Economic Development/Market Analysis and Opportunities and Constraints Analysis, the below land use plan (see Table 60 and Figure 27) and zoning district (see Table 61 and Figure 28) has been updated.

Table 59 - Proposed Land Use	
Land Use Designation	Acres
General Commercial	98.99
High Density Residential	65.09
Highway Commercial	31.95
Light Industrial	166.4
Low Density Residential	999.4
Low-Medium Density Residential	1.85
Neighborhood Commercial	5.88
Public/Quasi-Public	97.23
Service Commercial	14.45
Urban Reserve-Commercial	49.09
Urban Reserve-Residential	173
Rights-of-Way	242.2
Total	1,945.6
Source: Tulare County GIS	

Table 60 - Proposed Zoning Districts	
Zoning Districts	Acres
AE-40	794.1
C-1	5.84
C-1-MU	0.46
C-2	43.41
C-2-MU	46.28
C-3	0.29
C-3-MU	22.87
M-1	75.38
P-O	84.1
R-1	544.9
R-1-MU	23.81
R-2	1.85
R-3	59.69
R-3-MU	0.31
Rights-of-Way	242.2
Total	1,945.6
Source: Tulare County GIS	

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Figure 27 - Proposed Land Use Plan



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The proposed Zoning Districts Map (see **Figure 28**) for Earlimart is compatible to the Land Use Map outlined in the General Plan. Zoning changes that need to occur to allow the General Plan and Zoning Ordinance to be in conformity with each other.

Proposed Zoning Districts

The **MU (Mixed Use) Overlay Combining** Zone allows a mix of uses that promotes flexibility in the types of entitlements that can be issued. All uses outlined in the M-1, C-3, C-2, C-3, C-1, R-1, R-2, and R-3 uses are allowed.

The **R-1 (One Family)** Zone allows One-family dwellings of a permanent character placed in permanent locations and one-family manufactured homes installed on a foundation system pursuant to Section 18551 of the California Health and Safety Code which comply with Subsection G of this Section. Private garages to accommodate not more than three (3) cars.

The **R-2 (Two Family)** Zone allows any use permitted in the R-1 Zone, two-family dwellings, multiple dwellings subject to site review, and incidental and accessory uses to the aforesaid.

The **R-3 (Multiple-Family)** Zone is any use permitted in the R-2 Two-Family Zone. Multiple dwellings; provided, however, that if more than four (4) dwelling units are proposed to be constructed on one (1) lot, the construction of such units shall be subject to approval of a site plan pursuant to the procedure set forth in Paragraph 1 of Subsection G of Section 16.2 of this Ordinance.

The **C-1 (Neighborhood Commercial)** Zone is intended for retail stores and personal service businesses which are appropriately located in close proximity to residential areas, while minimizing the undesirable impact of such uses in the neighborhoods which they serve.

The **C-2 (General Commercial)** Zone is intended for retail stores and businesses which do not involve manufacturing and/or processing.

The **C-3 (Service Commercial)** Zone is intended for wholesale establishments and establishments engaged in repairing and servicing equipment, materials and products, but which do not involve the manufacturing, assembling, packaging or processing of articles of merchandise for distribution and retail sales.

The **PO (Professional and Administrative Office)** Zone is any use permitted in the R-3, Multiple-Family Zone. Apothecary (limited to the sale of pharmaceuticals, medical and hygienic supplies and packaged confections). Clinic, dental or medical. Laboratory, dental or medical. Museum. Mortuary. Business and Professional office. Optician. Optometrist. Parking Lot: no servicing, repair, dismantling or commercial freight handling will be permitted. Post Office. Telephone exchange. Incidental and accessory structures and uses located on the same site with and necessary for the operation of a permitted use.

The **M-1 (Light Manufacturing)** Zone is intended for establishments engaged in the manufacturing, assembling, packaging, treatment and processing of products other than those which may be obnoxious or offensive by reason of emission of odor, dust, smoke, gas, noise, or other similar causes.

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Mixed Use - Any combination of retail/commercial, service, office, residential, hotel, or other use in the same building or on the same site typically configured in one (1) of the following ways:

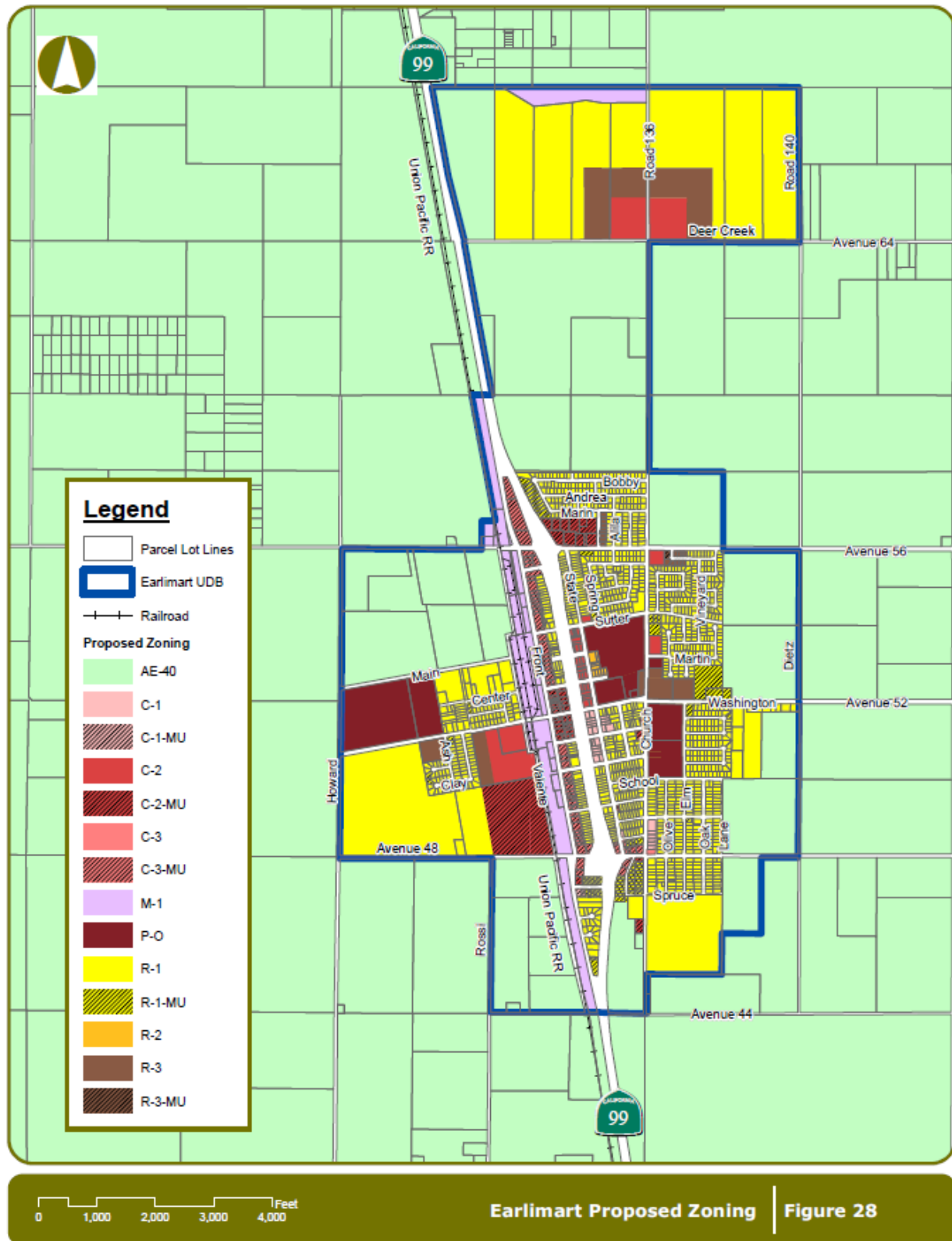
- **Vertical Mixed Use.** A single structure with the above floors used for residential or office use and a portion of the ground floor for retail/commercial or service uses.
- **Horizontal Mixed Use – Attached.** A single structure which provides retail/commercial or service use in the portion fronting the public or private street with attached residential or office uses behind.
- **Horizontal Mixed Use – Detached.** Two (2) or more structures on one (1) site which provide retail/commercial or service uses in the structure(s) fronting the public or private street, and residential or office uses in separate structure(s) behind or to the side.

Mixed Use allows for a variety of development projects. By allowing the community of Earlimart to respond to market forces, more opportunities are created for economic development and job development.

The **AE-40 (Exclusive Agricultural Zone 40 Acre Minimum)** The AE-40 Zone is an exclusive zone for intensive and extensive agricultural uses and for those uses which are a necessary and integral part of intensive and extensive agricultural operations.

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Figure 28 - Proposed Zoning Districts



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CIRCULATION ELEMENT

The purpose of this Circulation Element Update for the community of Earlimart is to provide for a safe, convenient and efficient transportation system. The Circulation Element has been designed to accommodate anticipated transportation needs based on the land use element. In compliance with state law, all city and county general plans must contain a circulation element that designates future road improvements and extensions, addresses non-motorized transportation alternatives, and identifies funding options. The intent of this Circulation Element is to:

- Identify transportation needs and issues within Earlimart, as well as regional relationships that affect the transportation system;
- Consider alternatives to the single-occupant vehicle as a means of providing services and access to facilities; and
- Establish policies that coordinate the Earlimart transportation and circulation system with the General Plan and area plan land use maps, and provide direction for future decision-making.

The proposed Circulation Plan (**see Figure 29**) and the changes to the circulation policies below are consistent with the Complete Streets Program for Earlimart (Adopted February 28, 2017).

Regional Transportation Planning

Tulare County Association of Governments (TCAG) Regional Transportation Plan

The Regional Transportation Plan (RTP) is a multi-modal, long-range planning document prepared by the Tulare County Association of Governments (TCAG). The RTP includes programs and policies for congestion management, transit, bicycles and pedestrians, roadways, freight, and finances for Tulare County. The RTP is prepared every four years and contains a listing of projects considered to be financially feasible within a 25-year planning time frame. All federally funded transportation projects must be consistent with the RTP.

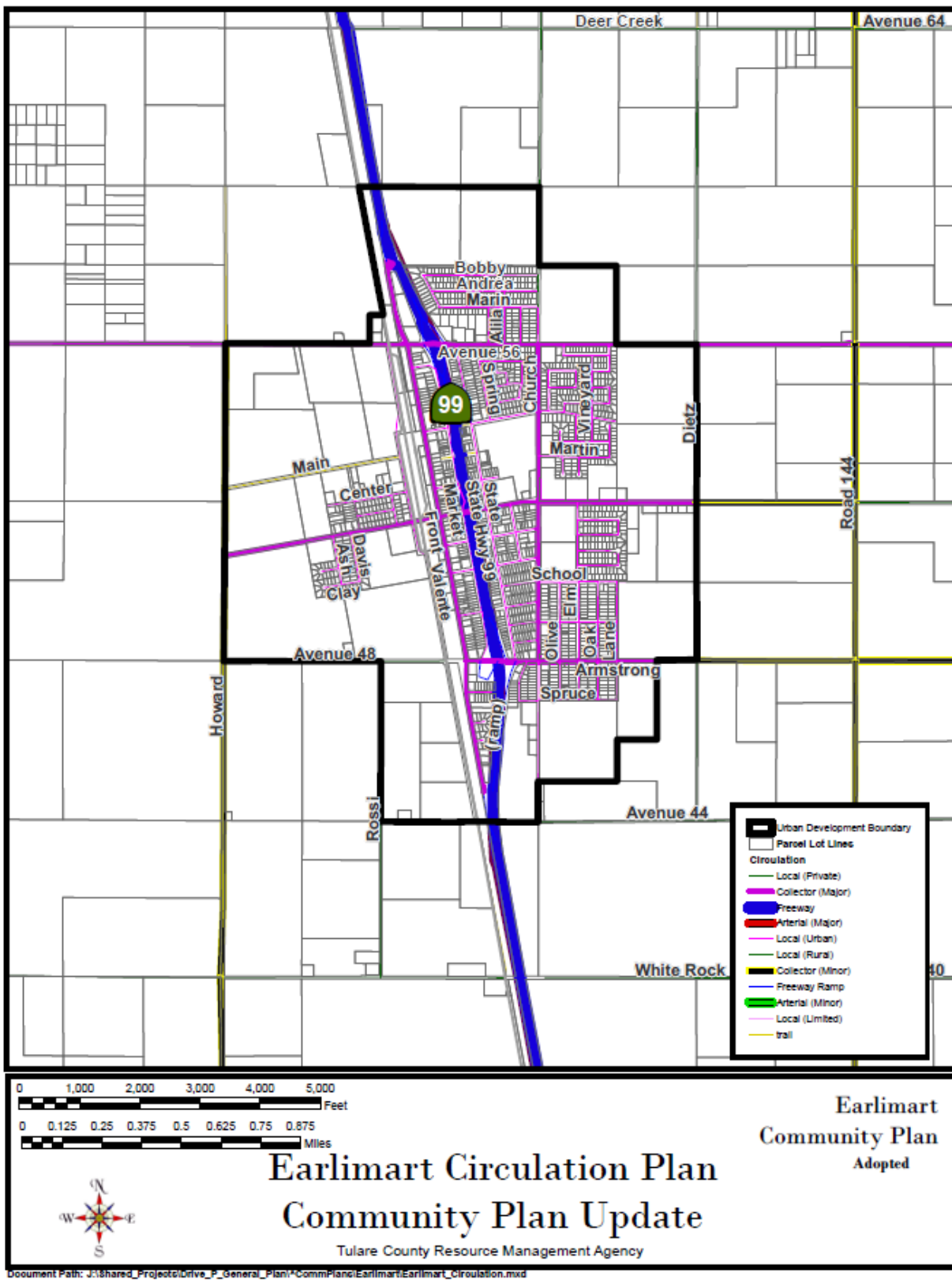
The RTP for Tulare is currently being updated and was adopted in July 2014. The RTP is the first to respond to state legislation (SB 375) that requires that the RTP show reductions in greenhouse gas emissions from passenger vehicles. Thus, there is an emphasis in the RTP on promoting ridesharing (transit, van and carpools) and active transportation (walking and bicycling). To this end, the RTP now includes a Sustainable Communities Strategy (SCS), a blueprint for land use patterns and transportation facilities and services that will facilitate fewer vehicle trips and vehicle miles traveled.

San Joaquin Valley Air Quality Management Plan

The San Joaquin Valley Air Pollution Control District (SJVAPCD) has prepared the Air Quality Management Plan (AQMP) and various other regulations to reduce air emissions. Both the plan and several regulations aim to reduce emissions from mobile sources – automobiles and trucks, as well as other modes of transportation.

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Figure 29 - Earlimart Circulation Plan



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Measure R ½ Percent Sales Tax for Transportation

Measure R is the half-percent sales tax measure for transportation improvements passed by the voters of Tulare County in 2006 and managed by the Tulare County Transportation Authority (TCTA). The Measure provides funding for transportation projects (highway, transit, and ridesharing) over the 20-year duration of the Measure. Measure R funds are used by the County in Earlimart to repair streets, and to improve the existing and planned transportation system.

Intelligent Transportation Systems (ITS) Planning

ITS Strategic Plan that may also consider countywide goals and policies to use communication and information technologies to improve mobility and enhance safety within the region. Potential ITS components include Freeway Management; Transit Management; Incident Management; Electronic Fare Payment; Electronic Toll Collection; Railroad Grade Crossings; Emergency Management Services; and Regional Multimodal Traveler Information. Being part of the ITS plan will assist the County with application for federal or State funding for specific types of ITS projects.

Public Transit and Active Transport Systems

While the private automobile is the dominant mode of travel within Earlimart, as it is throughout Tulare County, other modes of transportation are important. The Census bureau does not collect data on non-work trips, which represent a greater share of travel than work trips, but tend to be less concentrated in peak traffic periods. Off-peak trips also tend to have a greater proportion of shared ride and active (walk and bike) trips. While congestion is not a major issue in Earlimart, overreliance on automobiles creates other costs for both society and households, and means that many in the community who cannot drive (the young, the old, the disabled, the poor) must rely on those who can drive for their mobility. For this reason, it is important to encourage public transit systems and increased use of active modes of transportation, including bicycles and walking. The public transit system alternatives for Earlimart include fixed route public transit systems, common bus carriers, and other local agency transit and paratransit services.

Paratransit services are transportation services such as carpooling, vanpooling, taxi service, and dial-a-ride programs. The County supports reliable and efficient paratransit service by encouraging development of service systems that satisfy the transit needs of the elderly and physically handicapped. In addition the Dial-A-Ride provides same day service to the general public (i.e., non-ADA-certified) passengers based on space availability. Services are operated on weekdays from 6:00 am - 9:30 pm and on weekends from 8:00 am 6:30 pm. Orange Belt Stages also serves this location with one daily service eastbound to Las Vegas, and one westbound service to Hanford where connections can be made to San Luis Obispo.

Goods Movement

The ability of Tulare County to compete domestically and internationally on an economic basis requires an efficient and cost-effective method for distributing and receiving products. Earlimart is a part of this system with its proximity to both SR 99 and the Union Pacific Railroad mainline.

As industrial and economic growth is anticipated in Earlimart, industrial-related truck traffic will increase. Statewide, over three-quarters of all freight is shipped by truck. It is anticipated that the region's truck volumes will grow faster than auto traffic through 2040.

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Designated truck routes are intended to be used for long-distance truck movement. Truck movements for local deliveries within a community may use the most direct route to the delivery location, including local streets.

Air cargo is a growing method of transporting goods in and out of the Central Valley and is expected to continue to increase. As noted above, Fresno Yosemite International Airport is the major cargo-handling airport in the San Joaquin Valley.

The Union Pacific Railroad provides freight service, connecting Earlimart with major markets in northern and southern California and beyond. Rail can be the most cost-effective mode for long-haul traffic traveling to or from destinations beyond the Valley. Trucking is still likely to be the predominant mode for freight movements within the County and Valley for the foreseeable future.

Transportation Demand Management

Transportation demand management (TDM) strategies reduce dependence on the single-occupant vehicle, increase the ability of the existing transportation system to carry more people, and enhance mobility in the increasingly congested SR 99 corridor. Examples of TDM strategies include telecommuting, flexible work hours, and electronic commerce that enable people to work and shop from home. According to Caltrans, the major vanpool broker in the Valley, vanpools are becoming more prevalent for short-to-medium range commute trips, as well as for traditional long-distance usage: Key vanpool users include agricultural workers, and employees at large firms and government agencies. Park-n-ride facilities and carpooling will also continue to be a significant link between highway and transit modes.”⁷²

⁷² Earlimart Community Plan Traffic Impact Assessment and Circulation Plan, Pages 5 and 6. Prepared by VRPA Technologies (included as Appendix “E” of this DEIR).

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GOALS, POLICIES, AND STANDARDS

The intent of the Earlimart Community Circulation Element is to establish a comprehensive multi-modal transportation system that is efficient, environmentally and financially sound, and coordinated with the Land Use Element.

Goal 1: Design and implement a multi-modal transportation system that will serve projected future travel demand, minimize congestion, and address future growth in Earlimart.

Policies and Standards:

1. Utilize existing infrastructure and utilities to the maximum extent practical and provide for the logical, timely, and economically efficient extension of infrastructure and services.
2. Designate streets according to the following functional classifications:
 - a) Freeways and Expressways carry regional traffic through the community with access only at interchanges with major streets.
 - b) Arterials serve as the principal network for cross-town traffic flow. They connect areas of major traffic generation within the urban area and connect with important county roads and state highways. They also provide for the distribution and collection of through traffic to and from collector and local streets.
 - c) Collectors provide for traffic movement between arterial and local streets, traffic movement within and between neighborhoods and major activity centers, and limited direct access to abutting properties.
 - d) Local streets provide for direct access to abutting properties and for very localized traffic movements within residential, commercial and industrial areas.
 - e) All facility-types above (except freeways) should be capable of accommodating transit and paratransit vehicles. Furthermore, all facility-types except freeway should include provisions for active modes of transportation (walking and cycling).
3. Develop and apply consistent standards for new streets (and existing streets where feasible without substantial ROW takes) based on the roadway classification.
4. Require applicants for new development projects to dedicate needed ROW and construct and/or upgrade to County standards the streets and roads that will serve their projects.
5. Plan new arterial and collector streets as needed to improve access and enhance the develop potential of land designated for commercial and industrial uses.
6. Improvement standards for local and minor streets shall include perpendicular curbs, gutters and adequate street lighting at intersections.
7. Access to arterials by driveways, local and minor streets, and alleys should be controlled as needed in order to ensure efficient traffic flow and safety along these streets.
8. Local streets should be designed to discourage high traffic volumes and through traffic.
9. Develop a Circulation Map showing the public street system. Designated streets and recommended rights-of-way should be indicated on this map.
10. Allow standards for new street development to be altered or refined where it can be demonstrated that projected traffic flows can be accommodated.
11. Plan for peak-hour Level of Service (LOS) "D" or better throughout the circulation network.
12. Make intersection improvements to the existing major street system selectively, favoring traffic engineering solutions rather than major structural improvements. This could include signalization,

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intersection channelization, use of directional signs, and diversion of traffic onto underutilized streets.

13. Use complete streets concepts in the design of new local streets where such techniques will improve safety and manage traffic flow.
14. Ensure the street network provides efficient routes for emergency vehicles, meeting necessary street widths, turn around radius, and other factors as determined by the County in consultation with fire and other emergency service providers.
15. Cooperate with local, regional, State and federal agencies to plan for, establish and maintain good connectivity to an efficient multimodal regional transportation system.

Goal 2: Provide designated routes and loading standards that reduce the noise and safety concerns associated with truck traffic.

Policies and Standards:

1. Design interior street systems for commercial and industrial subdivisions to accommodate the movement of heavy trucks.
2. Restrict heavy duty truck through-traffic in residential areas and plan land uses so that trucks do not need to access these areas.
3. Design off-street loading facilities for all new commercial and industrial developments so that they do not face surrounding roadways or residential neighborhoods. Truck backing and maneuvering to access loading areas shall not be permitted on the public road system, except when specifically permitted by the County Engineer.

Goal 3: Provide safe and convenient pedestrian access between residential neighborhoods, parks, open space, and schools that service those neighborhoods.

Policies and Standards:

1. Provide a safe walking environment for pedestrians.
 - a) New development should include safe and pleasant designs which promote pedestrian access to arterials and collectors and consider the location of community services, such as schools, parks and neighborhood shopping activity centers in the accessibility of their design for all persons.
 - b) Require the installation of sidewalks as an integral part of all street construction where appropriate.
 - c) Require street lighting within the rights-of-way of all public streets.
 - d) Include pedestrian signal indicators as an integral part of the installation of traffic signals.
2. Maximize visibility and access for pedestrians and encourage the removal of barriers (walls, easements, and fences) for safe and convenient movement of pedestrians. Special emphasis should be placed on the needs of disabled persons considering ADA regulations.
3. Plan for pedestrian access consistent with road design standards while designing street and road projects. Provisions for pedestrian paths or sidewalks and timing of traffic signals to allow safe pedestrian street crossing shall be included.
4. Collaborate with the Earlimart schools and the School District to ensure that school children have adequate transportation routes available, such as a local pedestrian or bike paths, or local bus service.

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5. Encourage safe pedestrian walkways within commercial, office, industrial, residential, and recreational developments that comply with the Americans with Disabilities Act (ADA) requirements.
6. Coordinate with TCaT and private bus operators to ensure that pedestrian facilities are provided along and/or near transit routes, whenever feasible. New land developments may be required to provide pedestrian facilities due to existing or future planned transit routes even if demand for a pedestrian facility is not otherwise warranted.
7. Review all existing roadways without pedestrian facilities when they are considered for improvements (whether maintenance or upgrade) to determine if new pedestrian facilities are warranted. New roadways should also be assessed for pedestrian facilities.

Goal 4: Ensure the provision of adequate off-street parking for all land uses.

Policies and Standards:

1. Require all new development to identify adequate on-street and off-street parking based on expected parking needs.
2. Encourage shared parking among nearby uses with complementary parking demand patterns.
3. Provide adequate loading areas within off-street parking areas for all commercial and manufacturing land uses.
4. Anticipate parking needs at proposed and expected activity centers, particularly commercial areas.

Goal 5: Provide a transportation system that is integrated with the region.

Policies and Standards:

1. Coordinate local transportation planning with the TCAG Congestion Management Plan to ensure eligibility for state and federal funding.
2. Incorporate the Regional Transportation Plan, short- and long-range plans, and the Tulare County Short- and Long-Range Transit Plans into the Community Plan Circulation Element, and encourage the active participation of Caltrans in the design of highway capital improvement projects.

Goal 6: Encourage the use of public transit services to reduce reliance on the automobile.

Policies and Standards:

1. Encourage transit alternatives to meet the basic transportation needs of the young, the elderly, the handicapped, and people without access to an automobile.
 - a) Consider development of an integrated transit center within Earlimart where all transit services can connect with each other as well as with private ridesharing.
 - b) Encourage and provide for ridesharing, park and ride, and other programs that can reduce emissions, save energy, and reduce monetary costs for firms and workers.
2. Planning and development of arterial and collector streets shall include design features which can be used a future public transit stops.
3. Support the expansion and improvement of transit systems and ride sharing programs to reduce the production of automobile emissions.
4. Support the use of alternate fuel vehicles and fueling stations for public transit vehicles, and County public agency vehicles.

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5. Support TCaT and other transit operators' programs to foster transit usage.
7. Support all operator efforts to maximize revenue sources for short and long range transit needs that utilize all funding mechanisms available including federal grants, state enabling legislation, and farebox revenue. This can be accomplished through TCAG and the Tulare County Transit Agency (TCaT) through the development of the Short and Long Range Transit Plans.
8. Support programs developed by transit agencies/operators to provide paratransit service.
9. Incorporate the potential for public transit service in the design of developments identified as major trip attractions (i.e. community centers and employment centers).
10. Explore potential development of a park-n-ride lot in Earlimart.
11. Support continued improvements to AMTRAK rail passenger service within Tulare County and throughout the San Joaquin Valley.

Goal 7: Provide efficient goods movement.

Policies and Standards:

1. Encourage the efficient movement of goods and people by rail through a shift of a portion of the goods previously moved by trucks onto the rail freight system.
2. Implement Street and highway projects to provide convenient and economical goods movement, including access to rail terminals, in areas where large concentrations of truck traffic exist.
3. Identify street and highway improvement and maintenance projects that will improve goods movement and implement projects that are economically feasible.
4. Encourage use of rail for goods movement whenever feasible.

Goal 8: Provide safe and convenient facilities for non-motorized modes of transportation that enhance the future livability and character of Earlimart.

Policies and Standards:

1. Consider developing a Bikeway plan for Earlimart based on the following facility designations:
 - a) Bike Path (Class I). A special pathway for the exclusive use of bicycles, which is separated from motor vehicle facilities by space or a physical barrier. It is identified by guide signing and pavement markings.
 - b) Bike Lane (Class II). A lane on the paved area of a road for preferential use by bicycles. It is usually located along the right edge of the paved area or between the parking lane and the first motor vehicle lane. It is identified by a "Bike Lane" guide sign, special lane lines, and other pavement markings.
 - c) Bike Route (Class III). A recommended route for bicycle travel along an existing right-of-way, which is signed but not striped.
 - d) Bikeway. All facilities that explicitly provide for bicycle travel. The bikeway can be anything from a separate facility to a simple signed street.
2. Give priority to bikeways that will serve the highest concentration of cyclists and destination areas of highest demand, especially Earlimart Elementary School.
3. Provide bikeways in proximity to major traffic generators such as commercial centers, schools, recreational areas, and major public facilities.
4. Develop a visually clear, simple, and consistent bicycle system with standard signs and markings, as designated by the State of California Traffic Control Devices Committee and the State Bikeway Committee.

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5. Support the installation of bike parking racks at public and private places of assembly such as parks, schools, employment sites, churches, and retail commercial developments.
6. Provide non-motorized alternatives for commuter travel as well as recreational opportunities.
7. Provide separate rights-of-way for non-motorized facilities whenever economically and physically feasible.
8. Develop bikeways in compliance with the standards established in the Caltrans Highway Design Manual or other appropriate standards.

Goal 9: Design, construct, and operate the transportation system in a manner that maintains a High level of environmental quality.

Policies and Standards:

1. Control dust and mitigate other environmental impacts during all stages of roadway construction.
2. Protect residents from transportation generated noise hazards. Increased setbacks, walls, landscaped berms, other sound absorbing barriers, or a combination thereof shall be provided along four lane highways in order to protect adjacent noise-sensitive land uses from traffic generated noise impacts. Additionally, noise generators such as commercial, manufacturing, and/or industrial activities shall use these techniques to mitigate exterior noise levels to no more than 60 decibels.
3. Review and monitor proposals for expansion of pipelines for the transport of suitable products and materials, and require mitigation of environmental impacts.
4. Encourage the use of non-polluting vehicles for both public and private uses.
5. Include noise mitigation measures in the design of roadway projects in Earlimart.

Goal 10: Support the use of Transportation Demand Management (TDM) strategies to reduce dependence on the single-occupant vehicle, increase the ability of the existing transportation system to carry more people, and enhance mobility along congested corridors.

Policies and Standards:

1. New development shall consider Transportation System Management and Transportation Demand Management as strategies for the mitigation of traffic and parking congestion. Public transit, traffic management, ride sharing and parking management are to be used to the greatest extent practical to implement transportation management strategies.
2. Coordinate with Caltrans, TCAG, transit agencies and other responsible agencies to identify the need for additional park-n-ride facilities along major commuter travel corridors.

Goal 11: Utilize Intelligent Transportation Systems (ITS) to improve the safety and performance of the surface transportation system using new technology in detection, communication, computing, and traffic control.

Policies and Standards:

1. Encourage the integration of Intelligent Transportation Systems (ITS) consistent with the principles and recommendations referenced in the TCAG Regional Transportation Plan

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IMPLEMENTATION STRATEGY

The purpose of this chapter is to prescribe a proposed approach to implement the general plan recommendations contained in Chapters I through IV of the Earlimart Community Plan. There are several components that comprise the Earlimart Community Plan implementation strategy:

1. Zoning Code Changes
2. Complete Streets
3. Infrastructure

1. ZONING CODE CHANGES

Zoning District Changes

As part of this Implementation Program for the Community Plan for Earlimart, there are a variety of changes to existing zoning districts.

Proposed Zoning Districts

The MU (Mixed Use) Overlay Combining Zone allows a mix of uses that promotes flexibility in the types of entitlements that can be issued. All uses outlined in the M-1, C-3, C-2, C-3, C-1, R-1, R-2, and R-3 uses are allowed as discussed above.

The R-1 (One Family) Zone

The R-2 (Two Family) Zone

The R-3 (Multiple-Family Zone)

The C-1 (Neighborhood Commercial Zone)

The C-2 (General Commercial Zone)

The C-3 (Service Commercial Zone)

The PO (Professional and Administrative Office Zone)

The M-1 (Light Manufacturing Zone)

The AE-40 (Exclusive Agricultural 40-Acre Minimum Zone)

Revise Chapter 16 of the Zoning Code to limit the uses that require a Use Permit

As part the Economic Development Strategy, use permit requirements are streamlined to allow for uses to be developed without discretionary review. That is, proposed uses will not have to undergo an approval process that involves a decision making action by the Tulare County Planning Commission or Board of Supervisors. Project design features and Administrative approval will serve

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as the mechanism to allow (regulate) land uses, activities, densities, and other conditions typically applied through the special use permit process.

Elimination of SR Combining Zone

The SR combining zone requires site plan review for most uses in the combining zone. The primary purpose of the SR Combining zone is to ensure that proposed projects are designed to avoid conflicts with the existing right of way, setbacks and easements of public agencies and special districts. This alternative would eliminate the SR combining zone designation in the community of Earlimart.

Mixed Use Overlay

This alternative involves the creation of a Mixed-Use Zoning Overlay Designation for the community of Earlimart (See A-2 & A-3).

2. COMPLETE STREETS

The Complete Streets Programs Policies, Objectives, and Standards are hereby incorporated by reference. Included in the plan are policies and implementation measures as provided. The physical plan includes a bicycle network and connected pedestrian travel system incorporating complete safe routes to school network (**see Table 61**).

The Board of Supervisors on February 28, 2017, approved inclusion of the Complete Streets Program as part of the Circulation Element of the Community Plan (**see Attachment A-6**). The Complete Streets Programs Policies, Objectives, and Standards are incorporated by reference.

Table 61 - Proposed Complete Streets Projects for Earlimart					
Segment	Sidewalk	Curb & Gutter	Drainage	Lighting	Measure R Complete Streets
State Street: Avenue 56 (Sierra) to Avenue 48 (Armstrong)	X	X	X	X	X
Washington Avenue: Road 128 (Howard Rd) to State Street	X	X	X	X	X
Washington Avenue: State Street to east of Elm	X	X	X	X	X
Church Street: Armstrong Avenue to Sierra Avenue (56)	X	X	X	X	X
Source: Tulare County Complete Streets - Earlimart					

Bicycle Facilities

In Earlimart, Class I/II/III facilities are envisioned to be implemented along the major circulation segments of roadway that connect the overall County roadway network. This includes proposed Class III along parts of Washington Avenue, State Street, and Church Street (**see Figure 30**).

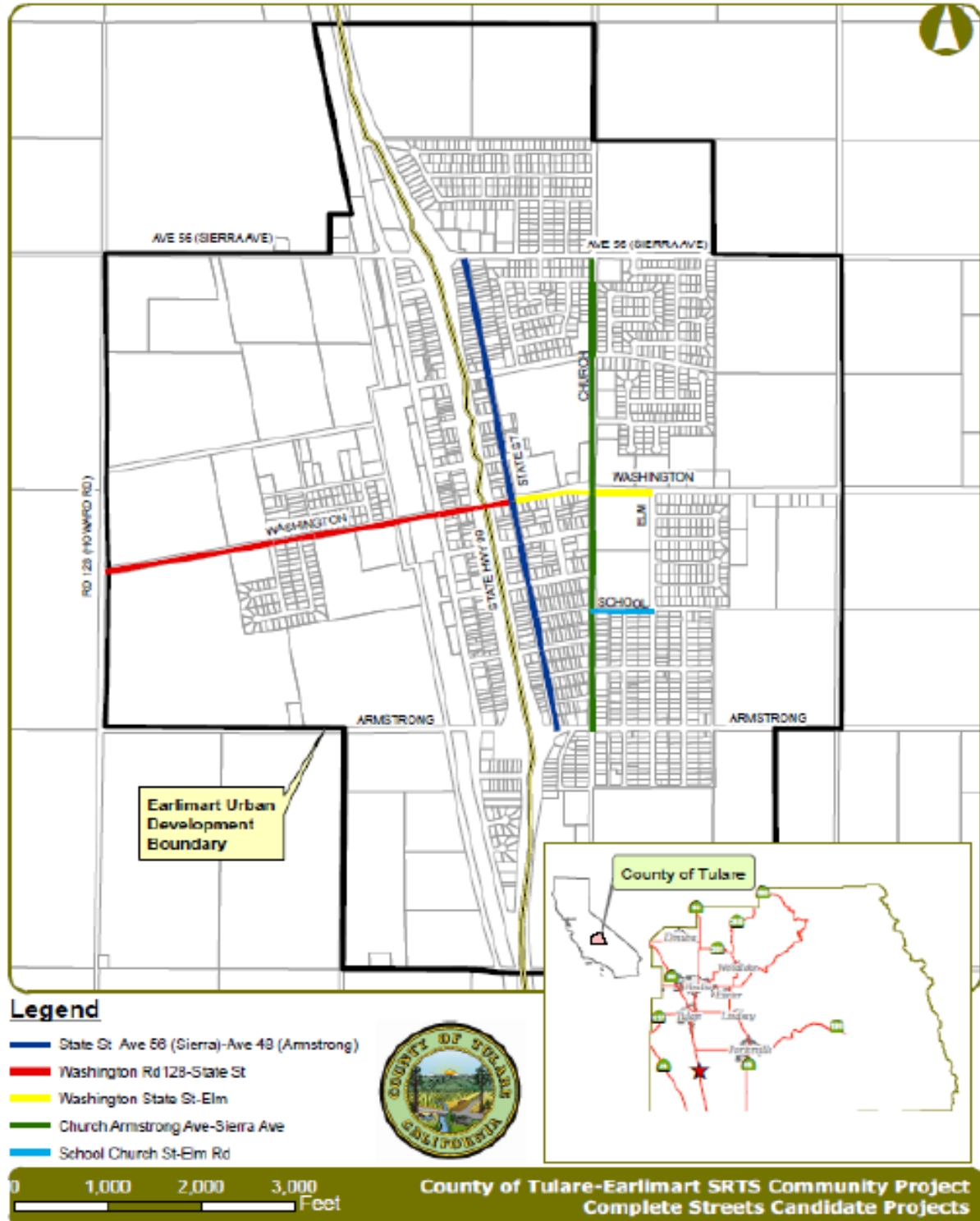
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Pedestrian Paths and Sidewalks

Pedestrian paths are primarily developed as part of the roadway and trail systems of a community and reflect the interconnected nature of circulation and transportation systems as a whole. Constructing wide streets increases the distance a pedestrian must travel to cross a street, thereby making it inconvenient for public use and inhibiting pedestrian circulation in the Community. Currently, limited continuous sidewalks are provided along major routes in the Community. In addition to connecting available pedestrian resources, the communities have prioritized the completion of sidewalks along safe routes to school. Along Washington Avenue enhanced pedestrian crossings and sidewalks are considered in areas where high pedestrian and school children demand occurs (such as to and around schools).

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Figure 30 - Earlimart Bicycle & Pedestrian Plan



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3. INFRASTRUCTURE

There currently is limited infrastructure capacity (water, waste water, and storm drainage) in the Community of Earlimart. The residents have concerns about safety issues due to street conditions and lighting, especially since resident trucks and tractors are driving through residential neighborhoods and parking on the streets within the neighborhoods.

These services are available more so in Earlimart than other communities within the County, and the conditions of the roads is monitored and maintained, as resources are available to do so. EPUD is currently potentially expanding their water capacity and is submitting a grant (as of November, 2017) to increase their sewer capacity. However, the updating of this Community Plan is the implementation strategy that is most likely to lead to infrastructure improvements and economic development within the Community by:

A. The updating of this Community Plan will allow for EPUD and the County to continue to submit grant applications for funding for infrastructure improvements, with more information available, and with environmental analysis completed under the California Environmental Quality Act. All of which should help with addressing the limited available infrastructure.

B. In addition, updating of this Community Plan will increase the potential for more amenities and increase efficiencies for entitlements. Both of which will improve the chances of development coming forward in Earlimart and paying for these improvements.

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ATTACHMENTS

- A-1 – Use Permit Requirement Changes (Zone Change Text)
- A-2 – Mixed Use Overlay District (Zone Change Text)
- A-3 – Development Standards (Mixed Use Zoning Districts)
- A-4 – General Plan Land Use and Zoning Consistency Matrix
 - A-5 – Description of Funding Sources
 - A-6 – Complete Streets

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A-1 Use Permit Requirement Changes (Zone Change Text)

SECTION 16: VARIANCES AND SPECIAL USE PERMITS

H. Additional By-Right Uses

All of the following, and all structures and accessory uses directly related thereto in this section are entitled without a Special Use Permit (Conditional Use Permit). The following is allowed only in the various zones indicated below and within a community plan that adopted development standards for such entitled use. The proposed use must adhere to the adopted development standards of the community. The proposed use must also qualify for an exemption under the California Environmental Quality Act as determined by the Permit Center. The Permit Center will review the project for General Plan Policy, Community Plan Policy and development standard consistency and determine which environmental document is appropriate. Projects where the Permit Center is unable to make an immediate determination will be required to go through the Project Review Committee (PRC).

Uses that have an environmental effect on adjacent properties or necessitate mitigation measures through the California Environmental Quality Act will be required to apply for a PRC and a traditional use permit and legislative process through the County. These uses may have environmental or land use issues that may not be compatible with adjacent uses. These impacts may include but are not limited to; hours of operation (night time), noise (i.e. power tools such as impact drivers, or loudspeaker, etc) air quality (idle running vehicles) traffic, (number of vehicles) and odor. The Permit Center process is to determine the whether the use is by right or must go through the traditional use permit process. The following uses and zones shall be considered:

Permitted Uses		
Zoning District Uses	New Entitled Zone	Prior Entitled Zone
Animal hospital, clinic, and veterinarian office wherein only small/domestic animals (i.e. dogs, cats, etc.) are treated. Structure < 10,000 sq. ft.	C-1, C-2, C-3, M-1, M-2	
Antique and art store. Structure < 10,000 sq. ft.	C-2, C-3, M-1, M-2, R-3	C-2, C-3, M-1
Antique store containing less than one thousand (1,000) square feet of floor area	C-1,C-2, C-3, M-1, R-2, R-3	C-1,C-2, C-3, M-1
Apartment Hotel Structure < 10,000 sq. ft.	O, CO, C-1,C-2, C-3, M-1, R-3	O
Apparel stores. Structure < 10,000 sq. ft.	CO, C-1,C-2, C-3, M-1, R-3	C-1, C-2, C-3, M-1
Arcades, including video. Structure < 10,000 sq. ft.	O, CO, C-1,C-2, C-3, M-1, R-3	C-2, C-3, M-1
Zoning District Uses	New Entitled Zone	Prior Entitled Zone
Assemblage of people for educational or entertainment purposes. Structure < 10,000 sq. ft.	O, CO, C-1,C-2, C-3, M-1, M-2	

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Assembly of electric appliances such as lighting fixtures, irons, fans, toasters and electric toys, refrigerators, washing machines, dryers, dishwashers and similar home appliances. Structure < 10,000 sq. ft.	C-2, C-3, M-1, M-2	M-1
Assembly of small electrical equipment such as home and television receivers. Structure < 10,000 sq. ft.	O, CO, C-1,C-2, C-3, M-1, R-3	M-1
Assembly of typewriters, business machines, computers, and similar mechanical equipment. Structure < 10,000 sq. ft..	O, CO, C-1,C-2, C-3, M-1, M-2, R-3	M-1
Automated car wash (coin operated only). Structure < 10,000 sq. ft.	O, CO, C-1,C-2, C-3, M-1,M-2, R-3, AP	C-2,C-3, M-1
Automobile parking lots, public parking areas or storage garages. Structure < 10,000 sq. ft.	O, CO, C-1,C-2, C-3, M-1, M-2, R-3, AP	
Automobile supply stores.	O, CO, C-1,C-2, C-3, M-1, R-3	C-2, C-3, M-1
Automobile washing, including the use of mechanical conveyors, blowers and steam cleaning.	C-2, C-3, M-1, M-2	C-3, M-1
Bakery [employing not more than five (5) persons on premises].	O, CO, C-1,C-2, C-3, M-1, R-3	C-1,C-2, C-3, M-1
Bakery goods store.	CO, C-1,C-2, C-3, M-1	C-2,C-3, M-1
Banks and financial institutions.	C-1,C-2, C-3, M-1, R-3	C-1,C-2, C-3, M-1
Barber shop or beauty parlor.	C-1,C-2, C-3, M-1, R-3	C-1,C-2, C-3, M-1
Bed and Breakfast Home with three or more guests rooms (Up to 5). Structure < 10,000 sq. ft.	CO, C-1,C-2, C-3, M-1, R-3, R-2	R-1
Zoning District Uses	New Entitled Zone	Prior Entitled Zone
Bicycle shops. Structure.	CO, C-1,C-2, C-3, M-1, R-3	C-2, C-3, M-1
Billiard or Pool hall Structure	C-2, C-3, M-1	C-2, C-3, M-1
Bird store or pet shop.	O, CO, C-1,C-2, C-3, M-1	C-2,C-3, M-1
Blueprinting and Photostatting shop.	CO, C-1,C-2, C-3, M-1, AP	C-2, C-3, M-1
Boat sales and service. Structure < 10,000 sq. ft.	O, CO, C-1,C-2, C-3, M-1	C-3, M-1
Book binding. Structure < 10,000 sq. ft.	O, CO, C-1,C-2, C-3, M-1, R-3	C-3, M-1
Book or stationary store. Structure < 10,000 sq. ft.	O, CO, C-1,C-2, C-3, M-1, R-3	C-1,C-2, C-3, M-1
Business and professional schools and colleges. Structure < 10,000 sq. ft.	CO, C-1,C-2, C-3, M-1	C-2,C-3, M-1
Business, professional and trade schools and colleges. Structure < 10,000 sq. ft.	CO, C-1,C-2, C-3, M-1	C-3, M-1
Catering Shops. Structure < 10,000 sq. ft.	CO, C-1,C-2, C-3, M-1	C-2, C-3, M-1

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Ceramic shops. Structure < 10,000 sq. ft.	CO, C-1,C-2, C-3, M-1	C-2, C-3, M-1
Christmas tree sales lots as a temporary use.	CO, C-1,C-2, C-3, M-1	C-1, C-2, C-3, M-1
Church. Structure < 10,000 sq. ft.	CO, C-1,C-2, C-3, M-1	
Zoning District Uses	New Entitled Zone	Prior Entitled Zone
Clothes cleaning and pressing establishment. Structure < 10,000 sq. ft.	CO, C-1,C-2, C-3, M-1	C-1,C-2, C-3, M-1
Clothing and costume rental. Structure < 10,000 sq. ft.	CO, C-1,C-2, C-3, M-1	C-2, C-3, M-1
Confectionery store. Structure < 10,000 sq. ft.	CO, C-1,C-2, C-3, M-1	C-1,C-2, C-3, M-1
Conservatory of Music. Structure < 10,000 sq. ft.	CO, C-1,C-2, C-3, M-1	C-2, C-3, M-1
Contractor's Storage Yards.	CO, C-1,C-2, C-3, M-1, AP	
Dairy products store. Structure < 10,000 sq. ft.	CO, C-1,C-2, C-3, M-1	C-1,C-2, C-3, M-1
Department store Structure < 10,000 sq. ft.	CO, C-1,C-2, C-3, M-1	C-1,C-2, C-3, M-1
Digesters	M-1	
Drug store or pharmacy. Structure < 10,000 sq. ft.	CO, C-1,C-2, C-3, M-1	C-1,C-2, C-3, M-1
Dry goods or notions store. Structure < 10,000 sq. ft.	CO, C-1,C-2, C-3, M-1	C-1,C-2, C-3, M-1
Electric appliance stores and repairs Structure < 10,000 sq. ft.	CO, C-1,C-2, C-3, M-1	C-2, C-3, M-1
Expansion, Alteration or Replacement of non-conforming buildings and uses. Structure < 10,000 sq. ft.	CO, C-1,C-2,M-1, R-1, R-2, R-3, R-A	
Family Day Care Home, Large (Up to CA State maximum).	CO, C-1,C-2, R-1, R-2, R-3, RA	
Zoning District Uses	New Entitled Zone	Prior Entitled Zone
Family Day Care Home, small.	CO, C-1,C-2, R-1, R-2, R-3, RA	R-1, R-2 R-3, C-1,C-2, C-3, M-1
Feed and seed stores. Structure < 10,000 sq. ft.	CO, C-1,C-2, C-3, M-1, AP	C-3, M-1
Fire Station.	CO, C-1,C-2, C-3, M-1, AP	
Firewood sales yard.	CO, C-1,C-2, C-3, M-1	C-3, M-1
Florist shop. Structure < 10,000 sq. ft.	CO, C-1,C-2,M-1, R-1, R-2, R-3, RA	C-1,C-2, C-3, M-1
Furniture store. Structure < 10,000 sq. ft.	CO, C-1,C-2, C-3, M-1	C-2, C-3, M-1
Furniture warehouses for storing personal household goods, provided ground floor front is devoted to stores. Structure < 10,000 sq. ft.	CO, C-1,C-2, C-3, M-1	C-2, C-3, M-1
Gasoline filling station. Structure < 10,000 sq. ft.	CO, C-1,C-2, C-3, M-1	C-1,C-2, C-3, M-1
Gift, novelty or souvenir. Structure < 10,000 sq. ft.	CO, C-1,C-2,M-1, R-2, R-3, RA	C-2, C-3, M-1

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Glass shop, retail, excluding major service activities. Structure < 10,000 sq. ft.	C-2, C-3, M-1	C-2, C-3, M-1
Grocery store. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	
Grocery store, fruit store or supermarket. Structure < 10,000 sq. ft.	C-1, C-2, C-3, M-1	C-1, C-2, C-3, M-1
Zoning District Uses	New Entitled Zone	Prior Entitled Zone
Gunsmith shops. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-3, M-1
Hobby and art supply store. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-2, C-3, M-1
Hospital, sanitarium and nursing home. Structure < 10,000 sq. ft.	C-1, C-2, C-3, M-1, PO	
Household and office equipment and machinery repair shops. Structure < 10,000 sq. ft.	C-2, C-3, M-1, PO	C-3, M-1
Household appliance stores. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-2, C-3, M-1
Ice storage house of not more than 5-ton storage capacity.	CO, C-1, C-2, C-3, M-1, AP	
Incidental manufacturing, processing and treatment of products. Structure < 10,000 sq. ft.	C-2, C-3, M-1	C-2, C-3, M-1
Interior decorating store. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-2, C-3, M-1
Jail or correctional (public facilities only).	C-2, C-3, M-1, M-2	
Jewelry store, including clock and watch repair. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-1, C-2, C-3, M-1
Laundries. Structure < 10,000 sq. ft.	C-2, C-3, M-1	C-3, M-1
Laundry, coin operated machines only. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-1, C-2, C-3, M-1
Leather goods and luggage stores. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-2, C-3, M-1
Linen supply services. Structure < 10,000 sq. ft.	C-2, C-3, M-1	C-3, M-1
Liquor store. Structure < 10,000 sq. ft. Not within 300' of residential/School Site.	CO, C-1, C-2, C-3, M-1	C-1, C-2, C-3, M-1
Zoning District Uses	New Entitled Zone	Prior Entitled Zone
Locksmiths. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-2, C-3, M-1
Massage or physiotherapy establishment Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-2, C-3, M-1
Meat market or delicatessen store. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1	C-1, C-2, C-3, M-1
Medical and orthopedic appliance stores. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1, PO	C-2, C-3, M-1
Medical laboratory. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1, PO	C-2, C-3, M-1
Memorial building, theatre, auditorium. Structure < 10,000 sq. ft.	CO, C-1, C-2, C-3, M-1, R-3	
Micro-brewery. Structure < 10,000 sq. ft. *Allowed in C-1 and C-2 in conjunction with a restaurant.	M-1, M-2, C-3, *C-2, *C-1	M-2
Mini-warehouses. Structure < 10,000 sq. ft.	C-2, C-3, M-1, AP	C-3, M-1

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Mobilehome for use by caretaker or night watchman.	CO, C-1,C-2, C-3, M-1	O, C-2, C-3, M-1
Motorcycle sales and service. Structure < 10,000 sq. ft.	C-2, C-3	C-3, M-1
Musical instrument repair shops. Structure < 10,000 sq. ft.	CO, C-1,C-2, C-3, M-1	C-3, M-1
Name plates. Structure < 10,000 sq. ft.	CO, C-1,C-2, C-3, M-1, R-3	R-3, C-1,C-2, C-3, M-1
Nursery school. Structure < 10,000 sq. ft.	CO, C-1,C-2, M-1, R-1, R-2, RA, R3, R-A	
Office, business or professional. Structure < 10,000 sq. ft.	CO, C-1,C-2, M-1, R-1, R-2, R-3,PO	C-1,C-2, C-3, M-1
Zoning District Uses	New Entitled Zone	Prior Entitled Zone
Opticians and optometrists shops. Structure < 10,000 sq. ft.	CO, C-1,C-2, C-3, M-1, PO	C-2, C-3, M-1
Paint and wallpaper stores. Structure < 10,000 sq. ft.	C-1, C-2, C-3, M-1	C-2, C-3, M-1
Pet shops. Structure < 10,000 sq. ft.	CO, C-1,C-2, C-3, M-1	C-2, C-3, M-1
Photo processing pick-up and delivery outlets. Structure < 10,000 sq. ft.	CO, C-1,C-2, C-3, M-1	C-1,C-2, C-3, M-1
Photographic and blueprint processing and printing. Structure < 10,000 sq. ft.	CO, C-1,C-2, C-3, M-1	C-3, M-1
Photographic developing and printing. Structure < 10,000 sq. ft.	CO, C-1,C-2, C-3, M-1	C-3, M-1
Photographic supply stores. Structure < 10,000 sq. ft.	CO, C-1,C-2, C-3, M-1	C-2, C-3, M-1
Picture framing shops. Structure < 10,000 sq. ft.	CO, C-1,C-2, C-3, M-1	C-2, C-3, M-1
Plumbing fixtures for retail sales. Structure < 10,000 sq. ft.	CO, C-1,C-2, C-3, M-1	C-2, C-3, M-1
Plumbing shops. Structure < 10,000 sq. ft.	C-2, C-3, M-1	C-3, M-1
Police station.	O,CO, C-1,C-2, C-3, M-1, M-2	
Post Office.	CO, C-1,C-2, C-3, M-1, R-1, R-2, R-3, PO	O, C-1,C-2, C-3, M-1
Pressing establishments. Structure < 10,000 sq. ft.	C-2, C-3, M-1	C-3, M-1
Zoning District Uses	New Entitled Zone	Prior Entitled Zone
Printing, lithography, engraving. Structure < 10,000 sq. ft.	CO, C-1,C-2, C-3, M-1	C-2, C-3, M-1
Private club, fraternity, sorority and lodge. Structure < 10,000 sq. ft.	CO, C-1,C-2, C-3, M-1	
Private greenhouses and horticultural collections. Structure < 10,000 sq. ft.	CO, C-1,C-2, C-3, M-1, R-1, R-2, R-3, R-A	R-1, R-2 R-3, C-1, C-2, C-3, M-1
Public library.	CO, C-1,C-2, C-3, M-1, R-1, R-2, R-3, R-A	R-3, C-1,C-2, C-3, M-1
Public Park or playground.	O, MR, CO, C-1,C-2, C-3, M-1, M-2 R-1, R-2, R-3, R-A, AP	

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Public utility structure.	CO, C-1,C-2, C-3, M-1, MR, RO, R-1, R-2, R-3, R-A, PO, O, AP	
Radio and television broadcasting studios. Structure < 10,000 sq. ft.	C-2, C-3, M-1	C-3, M-1
Radio and television repair shops. Structure < 10,000 sq. ft.	C-2, C-3, M-1	C-3, M-1
Radio, microwave and television towers (Over 75 feet or within 2 miles of an airport).	C-2, C-3, M-1, M-2	
Real Estate Offices. Structure < 10,000 sq. ft.	CO, C-1,C-2, C-3, M-1, R-1, R-2, R-3, R-A	
Recreation center. Structure < 10,000 sq. ft.	CO, C-1,C-2, C-3, M-1	
Repairing and altering of wearing apparel. Structure < 10,000 sq. ft.	CO, C-1,C-2, C-3, M-1	C-1,C-2, C-3, M-1
Resort Structure. < 10,000 sq. ft.	CO, C-1,C-2, C-3, M-1	
Zoning District Uses	New Entitled Zone	Prior Entitled Zone
Restaurant. Structure < 10,000 sq. ft.	CO, C-1,C-2, C-3, M-1	
Restaurant, tea room or cafe. Structure < 10,000 sq. ft.	CO, C-1,C-2, C-3, M-1, R-1, R-2, R-3, R-A	C-1,C-2, C-3, M-1
Retail office equipment sales. Structure < 10,000 sq. ft.	CO, C-1,C-2, C-3, M-1	C-2, C-3, M-1
Retail sales of sporting goods, boats, boat motors, boat trailers, trailer coaches and their repair, rental and storage. Structure < 10,000 sq. ft.	O, CO, C-1,C-2, C-3, M-1	
Retail stores and offices incidental to and located on the site of a hotel, motel, resort, restaurant or guest ranch. Structure < 10,000 sq. ft.	O, CO, C-1,C-2, C-3, M-1	
Rug and carpet cleaning and dyeing. Structure < 10,000 sq. ft.	CO, C-1,C-2, C-3, M-1	C-3, M-1
Satellite antenna sales. Structure < 10,000 sq. ft.	CO, C-1,C-2, C-3, M-1	C-2, C-3, M-1
Satellite television antennas.	CO, C-1,C-2, C-3, M-1	
School, private.	CO, C-1,C-2, C-3, M-1, PO	
School, public.	CO, C-1,C-2, C-3, M-1, PO	
Scientific instrument stores. Structure < 10,000 sq. ft.	CO, C-1,C-2, C-3, M-1	C-2, C-3, M-1
Secondhand stores, pawn shops and thrift shops. Structure < 10,000 sq. ft.	CO, C-1,C-2, C-3, M-1	C-2, C-3, M-1
Shoe repair shop. Structure < 10,000 sq. ft.	CO, C-1,C-2, C-3, M-1	C-1,C-2, C-3, M-1
Zoning District Uses	New Entitled Zone	Prior Entitled Zone
Shoe store. Structure < 10,000 sq. ft.	CO, C-1,C-2, C-3, M-1	C-1,C-2, C-3, M-1
Sign painting shops. Structure < 10,000 sq. ft.	C-2, C-3, M-1	C-3, M-1
Small appliance sales and service. Structure < 10,000 sq. ft.	CO, C-1,C-2, C-3, M-1	C-1,C-2, C-3, M-1
Soda fountains. Structure < 10,000 sq. ft.	CO, C-1,C-2, C-3, M-1	C-2, C-3, M-1
Sporting goods store. Structure < 10,000 sq. ft.	CO, C-1,C-2, C-3, M-1	C-1,C-2, C-3, M-1

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Stamp and coin stores. Structure < 10,000 sq. ft.	CO, C-1,C-2, C-3, M-1	C-2, C-3, M-1
Storage of petroleum products for use on the premises.	CO, C-1,C-2, C-3, M-1	
Studios (except motion picture). Structure < 10,000 sq. ft.	CO, C-1,C-2, C-3, M-1	C-2, C-3, M-1
Tinsmith Structure < 10,000 sq. ft.	C-2, C-3, M-1	C-2, C-3, M-1
Tire sales (no retreading or recapping). Structure < 10,000 sq. ft.	C-2, C-3	C-2, C-3, M-1
Tobacco and cigar stores. Structure < 10,000 sq. ft.	CO, C-1,C-2, C-3, M-1	C-2, C-3, M-1
Tourist Court. Structure < 10,000 sq. ft.	CO, C-1,C-2, C-3, M-1	
Toy store. Structure < 10,000 sq. ft.	CO, C-1,C-2, C-3, M1	C-2, C-3, M-1
Zoning District Uses	New Entitled Zone	Prior Entitled Zone
Trailer and recreation vehicle sales, service and rentals. Structure < 10,000 sq. ft.	C-2, C-3, M-1, M-2, AP	C-3, M-1
Travel agencies. Structure < 10,000 sq. ft.	CO, C-1,C-2, C-3, M-1	C-2, C-3, M-1
Variety store. Structure < 10,000 sq. ft.	CO, C-1,C-2, C-3, M-1	C-2, C-3, M-1
Video machine and tape sales/rental. Structure < 10,000 sq. ft.	CO, C-1,C-2, C-3, M-1	C-1,C-2, C-3, M-1
Warehouses except for the storage of fuel or flammable liquids and explosives. Structure < 10,000 sq. ft.	CO, C-1,C-2, C-3, M-1	C-3, M-1
Watch and clock repair shop. Structure < 10,000 sq. ft.	CO, C-1,C-2, C-3, M-1	C-2, C-3, M-1
Wedding chapel. Structure < 10,000 sq. ft.	CO, C-1,C-2, C-3, M-1	C-2, C-3, M-1

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A-2 – Mixed Use Overlay District (Zone Change Text)

Section 18.9: “MU” Mixed-Use Overlay Combining Zone

The following regulations shall apply in the community of Earlimart, unless otherwise provided in this Ordinance.

PURPOSE **A.** The purpose of this zone is to allow for mixed uses. Allowing a mix of uses promotes flexibility in the types of entitlements that can be issued. Economic Development can be pursued with a wide variety of development potential. In addition, mixed use can allow for decreased vehicles miles traveled if residential uses are mixed with uses for employment.

APPLICATION **B.** This overlay zone only applies to the community of Earlimart.

USE **C.** No building or land shall be used and no building shall be hereafter erected or structurally altered, except for one or more of the following uses allowed in this this overlay zone are outlined in the community plan for Earlimart.

Within the Mixed Use Zoning District, all uses outlined in the M-1, C-3, C-2, C-1, R-1, R-2 and R-3 uses are allowed. Uses and activities that are found by the Planning Director to be similar to and compatible with those specific zoning districts are also allowed. In addition, use and activities determined to be compatible by the Planning Commission and the Board of Supervisors with the above mentioned zoning districts are also allowed.

All conditional uses allowed in these zoning districts shall also be allowed by right with exception of the following combination of uses:

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All uses shall not be detrimental to the health, safety, peace, morals, comfort, and general welfare of persons residing or working in the neighborhood, or to the general welfare of the county. All uses shall limit impacts related to smoke, fumes, dust, gas, noise, odor, vibrations and other hazards to be considered an allowed use without the need for a special use permit. All allowed uses are subject to the determination of appropriateness by the Director of Planning.

The Director of Planning has the option of deferring any land use application allowed in this district to the Planning Commission for review and decision.

Uses/Combination of Uses reviewed by Planning Commission
Autowrecking and Residential
Battery Manufacture and Residential or Commercial
Biomass Fuel Production and Residential
Flammable Liquids over 10,000 gallons
Hazardous Waste Facility
Planning Mills and Residential or Commercial
Sand blasting
Slaughterhouse and Residential
Solid Waste Recycling and Residential
Super service stations and Residential
Airport
Heliport

DEVELOPMENT

- Height:** No building or structure hereafter erected or structurally altered shall exceed six (6) stories or seventy five (75) feet to uppermost part of roof.
- Front Yard:** 0 Feet
- Side Yard:** Where a lot abuts upon the side of a lot in any "R" Zone (R-A, R-O, R-1, R-2 and R-3), there shall be a side yard of not less than five (5) feet. Where a reversed corner lot rears upon a lot in any "R" Zone, the side yard on the street side of the reversed corner lot shall be not less than fifty (50) percent of the front yard required on the lots in the rear of such corner lot. In all other cases, a side yard for a commercial building shall not be required.
- Rear Yard:** Where a lot abuts upon the rear of a lot in any "R" Zone (R-A, R-O, R-1, R-2 and R-3), there shall be a rear yard of not less than fifteen (15) feet. In all other cases, a rear yard for a commercial building shall not be required.
- Lot Area:** The minimum lot area shall be ten thousand (10,000) square feet; provided, however, that where a lot has less area than herein required and was of record at the time this paragraph became effective, said lot may be occupied by not more than one (1) main building subject to the provisions of this Section.
- Floor Area Ratio:** The maximum Floor Area Ratio is 2. The Floor Area Ratio is the amount of square feet of all structure allowed on a parcel based on parcel size.
- Distance between structures:** The minimum distance between structures is 10 feet.
- Parking:** Off-street parking and loading shall be required in conformance with Section 15.

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9. Fences, Walls, and Screening: Where the side or rear lot line of a site adjoins or is located across an alley from any “R” Zone (R-A, R-O, R-1, R-2, and R-3), there shall be a solid wall, fence or equivalent landscaping screening at least six (6) feet in height located along the common lot line, except in the required front or side yard. Open storage of materials and equipment shall be permitted only within an area surrounded and screened by a solid wall or fence or compact evergreen hedge (with solid gates where necessary), not less than six (6) feet in height, provided that no materials shall be stored to a height greater than that of the wall, fence, or hedge. Fulfillment of the requirement of this paragraph shall not be required for buildings and uses which were established in accordance with all applicable buildings and zoning regulations and which were existing in a commercial or manufacturing zone on the effective date of this paragraph, until such time as a permit or other grant of approval for expansion, alteration or development of property is approved by Tulare County.

All other Development Standards are outlined in the Community Plan for Earlimart. Conformance to development standards is required for all development; however, the Planning Director, Planning Commission, or Board of Supervisors may provide exemptions to particular development standards when deemed appropriate.

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A-3 Development Standards (Mixed Use Zoning District)

To promote Economic Development within the Earlimart Urban Development Boundary, a Mixed Use Overlay zoning district is being established to allow for flexibility in the allowed uses within Earlimart. In addition, the use permit restriction is updated to allow for ministerial approval [by the Planning Director]. Development standards are established to ensure high quality development within this mixed-use overlay district.

ARCHITECTURE

A-1 Entries to buildings should be individualized and clearly identifiable.

A-2 Retail spaces should be accessed directly from the sidewalk, rather than through lobbies or other internal spaces.

A-3 Entrances to upper story uses should not be as prominent as the primary entrances to first story uses.

A-4 The height of first floor commercial should have a minimum ceiling height of 12 feet.

A-5 Architecturally distinguish the ground floor from the upper façade, to form a visual base for the building. Create an intimate scale for the pedestrian environment.

A-6 Each building should have a defined base, body, and cap segment

A-7 Blank walls on ground floor facades adjacent to public sidewalks, public right-of-ways, and public spaces are prohibited.

A-8 Ground floor window openings should range between fifty (50) to eighty (80) percent of the ground floor façade adjacent to sidewalks and private and public plazas, patios, and courtyards. These window openings should consist of transparent “storefront” windows. Second story windows should not exceed fifty (50) percent of the total exterior wall surface.

A-9 Three-dimensional cornice lines, parapet walls, and/or overhanging eaves should be used to enhance the architectural character of the building.

A-10 Wall surfaces should not exceed 250 square feet without including some form of articulation. Acceptable forms of articulation include use of windows, varied reveal patterns, change in material, texture, color, or detail; and a change in wall plane location or direction.

A-11 Openings in the façade should be accentuated with paint, tile, shutters, awnings, planters, and/or other appropriate architectural features in order to create varied shadows and a rich visual texture.

A-12 Articulation and detailing of the exterior walls at the ground level, should be integrated with landscape features (trees, plants, walls, trellises, and unique land forms) to ensure an appropriate transition from ground to wall plane.

A-13 An equal level of architectural detail and landscaping should be incorporated into all sides of freestanding buildings, because they are generally visible from all sides.

A-14 Architectural details should be fully integrated into the design of the building to avoid the appearance of afterthought elements or elements that are “tacked on” to a building.

A-15 Finish materials that give a feeling of permanence and quality should be used at ground level facades.

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A-16 A consistent use of window style, size, trims, and accents should be used to ensure a consistent character along the building façade.

A-17 Exposed structural elements (beams, trusses, frames, rafters, etc.) are acceptable when appropriately designed to complement the over design of the façade.

A-18 Tilt-up buildings should incorporate decorative trim, recessed/projecting panels, recessed windows/doors, accent materials, and varied roof height to increase visual interest.

A-19 New buildings located at the corner of the block may be more massive in scale than adjacent buildings to better define the street intersection.

A-20 Corner buildings should have a strong relationship to the corner of the intersection by incorporating a unique architectural element or detail at the corner; such as a tower or primary building entrance.

A-21 Corner buildings should present equally important facades of similar appearance on both streets.

A-22 Articulate side and rear facades in a manner compatible with the design of the front façade. Avoid large blank wall surfaces on side and rear facades which are visible from public areas. In these locations, display windows, store entrances, and upper windows are encouraged. When this is not feasible, consider the use of ornament, murals, or landscaping along large blank walls.

A-23 Remove alterations whose design and/or materials are not consistent with the overall character of the building.

A-24 Where off-street parking or an alley is provided behind a building, a secondary entrance to both first floor and upper floor uses should be provided at the rear of the building.

A-25 Locate and design required vents and access doors to minimize their visibility from public spaces.

A-26 Use high quality detailing for new buildings and replacement elements. For example, new or replacement windows should have sash and frame thicknesses and window depths which are similar to those of original or historic windows. Such level of detailing provides an interplay between light and shadow which adds interest and visual depth to the façade.

A-27 Loading docks, storage areas, and service facilities should be located at the rear of the building and screened from the street as necessary.

A-28 Conceal all electrical boxes and conduits from view, and position light sources to prevent glare for pedestrians and vehicles.

ROOFS AND AWNINGS

RA-1 Awnings should be compatible with other awnings nearby, particularly those on the same building, when these awnings complement the architectural character of the building.

RA-2 Canopies and awnings should be compatible with the style and character of the structure on which they are located.

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RA-3 Use matte canvas fabric for awnings; not vinyl, fiberglass, plastic, wood or other unsuitable materials. Glass and metal awnings may be appropriate for some buildings, but must be consistent with the architectural style of the building.

RA-4 Include architectural features such as awnings, canopies, and recessed entries that can protect pedestrians from inclement weather. Design these features as integral parts of the building.

RA-5 Awnings and canopies should not hang below the top of the first floor storefront window. In addition, awnings and canopies should be at least ten (10) feet above the sidewalk.

RA-6 Canopies and awnings should not project more than seven (7) feet from the surface of the building.

RA-7 Awnings and canopies that project into the public right-of-way should not impede pedestrian or vehicular movement.

RA-8 Roof forms, lines, masses, and materials should be continuous and consistent with the overall style, character, scale, and balance of the building.

RA-9 Roof overhangs and exposed structural elements should be designed to be consistent with the overall style and character of the building.

RA-10 Roof mounted HVAC equipment, ducts, vents, and other equipment should be screened from public view.

RA-11 Mansard roofs are prohibited.

RA-12 All flat roofs should have 90% of the roof area covered by solar panels. All sloped roofs should have 50% of the roof area

covered by solar panels. Roofs should be painted or colored with a bright white (or similar color) with a reflective glossy finish.

SITE PLANNING

SP-1 Place entrances to storefronts and other ground floor uses so that they are accessible directly from the public sidewalk, not internal lobbies.

SP-2 On corner sites, a prominent streetscape presence should be established and visual interest should be created by either locating buildings near the intersection to enliven the streetscape or using landscaping to frame the intersection. Parking areas immediately adjacent to intersections are discouraged.

SP-3 Structures and site improvements should be located and designed to avoid conflict with adjacent uses.

SP-4 Gates to parking areas should be designed with materials and color that are compatible with the site.

SP-5 Multi-story buildings that overlook private or common area open space of adjacent residences should be designed to protect privacy of these spaces.

SP-6 Gates to parking areas should be located to prevent vehicle stacking or queuing on the street.

SP-7 Primary site and building entry points are strongly encouraged to generate visual interest with special design features such as decorative or textured paving, flowering accents, special lighting, monuments, walls, shrubs, water features, and the use of sizeable specimen trees.

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SP-8 To the extent feasible and practicable, parcels should share access driveways to minimize curb cuts and traffic congestion.

SP-9 Cul-de-sacs are inappropriate except when a freeway, railroad, or canal prevents connectivity.

SP-10 Block lengths should be short, averaging 200 to 300 feet. Maximum block length is be 500 feet.

LANDSCAPING

LA-1 Projects should provide, and maintain, landscaped buffers between commercial uses and low-density residential uses, between industrial and residential uses, and between commercial and industrial uses. Plant material will be placed in a manner to suggest natural growth as opposed to a rigid barrier.

LA-2 A predominance of deciduous tree species is encouraged to shade western, southern, and southwestern exposures.

LA-3 The parking lot should not be the dominant visual element of the site as viewed from the street. Locate or place parking lots at the side and rear of buildings or use parking lot screening to soften their appearance. Screen parking lots: Utilize a hedge (recommended height of 36 inches) with a rolling berm to screen parking at the street periphery (Minimum shrub container size should be 5 gallons.)

LA-4 Project sites should be designed so that areas used for outdoor storage, and other potentially unsightly areas are screened from public view. All service yards and outdoor storage areas should be enclosed or screened from view.

LA-5 Loading areas, access and circulation driveways, trash, and storage areas, and rooftop equipment should be adequately screened from the street and adjacent properties, as deemed necessary. To the fullest extent possible, loading areas and vehicle access doors should not be visible from public streets.

LA-6 Loading driveways should not back onto streets or encroach into landscaped setback areas.

LA-7 Loading doors should be integrated into building elevations and given the same architectural treatment where feasible.

LA-8 Utility equipment such as electric and gas meters, electrical panels, and junction boxes should be screened from view or incorporated into the architecture of the building.

LA-9 Utility devices, such as transformers and backflow preventers, should not dominate the front landscape area.

LA-10 All utility lines from the service drop to the site should be located underground.

LA-11 When security fencing is required, it should be a combination of solid walls with pillars and offsets, or short solid wall segments and segments with metal fencing. Chain-link fencing is strongly discouraged when facing public view and should only be used as interior fencing.

LA-12 Retaining walls at retention basins should utilize a stepped or terraced motif as a visual tool to maintain appropriate human scale.

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LA-13 Retention basins visible to public view and common open spaces should be contoured and landscaped in a creative manner to minimize a harsh utilitarian appearance. When feasible, it is recommended to beneficially use the run-off storm water as supplemental watering for the landscape plants.

LA-14 Parking lot run-off should be routed through turf or other landscaping.

LA-15 Parking lots located adjacent to the sidewalks or right-of-ways should be screened to a height of thirty six (36) inches above the grade with landscaping and/or low high quality fencing.

REFUSE AND STORAGE AREAS

R-1 Trash storage must be enclosed within or adjacent to the main structure or located within separate freestanding enclosures.

R-2 Trash enclosures should be unobtrusive and conveniently accessible for trash collection but should not impede circulation during loading operations.

R-3 Trash enclosures should be located away from residential uses to minimize nuisance to adjacent properties.

R-4 Trash and storage enclosures should be architecturally compatible with the project design. Landscaping should be incorporated into the design of trash enclosures to screen them and deter graffiti.

LIGHTING

LI-1 Provide lighting at building entrances and for security at ground level.

LI-2 Lights should be shielded and point down toward the ground.

LI-3 Parking lot should have uniformly spaced night lighting.

LI-4 Well-lit sidewalks and/or pedestrian walkways should be located to provide safe access from the parking lot to the street sidewalk.

LI-5 Exterior architectural lighting should fully compliment a building's design and character. Light fixtures should work in conjunction (size, scale, and color) with the building's wall, roof.

LI-6 Street lighting features should be "pedestrian scale" at twelve (12) to eighteen (18) feet in height above the curb.

WALLS AND FENCES

WF-1 Wall/fence design should complement the project's architecture. Landscaping should be used to soften the appearance of wall surfaces.

WF-2 Walls and fences within front and exterior side yards of commercial sites should be avoided.

WF-3 Unless walls are required for screening or security purposes they should be avoided.

WF-4 Security fencing should incorporate solid pilasters, or short solid wall segments and view fencing.

WF-5 Front yard fences should not abut the sidewalk. The fence should be set back from the sidewalk at least 2 to 3 feet to allow room for landscape materials to soften the fence and to ensure pedestrian comfort.

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WF-6 Walls and fences should be designed in such a manner as to create an attractive appearance to the street and to complement the architecture of the industrial park.

WF-7 Gates should be provided in walls or fences where necessary to allow emergency access.

WF-8 High perimeter walls and walls topped with barbed wire, razor wire, or broken glass are strongly discouraged.

WF-9 Inordinately long walls or fences should be broken up by landscaping, pilasters, offsets in the alignment of the wall or fence, and/or changes in materials and colors.

WF-10 Chain link fences should not be visible from streets.

WF-11 Long expanses of fence or wall surfaces should be offset and architecturally designed to prevent monotony. Landscape pockets (12-feet wide by 3- feet deep) should be provided at 70-foot minimum intervals along the wall.

STREETSCAPE

ST-1 A consistent pavement material of varied texture and color should be applied to all crosswalks to clearly define pedestrian crossings, to slow down traffic.

ST-2 Sidewalks widths, excluding curbs, should be a minimum of five (5) feet.

ST-3 Curb and gutters should be constructed with all new development.

ST-4 A planting strip, or tree lawn, 3 to 5 feet wide should be located between the sidewalk and the curb of the street. Existing tree lawns should be preserved.

ST-5 New street trees should be planted on the curb edge of the sidewalk in front of all new development projects.

SIGNAGE

SI-1 Sign letter and materials should be professionally designed and fabricated.

SI-2 Each storefront with a ground floor entrance should be allowed two signs that should be attached to the building.

SI-3 All electrical conduits should be concealed from public view.

SI-4 For commercial uses, the primary wall sign should be in the space above a storefront and visibly oriented towards the street.

SI-5 For commercial uses, a secondary sign should be smaller than the primary sign and be oriented towards passing pedestrians. It should extend out perpendicular to the building façade and be mounted or hung from the wall beneath an awning or above a first floor window. The bottom of the wall-mounted sign should be located at least eight (8) feet above the sidewalk. The outer face of the sign should not extend more than four (4) feet from the edge of the building surface, and the maximum area of the sign should have no more than six (6) square feet.

SI-6 Signs should be designed to be compatible with building design in terms of relative scale, overall size, materials, and colors. No sign should dominate the façade. Signage elements should incorporate materials colors, and shapes that appropriately reflect and compliment the building's architecture.

SI-7 Large signs that dominate a building façade or the streetscape should not be permitted.

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SI-8 Signage should be constructed of high quality, low maintenance, and long lasting materials. Except for banners, flags, temporary signs, and window signs, all signs should be constructed of permanent materials and should be permanently attached to the ground, a building or another structure by direct attachment to a rigid wall, frame, or structure.

SI-9 No more than twenty (20) percent of window area should be obstructed by signs, posters, advertisements, painted signs, and/or merchandise, and the top one half of the window should be permanently clear and free of obstructions. Awning signage should be of a replaceable-type to accommodate tenant turnover.

SI-10 Wall, canopy, under-canopy, and marquee signs should not exceed three-fourths (3/4) square foot of aggregated display area per lineal foot of frontage.

SI-11 Awning sign should be mounted on the hanging border of the awning and should not protrude beyond the awning surface.

SI-12 Wall signs or advertisements should not project more than twelve (12) inches from the wall face to which they are mounted, should not project beyond building eaves, and should be mounted flat throughout their length and height.

SI-13 Signs for individual tenants within a multiple-tenant, such as offices located above the ground floor, should be grouped together and appropriately scaled to a pedestrian-oriented retail environment.

SI-14 Fin signs or under marquee sign are permitted provided that they are installed with a minimum of eight (8) feet clearance from the lowest point on the sign and support to the top of the walking surface below it.

SI-15 Awning signs and face-mounted signs are permitted provided that the sign should have no more than one line of text and that maximum text height is twelve (12) inches.

SI-16 No signs should be erected in any manner in which the sign, in whole or in part, would create a hazardous condition to pedestrian or automobile traffic alike.

SI-17 Additional business signs should be permitted on windows and on the vertical face of awning valances provided that the signs are permanent in nature and of high quality.

SI-18 The following signs are strictly prohibited:

- Roof signs, signs located above the roof or parapet lines.
- Permanent banner signs.
- Posters.
- Painted window advertisements.
- Billboards
- Large auto-oriented pole-mounted or “lollipop signs.”
- Moving signs and flashing signs.

SI-19 Signs advertising an activity, business product, or service no longer conducted on the premises, and/or signs frames, structural members, or supporting poles remaining unused for a period of six (6) months should be removed from the site or building by the property owner.

SI-20 Address markers should be easily identifiable and readable from the street.

SI-21 Freestanding, ground-mounted and monument signs should be not less than one (1) foot behind a property line or designated right-of-way for vehicular and pedestrian traffic, but in no case should be more than ten (10) feet behind a sidewalk and ten (10) feet from any vehicular entrance or driveway.

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These signs should not interfere with the safety of vehicular traffic entering or exiting the premises.

SI-22 The maximum height of monument signs should be five (5) feet above the top of concrete curb.

SI-23 One freestanding or monument sign with a maximum of thirty-two (32) square feet of display area should be allowed on each street frontage of more than fifty (50) feet. Where two (2) or more freestanding or monument signs are allowed on a single street frontage, one freestanding or monument sign with a maximum of fifty (50) square feet of display area may be used in lieu of several signs on the same frontage.

SI-24 All gateway signs should have a consistent character and style.

SI-25 A hierarchy of gateway signs should be established to differentiate between major and minor gateway entrances.

SI-26 Major gateway signs should be designed as visually prominent towers, monuments, or street spanning arches.

SI-27 Minor gateway signs should be visible to automobile traffic, but also be low enough to be visible to pedestrian traffic.

SERVICE STATIONS AND CAR WASHES

SS-1 Service and carwash bays should not face residential properties or the public street. The visibility of service bays and carwash opening should be minimized.

SS-2 Gas pump canopies should be ancillary to the main building structure. The retail market/office building segment of the facility should be oriented along the street frontage, whenever possible.

SS-3 All structures on the site (including kiosks, carwash buildings, gas pump columns, etc.) should be architecturally consistent and related to an overall architectural theme.

SS-4 Canopy light fixtures should be recessed into the canopy.

SS-5 Outdoor equipment, such as vent risers and clean air separators, should be screened either with an enclosure or if site configuration topography permits, away from street view, screened with landscaping or located at a grade differential.

SS-6 Site-specific architectural design contextual to surroundings is strongly encouraged. Designs based solely on corporate or franchise models are strongly discouraged.

AUTO REPAIR SERVICES

AR-1 Building design should be stylistically consistent, and compatible with surrounding buildings through use of similar scale, materials, colors, and/or detailing.

AR-2 Building materials should have the appearance of substance and permanency; lightweight metal or other temporary appearing structures are discouraged.

AR-3 Vehicle drop-off areas should be provided to prevent vehicle overflow to adjacent streets.

CONTRACTOR, BUILDING SUPPLY, OR LANDSCAPING YARDS

BS-1 The main office or building should be located along the street frontage to screen outdoor sales and minimize the visibility of storage of materials and vehicles.

BS-2 Customer parking should be provided close to the building and not interspersed in the yard.

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BS-3 All outdoor contractor vehicle storage areas should be enclosed with a screen of sufficient height and constructed with durable and high-quality materials that are compatible with the building and site.

CONSUMER STORAGE FACILITIES

SF-1 The administrative office should be located in a building or building element that is human scale and located in proximity to the street.

SF-2 Parking for visitors should be located near the administrative office, outside of any gated portion of the facility.

SF-3 A storage facility should be consistent with its surrounding area in scale and appearance, through the use of building size transitions, architecture, and landscaping.

SF-4 Loading doors for individual storage units should not face outward toward streets.

SF-5 In order to break up the mass of larger buildings which containing storage units, provide horizontal and vertical articulation through the use of building offsets, windows, and variations in colors and materials.

SF-6 Any area intended for the storage of automobiles and recreational vehicles should be located towards the rear of the site or screened with an enclosure of adequate height

SPECIAL CONDITIONS

SC-1 The project should emit no smoke or should reduce the amount of smoke from an existing use.

SC-2 The project should emit no fumes or should reduce the amount of fumes from an existing use.

SC-3 The project should implement dust control measures sufficient to minimize or prevent dust emissions. Measures should be consistent with, or more effective than, those required by the Valley Air District.

SC-4 The project should emit no odors or should reduce the amount of odors from an existing use.

SC-5 The project should not create noticeable vibrations.

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A-4 General Plan Land Use And Zoning Consistency Matrix

Tulare County Zoning Districts General Plan Land Use Designations	R-A	R-1	R-2	R-3	C-1	C-2	C-3	M-1	M-2	AE-20	AE-40	O	CO	PO
Urban Reserve Residential														
Low Density Residential														
Low-Medium Density Residential														
Medium Density Residential														
Medium-High Density Residential														
High Density Residential														
Neighborhood Commercial														
General Commercial														
Community Commercial														
Highway Commercial														
Town Center														
Service Commercial														

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Tulare County	R-A	R-1	R-2	R-3	C-1	C-2	C-3	M-1	M-2	AE-20	AE-40	O	CO	PO
Zoning Districts														
Office Commercial														
Commercial Recreation														
Urban Reserve Commercial														
Mixed Use														
Planned Community Area														
Light Industrial														
Heavy Industrial														
Urban Reserve Industrial														
Public/Quasi-Public														
Public Recreation														

Note 1: A shaded box denotes consistency between land use designations and zone; a blank box denotes a lack of consistency.

Note 2: A General Plan Amendment is not required to develop land designated under a reserve classification provided that the proposed development is consistent with policy Land Use 1.6.

Note 3: AE-20 and AE-40 may be used as a holding zone in certain areas pending availability of services.

Note 4: The Mixed Use Zoning Overlay may be applied to the following zoning districts, R-1, R-2, R-3, C-1, C-2, C-3, M-1 and vacant or agricultural land zoned AE-20 or AE-40.

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A-5 Description of Funding Sources

CalTrans Active Transportation Program (ATP):

On September 26, 2013, Governor Brown signed legislation creating the Active Transportation Program (ATP) in the Department of Transportation (Senate Bill 99, Chapter 359 and Assembly Bill 101, Chapter 354). The ATP consolidates existing federal and state transportation programs, including the Transportation Alternatives Program (TAP), Bicycle Transportation Account (BTA), and State Safe Routes to School (SR2S), into a single program with a focus to make California a national leader in active transportation. The projects associated with the Completes Streets Program for the Community of Earlimart will be suggested at the next available round of ATP funding.

Tulare County Measure R

On November 7, 2006, the voters of Tulare County approved Measure R, imposing a 1/2 cent sales tax for transportation within the incorporated and unincorporated area of Tulare County for the next 30 years. The transportation measure will generate slightly more than \$652 million over 30 years to Tulare County's transportation needs.

Local Projects (35% of Measure R Funding)

The Measure R Expenditure Plan allocated 35% of revenues to local programs. Each city and the county will receive funding based on a formula using population, maintained miles, and vehicles miles traveled. The funding will help cities and the county to meet scheduled maintenance needs and to rehabilitate their aging transportation systems.

Regional Projects (50% of Measure R Funding)

The Regional Projects Program comprises 50% of Measure R and includes specific funding for: interchange improvements, regional bridges, regional railroad crossings, regional signals, regional widening projects, and signal synchronization projects. These projects provide for the movement of goods, services, and people throughout Tulare County. Major highlights of this program include the funding of regional projects throughout the county.

Bike / Transit / Environmental Projects (14% of Measure R Funding)

The Goals of Measure R include air quality improvement efforts that will be addressed in the Measure R Expenditure Plan through the Transit/Bike/Environmental Program, which includes funding for transit, bike, and pedestrian environmental projects. The goal of this program is to expand or enhance public transit programs that address the transit dependent population, improve mobility through the construction of bike lanes, and have a demonstrated ability to get people out of their cars and improve air quality and the environment.

San Joaquin Valley Air Pollution Control District (SJAPCD) Bike Path Grants

The District has a grants program for the construction of bicycle infrastructure projects, including Class I (Bicycle Path Construction) or Class II (Bicycle Lane Striping) projects. These grants provide funding to assist with the development or expansion of a comprehensive bicycle-transportation network.

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Strategic Growth Council Grants (SGC)

Affordable Housing - Sustainable Communities

The SGC will allocate 50% of its Cap and Trade funding toward disadvantaged communities and 50% for affordable housing. Projects will include: affordable housing that supports infill and compact development, transit capital and programs that support transit ridership, active transportation projects (infrastructure, and non-infrastructure), TOD projects, capital projects that implement complete streets, projects that reduce CHG emissions by reducing auto trips and VMT, acquisition of easements or other approaches to protect agricultural lands under threat of development, planning to support SCS (sustainable communities scope) implementation, including local plans, must be in or adopted SCS, subject to SGC guidelines.

CMAQ (TCAG Funds)

Congestion Mitigation Air Quality (CMAQ) funds are allocated through the Tulare County Association of Governments (TCAG). The CMAQ program funds transportation projects or programs that will contribute to improved air quality standards. Projects include: transportation activities, transportation control measures, public-private partnerships, alternative fuel programs, traffic flow improvements, transit, bicycle/pedestrian projects, rideshare activities, telecommuting, planning, experimental pilot projects, intermodal freight, and public outreach.

DOT: TIGER

TIGER is a multimodal, merit-based discretionary grant program that funds surface transportation capital projects, including transit and rail. Open to state, tribal, local agencies, and subdivisions.

CDBG (Business Assistance)

The CDBG Economic Development grant provides assistance to local businesses and low-income microenterprise owners to create or preserve jobs for low-income workers in rural communities. Funding includes planning and evaluation studies related to any activity eligible for these allocations, business lending, and public infrastructure.

Choice Neighborhoods

Choice Neighborhoods Planning Grants support the development of comprehensive neighborhood revitalization plans which focused on directing resources to address three core goals: Housing, People and Neighborhoods. To achieve these core goals, communities must develop and implement a comprehensive neighborhood revitalization strategy, or Transformation Plan. The Transformation Plan will become the guiding document for the revitalization of the public and/or assisted housing units while simultaneously directing the transformation of the surrounding neighborhood and positive outcomes for families. ***Choice Neighborhoods Implementation Grants*** support those communities that have undergone a comprehensive local planning process and are ready to implement their “Transformation Plan” to redevelop the neighborhood.

California Department of Water Resources Prop 50 (Contaminant Removal)

Funds are available to disadvantage communities for developing UV or Ozone systems to disinfect drinking water or to set up pilot/demonstration sites.

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Drought Response Funding (SWRCB)

The Governor and Legislature have directed DWR to expedite the solicitation and award of \$200 million (of the \$472.5 million) in IRWM funding to support projects and programs that provide immediate regional drought preparedness, increase local water supply reliability and the delivery of safe drinking water, assist water suppliers and regions to implement conservation programs and measures that are not locally cost-effective, and/or reduce water quality conflicts or ecosystem conflicts created by the drought.

DWR: Water-Energy Grant Program

The 2014 Water-Energy grant supports the implementation of residential, commercial, and institutional water efficiency programs or projects that reduce Green House Gas emissions and also reduce water and energy use. Funding will go toward urban water management, groundwater management, and surface water diversion.

CDPH Clean Water SRF

The Safe Drinking Water State Revolving Fund (SDWSRF) provides funding to correct public water system deficiencies based upon a prioritized funding approach that addresses the systems' problems that pose public health risks, systems with needs for funding to comply with requirements of the Safe Drinking Water Act, and systems most in need on a per household affordability basis.

iBank (Infrastructure State Revolving Fund Program and Economic Development Bank)

iBank provides low cost, long term financing for local governments to fund a variety of public infrastructure projects. (Although this is not a grant, loan rates are largely determined by level of distress within a disadvantaged community).

EARLIMART COMMUNITY PLAN UPDATE

A-6 Complete Streets

Earlimart Complete Streets Policy Document (Adopted February 28, 2017)



Tulare County Complete Streets Earlimart

Final



Prepared by:

**Tulare County Resource
Management Agency**

Paid for by a grant from:



Tulare County Complete Streets – Earlimart
To be incorporated as a Appendix E of the Earlimart Community Plan

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Introduction

Complete Streets Vision

The California Complete Streets Act (AB 1358) of 2008 was signed into law on September 30, 2008. Beginning January 1, 2011, AB 1358 requires circulation elements to address the transportation system from a multimodal perspective. The bill states that streets, roads, and highways must “meet the needs of all users in a manner suitable to the rural, suburban, or urban context of the general plan.”

The Tulare County Resource Management Agency (RMA) is committed to fully integrating modal options in its General Plan and various Community Plans within Tulare County. This includes supporting projects that enhance walking and bicycling infrastructure. Additionally, RMA will improve access to public transportation facilities and services. This includes supporting urban development patterns and Americans with Disabilities Act (ADA) infrastructure that allow for greater accessibility to transit stops and stations. Finally, RMA continues to improve safety for all users and encourages street connectivity to create a comprehensive, integrated and connected circulation network. This is particularly important for those who rely on transportation infrastructure to be physically active and for students who walk or bike to school.

Steady population growth in Tulare County has directly impacted transportation needs. In the past, many of Tulare County’s federal, state, and local funding sources were used to develop new or improved traffic signals, interchanges, provide more travel lanes and to maintain existing roadway facilities. Historically, these funding sources have run well short of what is needed. The typical roadway transportation project that adds capacity and infrastructure is insufficient given these conditions. The RMA must adhere to its vision, which is to “provide a safe, convenient and effective County transportation system that enhances mobility and air quality for residents and visitors.”

Recent RMA and RMA-supported projects have already fulfilled some of these desires. There are already expanded bus transit routes in the County and more are being constructed for implementation in the near future. New transit centers are being placed throughout the County and efforts are underway to add more bicycle lanes and routes. Recent planning studies are looking to improve roadway safety, pedestrian safety, and access management between roadways and building developments. These efforts are consistent with green house gas (GHG) emissions reductions efforts to reduce vehicle miles travelled (VMT) set forth under SB 375.

Promoting Complete Streets projects can offer Tulare County the ability to reduce traffic congestion, improve air quality, and increase the quality of life of residents by providing safe, convenient, and comfortable routes for walking, bicycling, and public transportation. Integration of Complete Streets into Tulare County’s existing policies allows the potential to prevent chronic diseases, reduce motor vehicle related injury and deaths, improve environmental health, stimulate economic development, and ensure access of transportation options for all people in Tulare County.

Complete Streets Definition

Complete Streets are roadways designed to safely and comfortably accommodate all users, regardless of age, ability or mode of transportation. Users include motorists, cyclists, pedestrians and all vehicle types, including public transportation, emergency responders, and freight and delivery trucks among others. In addition to providing safety and access for all users, Complete Street design treatments take into account accommodations for disabled persons as required by the ADA. Design considerations for connectivity and access management are also taken into account for non-motorized users of the facility.

Implementation of Complete Street design treatments will be based on whether it connects the networks for all modes, whether it improves the functionality for all users, and whether it is appropriate given the surrounding context of the community. The final elements of a Complete Street roadway will be largely based on these factors. At a minimum, a Complete Street roadway includes sidewalks and sidewalk amenities, transit shelters and facilities whenever there is a route along the corridor, and provisions for bicycle facilities.

Complete Streets Attributes

While every street cannot be designed to serve all users equally, there are opportunities to enhance service for all users while maintaining its principal transportation function. Complete Streets incorporate community values and support adjacent land uses while ensuring safety and mobility. Proper applications of Complete Streets concepts support sustainable growth and preservation of scenic, aesthetic and historic resources.

Report Outcomes

As a part of the Circulation Element for the Community Plan Update, this Complete Streets Report (Implementation Work Plan) and the following Implementation and Policies Section achieved the following outcomes:

- (i) *Addressed* congestion, climate change and oil dependence by shifting to lower-carbon modes;
- (ii) *Improved* safety by addressing shoulders, sidewalks, better bus placement, traffic speed reduction, treatments for travelers with disabilities;
- (iii) *Created* “livable communities” by encouraging walking and bicycling for health, and by providing a safe walking and bicycling environment as an essential part of improving transportation movement and safety within the roadways studied.

These outcomes are achieved by the following:

- (a) *Included all users* namely, pedestrians, bicyclists, transit vehicles and users, and motorists. In drafting this report, all users were invited to comment on how the County could better serve the community. The implementation of complete streets directly shifts the emphasis to lower-carbon (using) modes of transportation. The shift from the gas using automobile to pedestrian and bicycle transport is achieved through the creation of sidewalks, improving sidewalks, and including bike lanes and/or bike routes for a wider ranger of people to use. The shift to transit is included in improving policies, programs and facilities in the operations of County’s transit systems.
- (b) *Created a comprehensive integrated and connected network* that supports “livable communities” that promote a safe interwoven fabric are provide for by the Policies Section using the transportation goals in the 2030 General Plan Circulation Element and by further defining complete streets network (see Appendix C).
- (c) *Emphasized flexibility* recognizing that all streets with these communities are different, and thus, balancing user needs. No one standard was applied to all streets and the street designs were adjusted to existing conditions, differing jurisdictions and the desires of the community.

- (d) *Considered both new and retrofit projects*, including design, planning, maintenance, and operation, for the entire right-of-way within these communities. In addition to the various sections discussed below Appendix A – D include plans that show the plans, designs, and existing and proposed maintenance plans and operations of the Complete Streets Plan.
- (e) *Used the latest and best design standards*. By using newer design standards as represented in the preliminary design plans verses the County’s Roadway Standards the County is able to provide wider sidewalks and include such amenities as traffic calming measures (bulbouts).
- (f) *Conducted extensive public outreach* to ascertain the solutions that best fit within the context of these communities. The community was consulted and provided final feedback on preliminary designs.

Conclusions and Future Funding Opportunities

The intended effect of identifying the outcomes and reaching the conclusions in this report is that future funding opportunities will be enhanced because the Community will be supported by fully updated Community Plans. The conclusion to the report includes the Circulation Element of the Community Plan including the policies, and plans. The other conclusion to the report includes preliminary design drawings.

Specifically, the funding sources that are found in the Funding Section will be pursued actively by Tulare County to complete the work identified in the studies include, but are not limited to, the following:

- **California Safe Routes to Schools Funds**
- **Federal Safe Routes to Schools Funds**
- **Highway Safety Improvement Funds**
- **Federal Transportation Activity Program (TAP) Funds**
- **Federal Transit Funds**
- **Federal Communities Putting Prevention to Work Grant**
- **Federal Highway Administration Pedestrian Safety and Design**
- **Strategic Growth Council**
- **Walkable and Livable Communities Institute**
- **California’s Local Public Health and Built Environment Program**
- **State Cap and Trade Funding**

Implementation

Selection of Communities

An effort is under way in Tulare County to implement Complete Streets Policies in the unincorporated communities within Tulare County's boundary. Just as the County updated its General Plan in 2012, many of the Community Plans are going through the update process. As a result of the Community Plan update process, several public meetings have been held in order to garner input from the local residents and business owners. Balancing the needs of what the people want while following local, state and federal policies and laws with a limited amount of available funding is the principal challenge in each community.

Transportation and related infrastructure costs tend to be exceedingly high may take years to implement. For purposes of this Study, four transportation corridors were selected within the community (see Appendix A), and two roadway segments in the community were selected to be evaluated for implementation of Complete Street standards. These roadway segments generally represent the highest volume roadways with a blend of residential and mixed land uses that also provide for regional access. Local streets and freeways were not selected, however tying into these facilities is considered.

General themes that were voiced from residents in each community related to transportation included the need for:

- Sidewalks
- Better road conditions
- Safe walking and biking areas
- Street lights
- Pedestrian crossings
- Safe (lower) vehicle speeds
- Improved drainage
- Increased transit stops
- Improved connectivity (railroad crossings)

Given the information provided by the residents and business owners, conceptual layouts and designs based upon the citizens concerns were presented to collect input. Based upon the community planning process, the following sections identify proposed projects for each community.

State Street – Avenue 48 to Avenue 56 and Washington Avenue:

Along the State Street corridor between Avenue 48 (Armstrong) to Avenue 56 (Sierra), the project proposes to install new curb, gutter, sidewalk, at designated intersections, pedestrian ramps, relocate utilities and drainage. In the spirit of Safe Routes to School, land uses along this corridor include residential, highway commercial (mini-marts, service stations), with the school and children as the main focus of the safety improvements. Similar improvements are contemplated for Washington Ave.

Project Phasing

Tulare County RMA is proposing two types of projects coming from the community based upon the complexity of the project. The first types of projects could be built with limited improvement. They would be considered Phase 1 Projects and would have only minor needs for storm drain facilities, fence relocations, utility conflicts, etc. Phase 2 Projects are more inclusive and would be classified as medium

to long range projects. These projects would need other infrastructure improvements such as storm water basins, major storm drain improvements, utilities to be undergrounded, Caltrans encroachment permits etc.

Phase 1 Projects	Phase 2 Projects
Curb, gutter & sidewalk (storm drain water into existing system); pedestrian ramps; bulb outs (where appropriate)	Curb, gutter & sidewalk (new drainage system)
Street lights	Major storm drain facilities (new pipelines and storm water basins)
Bus shelters, benches, trash receptacles, etc.	Utility relocations (undergrounding)
Fence relocations	Major land acquisition
Street signage and striping	Railroad crossing improvements
Minor utility conflicts	Caltrans Bridge Improvements
Minor land acquisition	

Complete Street Policies

Complete Street Goals

The purpose of the RMA Complete Streets Policy is to create a comprehensive and uniform Complete Streets vision and policy for Tulare County. This will allow the implementing entities to incorporate Complete Streets guidelines and standards into both development and redevelopment actions. The County's goals are:

- Tulare County's transportation network will be supported through a variety of feasible transportation choices, which allows for sustainable growth.
- The livability of neighborhoods and commercial centers located along the County's transportation corridors will be enhanced by a safe and inviting pedestrian environment.
- The design of multimodal roadway facilities will not compromise the needs of larger vehicles such as transit vehicles, fire trucks and freight delivery trucks.
- Inclusion of Complete Streets design elements will allow for design flexibility on different street functions and neighborhood contexts.
- Inclusion of Complete Streets design elements will improve the integration of land use and transportation, while encouraging economic revitalization through infrastructure improvements.

Complete Streets Objectives

- To create an integrated and connected transportation network that supports transportation choices and sustainable growth.
- To ensure that all transportation modes are accommodated to the extent possible in all public roadway facilities in the County.
- To develop and use the latest design standards and guidelines in the design of Complete Streets.
- To provide flexibility in the implementation of this policy so that streets chosen for implementation of Complete Streets elements can be developed to fit within the context of their principal purpose and surroundings without compromising the safety of users and needs of larger vehicles.

Complete Streets Policies

Tulare County General Plan Policies

The Tulare County General Plan Update (2030) in complying with AB 1358 calls for 4 Complete Streets related principles including:

Principle 1: County-wide Collaboration

Support countywide transportation plans that provide choices in travel modes.

Principle 2: Connectivity

Emphasize connectivity among cities, communities, and hamlets to ensure County residents have access to jobs and services.

Principle 3: Community Circulation

Anticipate and provide transit, traffic, and roadway connections that support the interconnectivity of all communities.

Principle 4: Pedestrian and Bicycle Facilities

Plan for the development and expansion of pedestrian paths and bicycle facilities that provide residents, with alternative modes of travel.

These principles are expressed mainly in following policies including:

- TC-1.6 Intermodal Connectivity
- TC-1.7 Intermodal Freight Villages
- TC-5.1 Bicycle/Pedestrian Trail System
- TC-5.2 Non-motorized Modes in Planning and Development

Complete Street Policy Design Criteria

1. Tulare County promotes the incorporation of Complete Streets concepts and design standards in all appropriate new and retrofit County public streets (except State highways and freeways).
2. Tulare County will seek every opportunity to provide funding for the planning, design, and implementation of Complete Streets.
3. New Class I Multi-use Paths should be a minimum of eight (8) feet wide.
4. New Class II Bike Lanes should be a minimum of five (5) feet wide.
5. New sidewalks should be a minimum of five (5) feet wide.
6. Bulb-outs should be considered in areas of higher speed (35 mph or greater) where sufficient turning radii for trucks is available or as determined by the County Engineer.
7. As determined by the County Engineer, installation of posted speed limit vehicle activated traffic calming signs (VATCS) are encouraged in instances of high speed to promote safety.
8. Transit shelters and benches are encouraged at all County transit stops if FTA grants are available.
9. Street lighting and cross walk are encouraged to promote safety if considered feasible by the County Engineer.
10. Design policies should be consistent with the Tulare County Improvement Standards; other references include existing design guides, such as those issued by Caltrans, AASHTO and the ADA Accessibility Guidelines.

11. Public streets excluded from this policy include those where:
 - Complete streets concepts are in conflict with existing laws, codes, or ordinances.
 - Compliance with this policy would conflict with goals or physical conditions related to the unique aspects of the location.
12. Exceptions from Complete Street Policies:
 - Accommodation is not necessary where non-motorized use is prohibited, such as freeways.
 - Cost of accommodation is excessively disproportionate to the need or probable use as determined by the County Engineer.
 - A documented absence of current or future need.

Complete Street Mobility Plan

The California Complete Streets Act (AB 1358) of 2008 was signed into law on September 30, 2008. Beginning January 1, 2011, AB 1358 requires circulation elements to address the transportation system from a multimodal perspective. The bill states that streets, roads, and highways must “meet the needs of all users in a manner suitable to the rural, suburban, or urban context of the general plan.” Essentially, this bill requires a circulation element to plan for multimodal transportation accommodating all modes of transportation where appropriate, including walking, biking, car travel, and transit. The current functional classification system plan is shown in Appendix B (Circulation Plan).

The Complete Streets Act also requires circulation elements to consider the multiple users of the transportation system, including children, adults, seniors, and the disabled. For further clarity, AB 1358 tasks the Governor’s Office of Planning and Research to release guidelines for compliance with this legislation by January 1, 2014. Implementation of complete streets principles should be tailored to the individual jurisdiction and the individual roadway. The Complete Streets Program for Tulare County focuses on a network-based approach that has been tailored to the needs of the Community of Earlimart. Another principle that is being applied is under SB 743, requiring a change to evaluating traffic using Vehicle Miles Traveled versus Level of Service under CEQA analysis, and under AB 32 in reducing Green House Gasses.

Complete Streets: According to the National Complete Streets Coalition, complete streets are a means by which, “... planners and engineers (can) build road networks that are safer, more livable, and welcoming to everyone.... Instituting a complete streets policy ensures that transportation planners and engineers consistently design and operate the entire roadway with all users in mind – including bicyclists, public transportation vehicles and riders, and pedestrians of all ages and abilities.”

Network-Based Complete Streets: Combines individual travel mode networks into one multimodal transportation system, integrating infrastructure where appropriate, ultimately ensuring that all users can safely and efficiently access their destination.

Vehicle Miles Traveled (VMT): Vehicle miles traveled is the metric that identifies the total distance traveled in a car per driver. VMT drives roadway needs (the more people who drive, the more capacity and maintenance are needed on the roadway system). Under the Tulare County Climate Action Plan, in reducing VMT green house gas emissions are reduced and the County has an overall target of reducing 6% of its green house gas emissions through a reduction in VMT.

Community Plans adopt these principles, which are combined into the following mission statement:

The Community Complete Streets Network comprises four types of facilities—vehicular, pedestrian, bicycle, and public transit. This complete streets approach will enable

residents to choose which travel mode best suits them. It also will ensure that streets are designed with the users in mind—accommodating for businesses, children, the elderly, bicyclists, and transit users.

Caltrans and Complete Streets

Under Caltrans District Order 64-R1, Caltrans requires that a Complete Streets Implementation Action Plan be developed and implemented for Caltrans owned and maintained Streets. Their Implementation Action plan provides a background by which the Tulare County Completes Street Plan will be implemented.

TCAG, Tulare County Regional Bicycle Transportation Plan, Regional Transportation Plan (RTP) and Sustainable Communities Strategy (SCS)

TCAG in 2014 updated a Regional Bicycle Plan that does not include any bicycle facilities through the Community of Earlimart. TCAG funded the grant for this Complete Streets Policy and in the RTP Action Element describe bicycle circulation patterns and Pedestrian policies focusing on the Americans with Disabilities Planning Strategies and Transportation Demand Management to increase pedestrian activity. In addition, rail and goods movement is part of the Sustainable Communities Strategy in lieu of utilizing diesel powered freight trucks.

Tulare County Climate Action Plan (CAP)

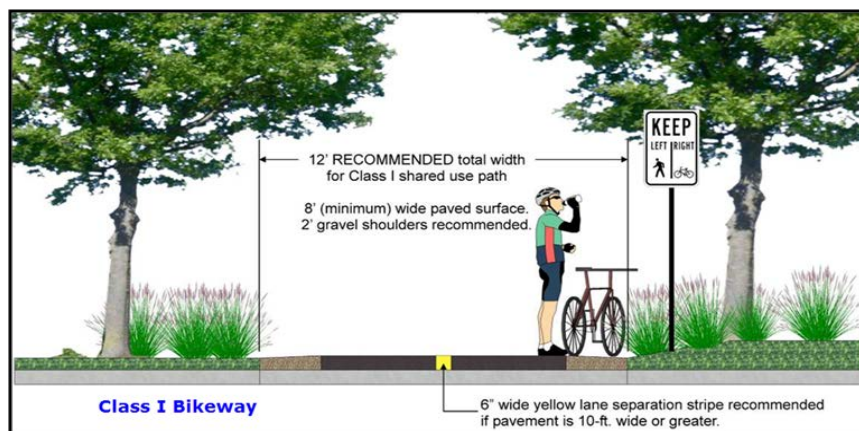
The Tulare County CAP calls for a reduction on a project (over 50 vehicles) by project basis of 6% through a mixture of measures that are spelled out in Appendix J of the CAP. Utilization of alternative means of transportation will reduce GHG emissions and will help projects and the region meet their targets.

Bicycle Facilities

Bicycle facilities consist of Class I, Class II, and Class III facilities as defined below. In Tulare County, this General Plan and the Bicycle Transportation Plan envision a system of bicycle lanes on roadways that will connect the activity centers of the communities to the residents. County has identified pedestrian corridors on the Community of Earlimart Bicycle, Bus and Pedestrian Plan (see Appendix C).

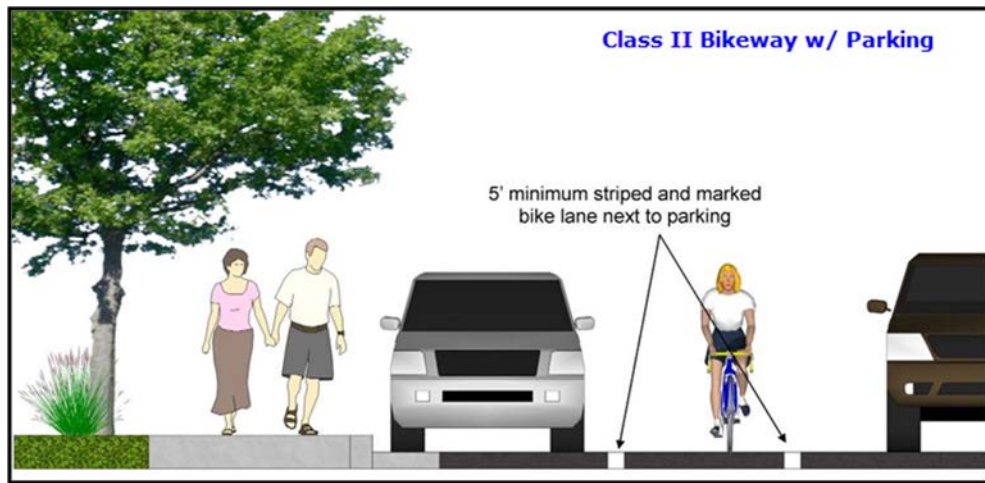
Class I

Bike path providing completely separated right-of-way designated for the exclusive use of bicycles and pedestrians. In Tulare County, Class I facilities will primarily be implemented through TCAG. Future bicycle facilities have also been identified through the *Bicycle Transportation Plan* (TCAG - 2010). There is no existing or proposed Class I bicycle facilities in Earlimart.



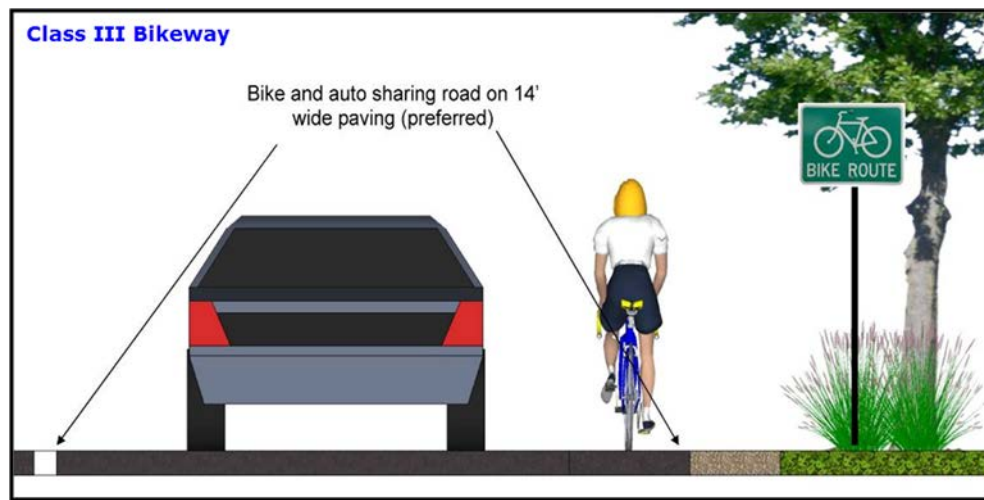
Class II

Bikeway that provides designated lanes for the use of bicycles through the use of striping on the roadway and signage designations for the facility. For the purposes of Complete Streets, the County is proposing Class II bicycle facilities on State Street.



Class III

Bikeway that provides route designation by signage. Roadways are shared between bicyclists and motorists. Class III facilities in Tulare County are envisioned to be implemented along the major circulation segments of roadway that connect the overall County roadway network. Class III facilities are proposed on Washington Ave, Church and School Streets. Although not signed on many local roads in Earlimart, bicyclists are allowed use the side of the road or share the road on all County roadway facilities excluding freeways.



Pedestrian Facilities

Pedestrian Paths and Sidewalks

Pedestrian paths are primarily developed as part of the roadway and trail systems of a community and reflect the interconnected nature of circulation and transportation systems as a whole. Constructing wide streets increases the distance a pedestrian must travel to cross a street, thereby making it inconvenient for public use and inhibiting pedestrian circulation in the community. Currently, limited continuous sidewalks are provided along major routes in the community. In addition to connecting available pedestrian resources, the communities have prioritized the completion of sidewalks along safe routes to school. Along Washington Ave. enhanced pedestrian crossings and sidewalks are considered in areas where high pedestrian and school children demand occurs (such as to and around schools).

Multiuse Trails

Multiuse trails are facilities that can be used by bicycles, pedestrians, equestrians, and other recreational users. No multiuse trails exist or are proposed in Earlimart.

Transit Facilities

Transit options give users the ability to get to a destination without relying on the automobile. This also provides other community benefits, including reduced vehicle miles traveled (VMT). Reducing VMT will help the County achieve their greenhouse gas reduction target,

Public transportation services and facilities in Tulare County consist of public bus service, paratransit service, and could also consider park-and-ride locations.

Public Bus Service

Public bus service is provided by Tulare County Area Transit (TCAT) in rural areas such as Earlimart and by local City transit agencies in transitioning areas, which enables commuters to travel within the communities and adjacent cities with minimal transfers. Existing transit routes and designated bus stops are shown in the following figures.

Tulare County Area Transit (TCAT)

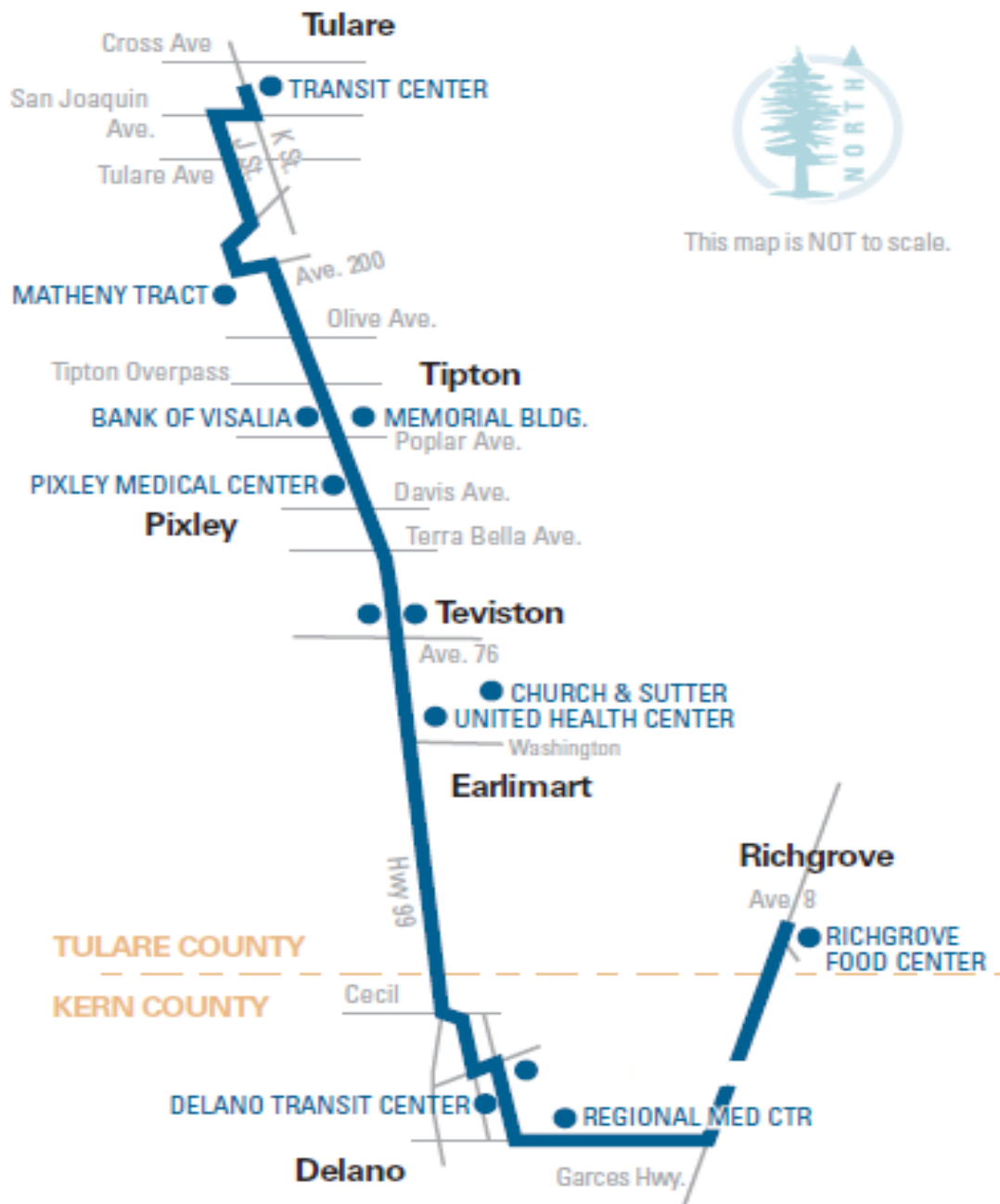
Transit service is provided in Earlimart through the Tulare County Area Transit (TCAT). Additionally, Tulare County has provided guidance for including transit within facilities. These guidelines should be applied when considering new development to ensure appropriate connectivity and design features to support bus service.

Paratransit Service

Paratransit is an alternative mode of passenger transportation that does not follow fixed routes or schedules. Typically, vans or minibuses are used to provide paratransit service. Paratransit services vary considerably on the degree of flexibility they provide their customers. The most flexible systems offer on-demand, call-up, door to door service from any origin to any destination in a service area.

Park-and-Ride Lots

Park-and-ride lots provide places for people to meet up and carpool to areas outside of the Community. A Park and Ride facility could also provide a compressed natural gas refueling station. As the community's



Pass Sales Outlets

Dinuba Transit Center	189 Merced St., Dinuba
Porterville Transit Center	35 W. Oak St., Porterville
Tulare County Government Plaza	5961 S. Mooney Blvd., Visalia
Tulare Transit Center	360 N. 'K' St., Tulare
Visalia Transit Center	Oak & Santa Fe, Visalia

population grows and given the large number of commuters, a park-and-ride location would be best sited near the edges of the Community.

Cost Benefits Analysis, Implementation, and Funding Mechanisms

Caltrans lists the following benefits of Complete Streets in their implementation plan. They include:

- Increased Transportation Choices: Streets that provide travel choices can give people the option to avoid traffic congestion, and increase the overall capacity of the transportation network.
- Economic Revitalization: Complete streets can reduce transportation costs and travel time while increasing property values and job growth in communities.
- Improved Return on Infrastructure Investments: Integrating sidewalks, bike lanes, transit amenities, and safe crossings into the initial design of a project spares the expense of retrofits later.
- Quality of Place: Increased bicycling and walking are indicative of vibrant and livable communities.
- Improved Safety: Design and accommodation for bicyclists and pedestrians reduces the incidence of crashes.
- More Walking and Bicycling: Public health experts are encouraging walking and bicycling as a response to the obesity epidemic. Streets that provide room for bicycling and walking help children get physical activity and gain independence.

Benefits of Complete Streets

Cost of Highway Accident	Dollars Per Accident
---------------------------------	-----------------------------

Fatal Accident	\$4,800,000
Injury Accident	\$67,400
Property Damage Only (PDO) Accident	\$10,200
Average Cost per Accident	\$52,500

Cost of an Event	Dollars Per Event
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Cost of a Fatality	\$4,400,000
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Cost of an Injury

Level A (Severe)	\$221,400
Level B (Moderate)	\$56,500
Level C (Minor)	\$26,900

Cost of Property Damage	\$2,500
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Source: California Department of Transportation

The collision history within the Earlimart area can be traced using the Transportation Injury Management System hosted by UC Berkeley SafeTrec. When reviewing the 5 year injury collision history, a total of three collisions have occurred involving a pedestrian or bicyclist and a motor vehicle. In one pedestrian collision, on Washington St. and Elm Rd a pedestrian was hit when they were on traveling southbound on the road.

Since there are currently no sidewalks many pedestrians walk in the street. Vehicles cannot distinguish roadway with walking paths when curbs do not exist. Any amount of collisions involving pedestrians or bicyclists should be of great concern, especially adjacent to a local school.



West Side of Washington Street.

Factors such as speed, visibility and general pedestrian/bicyclist awareness are identified as contributing factors relating to the incidents. As a result, the project was developed with these specific factors in mind. The proposed design improvements have been proven to be effective and have also been proven to reduce speeds, increase visibility and increase overall awareness of pedestrian activity.

Community Specific Complete Street Implementation Measures

As part of a network-based approach, the County has identified (and will implement through pursuing further roadway studies and infrastructure design updates) a complete network for pedestrians. The County will also work to deliver infrastructure to support all modes of transportation. In addition to the General Plan Circulation Element Implementation Section, the key implementation measures include:

1. Evaluating Roadways as potential Bike/Pedestrian travel routes,
2. Completing pedestrian infrastructure, as appropriate,
3. Providing safe and accessible pedestrian facilities in high use areas,
4. Designing and building sidewalks for safer routes to school,
5. Designating roadways for bicycle routes that are aligned with the Tulare County comprehensive bicycle network,
6. Coordination with County Transit.
7. Submitting the following list of project and cost to TCAG and Caltrans for consideration under further grant funding opportunities.

Measure R

Bike/Transit/Environmental Projects (14% of Measure R Funding)

On November 7, 2006, the voters of Tulare County approved Measure R, imposing a ½ cent sales tax for transportation within the incorporated and unincorporated area of Tulare County for the next 30 years.

The transportation measure will generate slightly more than \$652 million over 30 years to Tulare County's transportation needs.

The Goals of Measure R include air quality improvement efforts that will be addressed in the Measure R Expenditure Plan through the Transit/Bike/Environmental Program, which includes funding for transit, bike, and pedestrian environmental projects. The goal of this program is to expand or enhance public transit programs that address the transit dependent population, improve mobility through the construction of bike lanes, and have a demonstrated ability to get people out of their cars and improve air quality and the environment.

Active Transportation Program (ATP)

On September 26, 2013, Governor Brown signed legislation creating the Active Transportation Program (ATP) in the Department of Transportation (Senate Bill 99, Chapter 359 and Assembly Bill 101, Chapter 354). The ATP consolidates existing federal and state transportation programs, including the Transportation Alternatives Program (TAP), Bicycle Transportation Account (BTA), and State Safe Routes to School (SR2S), into a single program with a focus to make California a national leader in active transportation.

Citizen Feedback

Public Outreach Efforts

The purpose public workshops or community meetings is to engage in discussions with local residents and business owners regarding specific topics, e.g., transportation related improvements. Public outreach efforts were held in several formats including formally and informally. Formal community meetings were held at local schools, community service districts/public utility districts (CSDs/PUDs), town council forums and other well-known locations. Informal meetings were conducted with individual business or property owners associated to specific access concerns or other issues.

Publicity for meeting times and locations generally consisted of newspaper releases, local newsletter informational items, citizens distributing fliers, handing out bi-lingual fliers to school children to be given to the student's guardian, posting fliers at local community businesses, local school board meeting agendas, area congressional office and non-profit agency assistance, local senior centers and health clinics (if applicable), email and other forms of communication. Formal public meetings were held in the various communities shown below. A summary of additional information – Tulare County Resource Management Agency Complete Streets and Community Plan Outreach (2015) – is located in Appendix G.

Earlimart Public Meetings 15 meetings to date, 4 for Complete Streets

- Complete Streets Meeting February 5, 2015
- Complete Streets Meeting March 5, 2015
- Complete Streets Meeting April 9, 2015
- Complete Streets Meeting April 15, 2016



Community feedback was gathered at the February and March meetings and incorporated into the design of the Complete Street Plans and further discussed in the February and March, 2015 meetings to receive further community feedback. These designs were edited to include feasible improvements and cost estimates were assigned to each project within the respective community for each study roadway segment.

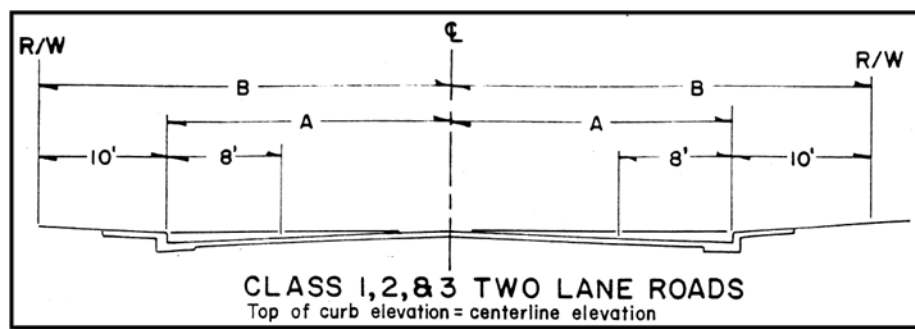
Design Facilities

Improvement Standards

The purpose public workshops or community meetings is to engage in discussions with local residents and business owners regarding specific topics, e.g., transportation related improvements. Transportation related facilities for public use are built within existing right of way (R/W) owned by a public agency,

define the configuration of existing or proposed roadways at right angles to the centerline (CL). Typical sections show the width, thickness and descriptions of the pavement section, as well as the geometrics of the graded roadbed, side improvements and side slopes.

In Tulare County, the two most common cross sections are shown for two or four lane roads, varying in width based upon the number of lanes, parking, sidewalks, shoulders, bike lanes, etc. Figure 1 shows the cross section for two lane roads and Figure 2 identifies a typical four lane cross section.



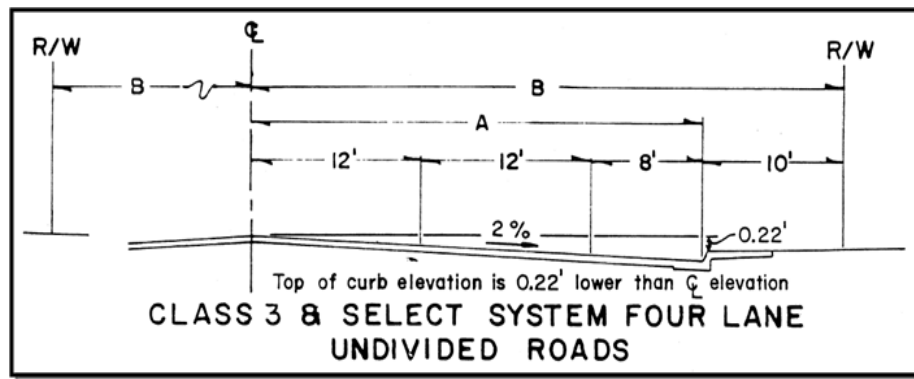


Figure 2 - Tulare County Class 3 Four Lane Road

Tulare County Pavement Management System

Pavement Management

Pavement management is the process of planning the maintenance and repair of a network of roadways or other paved facilities in order to optimize pavement conditions over the entire network. Pavement management incorporates life cycle costs into a more systematic approach to minor and major road maintenance and reconstruction projects. The needs of the entire network as well as budget projections are considered before projects are executed. Pavement management encompasses the many aspects and tasks needed to maintain a quality pavement inventory, and ensure that the overall condition of the road network can be sustained at desired levels.

Pavement Management System

The Tulare County Pavement Management System (PMS) is a planning tool used to aid pavement management decisions. PMS software programs model future pavement deterioration due to traffic and weather, and recommend maintenance and repairs to the road's pavement based on the type and age of the pavement and various measures of existing pavement quality. Measurements can be made by persons on the ground, visually from a moving vehicle, or using automated sensors mounted to a vehicle. PMS software assists RMA staff to create composite pavement quality rankings based on pavement quality measures on roads or road sections. Recommendations are usually biased towards preventive maintenance, rather than allowing a road to deteriorate until it needs more extensive reconstruction.

Typical tasks performed by Tulare County PMS include:

- Inventory pavement conditions, identifying good, fair and poor pavements;
- Assign importance ratings for road segments, based on traffic volumes, road functional class, and community demand;
- Schedule maintenance of good roads to keep them in good condition; and,
- Schedule repairs of poor and fair pavements as remaining available funding allows.

Research has shown that it is far less expensive to keep a road in good condition than it is to repair it once it has deteriorated. This is why pavement management systems place the priority on preventive maintenance of roads in good condition, rather than reconstructing roads in poor condition. In terms of lifetime cost and long term pavement conditions, this will result in better system performance.

The County is proposing a Road Maintenance Plan (see Appendix D) for the community of Earlimart that is a result of the PMS.

Projects

Complete Streets Project Plans

The plans and projects in the appendices are identified as part of the complete streets policy to identify corridors for various user types and to demonstrate examples of design policies. These plans and are the result of input obtained through the community outreach process, multiple Tulare County agencies and divisions and professional engineering consultants.

The six projects identified herein represent the priority improvements to the backbone of the complete streets network within the community of Earlimart. Three of these projects were developed to a 30% design stage and the remaining three projects have been preliminarily scoped and budgetary estimates have been prepared. These six projects were developed to provide the County and various funding agencies with a list of projects to move toward funding, design, and ultimately construction.

- 1) State Street: Ave 56 (Sierra) to Ave 48 (Armstrong)
 - a. Sidewalk
 - b. Curb and Gutter
 - c. Drainage
 - d. Lighting
 - e. To be place on Measure R Complete Streets
- 2) Washington Ave: Rd 128 (Howard Rd) to State St
 - a. Sidewalk
 - b. Curb and Gutter
 - c. Drainage
 - d. Lighting
 - e. To be placed on Measure R Complete Streets
- 3) Washington Ave: State St to east of Elm
 - a. Sidewalk
 - b. Curb and Gutter
 - c. Drainage
 - d. Lighting
 - e. Bike Route (Class III facility)
 - f. To be placed on Measure R Complete Streets
- 4) Church St: Armstrong Ave to Sierra Ave(56)
 - a. Sidewalk
 - b. Curb and Gutter
 - c. Drainage
 - d. Lighting
 - e. To be placed on Measure R Complete Streets
- 5) School Ave: Church St to Elm Rd
 - a. Sidewalk
 - b. Curb and Gutter
 - c. Drainage
 - d. Lighting
 - e. To be placed on Measure R Complete Streets

- 6) Elm Road (Potential Future Alternative Street)
 - a. Sidewalk
 - b. Curb and Gutter
 - c. Drainage
 - d. Lighting
 - e. To be placed on Measure R Complete Streets

Complete Streets Funding Opportunities

The following sections identify opinions of probable cost estimates for Complete Street transportation related improvements in Earlimart. As shown in the tables, the funding sources include local, state and federal programs. Typically, local matches are required for acquiring state and federal funds. Measure R, a Tulare County sales tax for transportation, is available for such matches.

Cost Estimates

Detailed cost estimates are included in Appendix E.

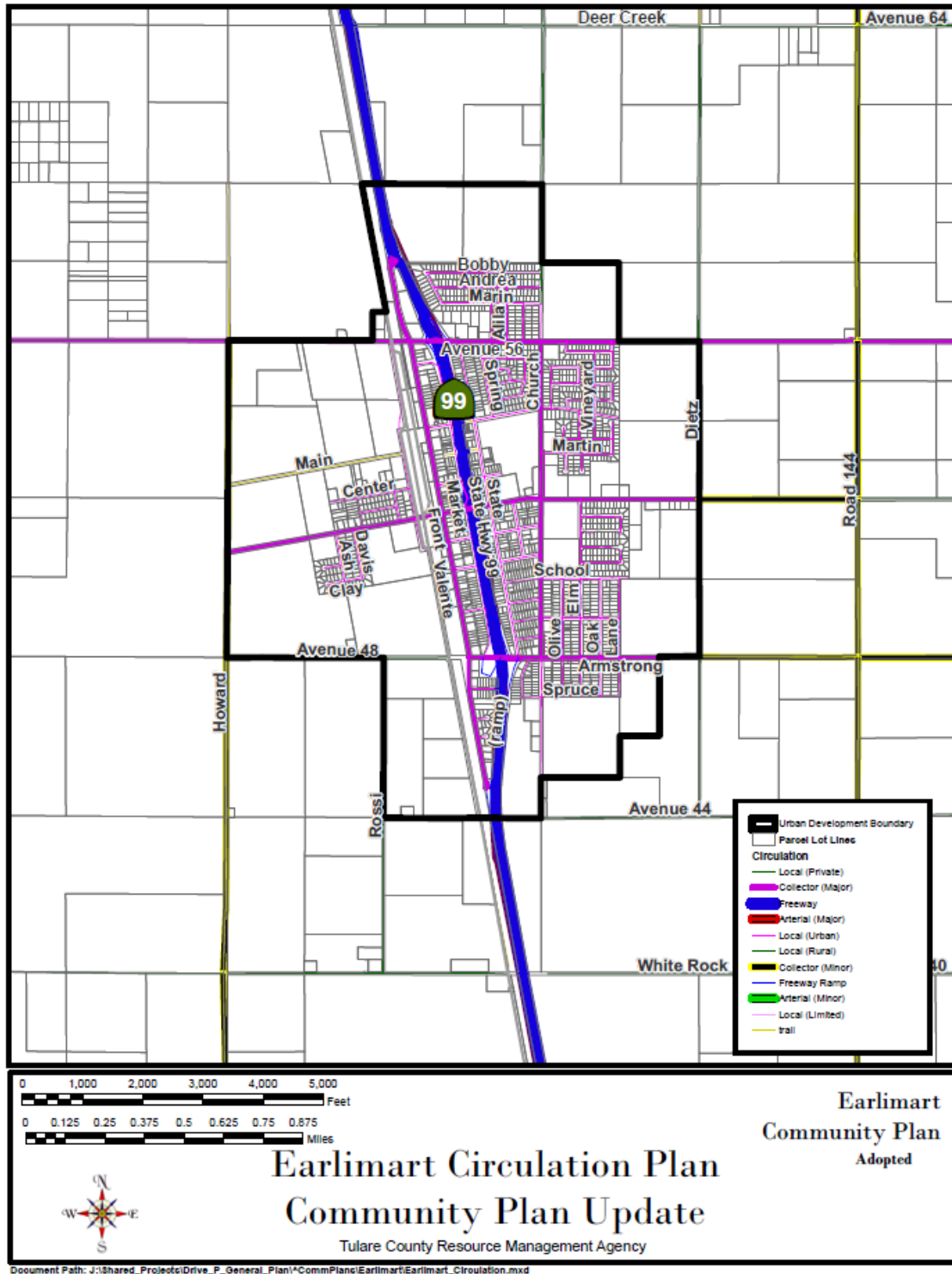
Appendix A – Proposed Complete Streets Projects

- 1) State Street: Ave 56 (Sierra) to Ave 48 (Armstrong)
 - a. Sidewalk
 - b. Curb and Gutter
 - c. Drainage
 - d. Lighting
 - e. To be place on Measure R Complete Streets
- 2) Washington Ave: Rd 128 (Howard Rd) to State St
 - a. Sidewalk
 - b. Curb and Gutter
 - c. Drainage
 - d. Lighting
 - e. To be placed on Measure R Complete Streets
- 3) Washington Ave: State St to east of Elm
 - a. Sidewalk
 - b. Curb and Gutter
 - c. Drainage
 - d. Lighting
 - e. Bike Route (Class III facility)
 - f. To be placed on Measure R Complete Streets
- 4) Church St: Armstrong Ave to Sierra Ave(56)
 - a. Sidewalk
 - b. Curb and Gutter
 - c. Drainage
 - d. Lighting
 - e. To be placed on Measure R Complete Streets
- 5) School Ave: Church St to Elm Rd
 - a. Sidewalk
 - b. Curb and Gutter
 - c. Drainage
 - d. Lighting

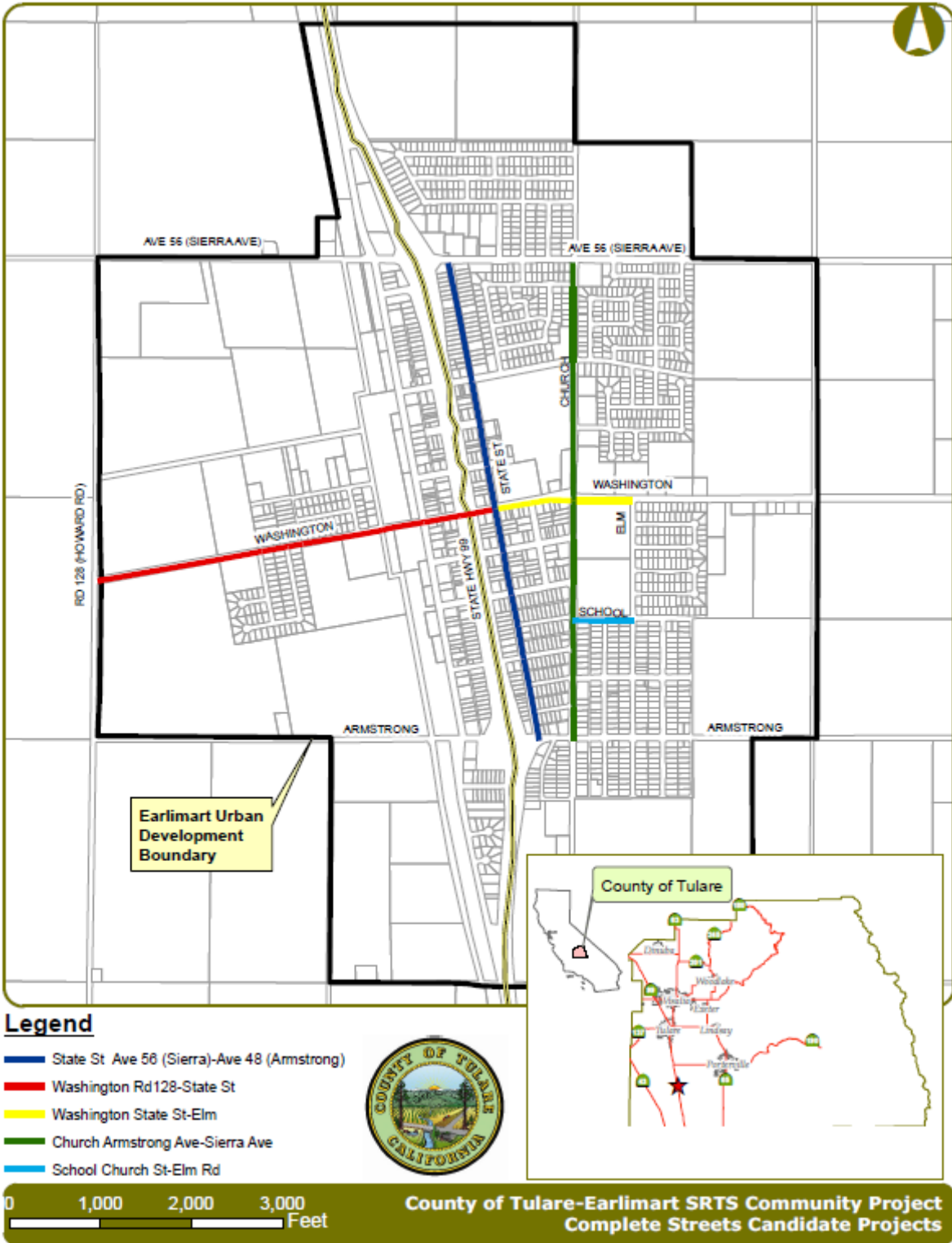
To be placed on Measure R Complete Streets



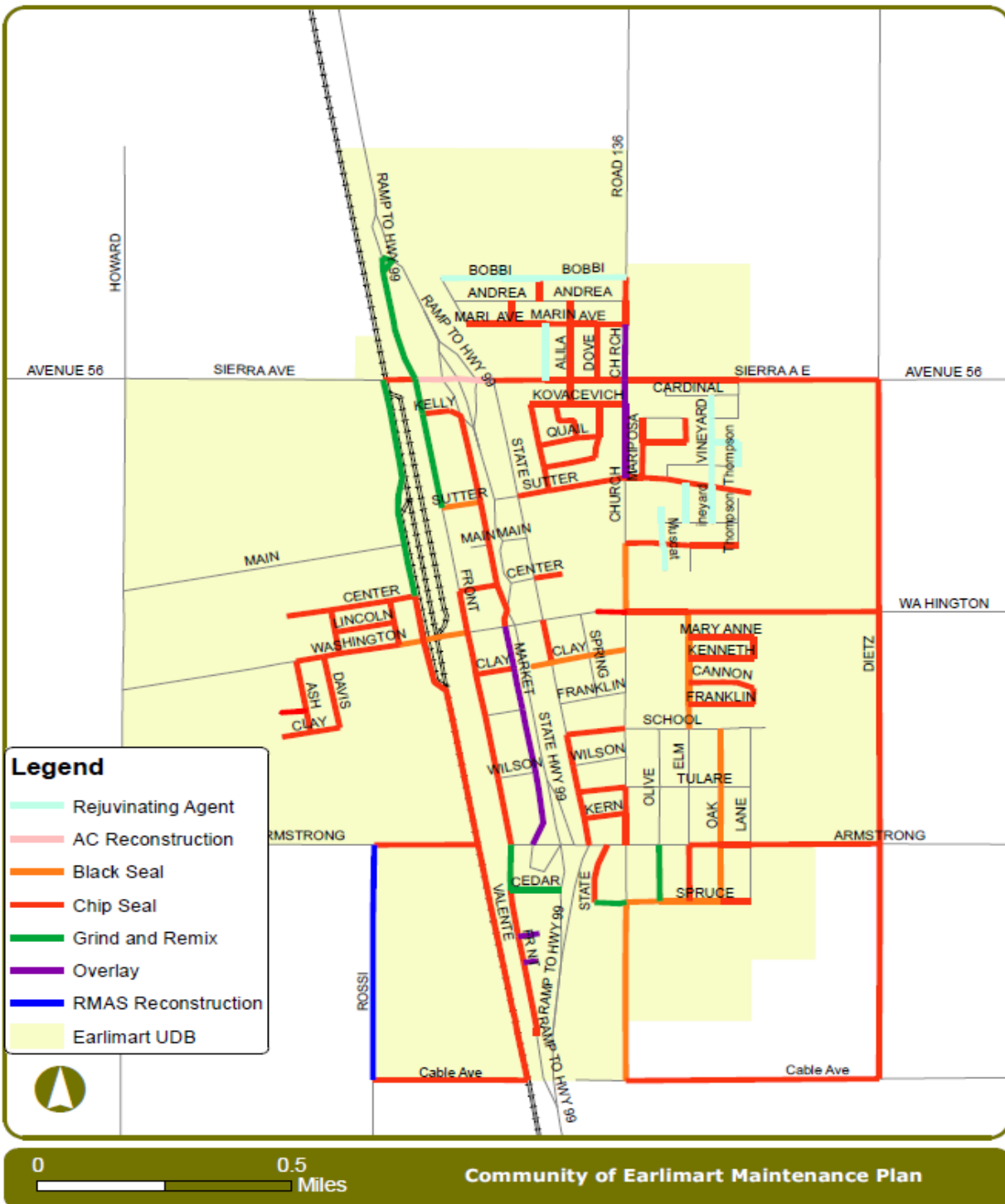
Appendix B – Circulation Plan



Appendix C – Bicycle, Bus, and Pedestrian Plan



Appendix D – Road Maintenance Plan



Cost Estimates for Earlimart: State Street and Washington Road

PRELIMINARY COST ESTIMATE (30-Percent)						
Earlilmar Complete Street Improvements				Date:	11/12/2015	
County of Tulare						
Construction						
ITEM NO	(F)	ITEM DESCRIPTION	UNIT OF MEASURE	ESTIMATED QUANTITY	ITEM PRICE	TOTAL
1	-	MOBILIZATION	LS	1	\$ 250 300.00	\$ 250 300
2	-	JOB SITE MANAGEMENT	LS	1	\$ 8 000.00	\$ 8 000
3	-	WATER POLLUTION CONTROL PROGRAM	LS	1	\$ 4 500.00	\$ 4 500
4	-	CONSTRUCTION AREA SIGNS	LS	1	\$ 10 000.00	\$ 10 000
5	-	TRAFFIC CONTROL SYSTEM	LS	1	\$ 30 000.00	\$ 30 000
6	-	STORM DRAIN BASIN EXCAVATION	LS	1	\$ 50 000.00	\$ 50 000
7	-	RELOCATE FENCE (CHAIN LINK)	LF	976	\$ 50.00	\$ 48 805
8	-	RELOCATE FENCE (WROUGHT IRON)	LF	632	\$ 90.00	\$ 56 895
9	-	RELOCATE FENCE (BRICK AND WROUGHT IRON)	LF	202	\$ 150.00	\$ 30 342
10	-	REMOVE CULVERT	LF	283	\$ 100.00	\$ 28 322
11	-	RESET SIGN	EA	11	\$ 500.00	\$ 5 500
12	-	RELOCATE SIGN	EA	12	\$ 500.00	\$ 6 000
13	-	RELOCATE FIRE HYDRANT	EA	3	\$ 5 500.00	\$ 16 500
14	-	RELOCATE WATER METER	EA	2	\$ 3 500.00	\$ 7 000
15	-	RELOCATE WATER VALVE	EA	1	\$ 1 000.00	\$ 1 000
16	-	REMOVE SHRUB	EA	1	\$ 100.00	\$ 100
17	-	REMOVE TREE	EA	2	\$ 800.00	\$ 1 600
18	-	REMOVE DRAIN INLET	EA	9	\$ 100.00	\$ 900
19	-	ADJUST DRAIN INLET TO GRADE	EA	4	\$ 1 500.00	\$ 6 000
20	-	ADJUST WATER VALVE COVER TO GRADE	EA	9	\$ 800.00	\$ 7 200
21	-	ADJUST FIRE HYDRANT TO GRADE	EA	6	\$ 2 000.00	\$ 12 000
22	-	ADJUST WATER METER BOX TO GRADE	EA	23	\$ 3 500.00	\$ 80 500
23	-	ADJUST MANHOLE TO GRADE	EA	1	\$ 1 200.00	\$ 1 200
24	-	ABANDON STORM DRAIN	EA	3	\$ 1 500.00	\$ 4 500
25	-	CLEARING AND GRUBBING	LS	1	\$ 20 000.00	\$ 20 000
26	-	2" BARK MULCH	SQFT	311	\$ 35.00	\$ 10 877
27	-	48" STORM DRAIN MANHOLE	EA	7	\$ 6 000.00	\$ 42 000
28	(F)	ROADWAY EXCAVATION	CY	4 955	\$ 60.00	\$ 297 316
29	(F)	CLASS 2 AGGREGATE BASE	CY	2 139	\$ 60.00	\$ 128 326
30	-	HOT MIX ASPHALT (TYPE A)	TON	1 353	\$ 110.00	\$ 148 878
31	-	15" REINFORCED CONCRETE PIPE	LF	2 867	\$ 165.00	\$ 473 037
32	-	MINOR CONCRETE (MINOR STRUCTURE-TYPE GO INLET)	EA	9	\$ 3 500.00	\$ 31 500
33	-	MINOR CONCRETE (CURB AND GUTTER)	LF	9 676	\$ 25.00	\$ 241 888
34	-	MINOR CONCRETE (VEE GUTTER)	SQFT	2 711	\$ 10.00	\$ 27 106
35	-	MINOR CONCRETE (SIDEWALK)	SQFT	72 885	\$ 7.00	\$ 510 195
36	-	MINOR CONCRETE (DRIVEWAYS AND CURB RAMPS)	SQFT	12 625	\$ 15.00	\$ 189 374
37	-	DETECTABLE WARNING SURFACE	SQFT	459	\$ 45.00	\$ 20 647
38	-	SIGNING & STRIPING	LS	1	\$ 25 000.00	\$ 25 000
39	-	EROSION CONTROL	LS	1	\$ 10 000.00	\$ 10 000
40	-	MISCELLANEOUS ITEMS	LS	1	\$ 648 252.00	\$ 648 252
		Total - Construction Items 1-40				\$3 500 560
		Contingency (25%)				\$ 875 140
		Recommended Total Construction Budget				\$4 375 700
Non-Construction Related Costs						
ITEM NO	(F)	ITEM DESCRIPTION	UNIT OF MEASURE	ESTIMATED QUANTITY	ITEM PRICE	TOTAL
41	-	Environmental Clearance	% of CON	5%	\$3 500 560.00	\$ 175 028
42	-	Right of Way Acquisition (Capital)	LS	1	\$ 1 000.00	\$ 1 000
43	-	Right of Way Acquisition (Support)	Parcel	1	\$ 5 000.00	\$ 5 000
44	-	Final Engineering Design	% of CON	15%	\$3 500 560.00	\$ 525 084
45	-	Construction Support	% of CON	2%	\$3 500 560.00	\$ 70 011
46	-	Construction Management	% of CON	15%	\$3 500 560.00	\$ 525 084
47	-	Utility Relocations	LS	1	\$ 150 000.00	\$ 150 000
		Total - Non-Construction Items 41-47				\$1 451 207
		Total Construction & Non-Construction Items				\$5 826 908

Detailed Engineer's Estimate						
	Agency:	Tulare County Resource Management Agency				
	Project Name:	Complete Streets Phase 3 Earlimart				
	Project Location:	Washington Ave - Alila School to Elm St				
	Date of Estimate:	December 8, 2016				
	Prepared by:	Pedro Ornelas/Diego Corvera				
Construction Items						
Item No.	Caltrans Item code	Description	Units	Quantity	Unit Cost	Total
1	999990	Mobilization	LS	1	\$100,000	\$100,000
2	050126	Construction Staking	LS	1	\$20,000	\$20,000
3	120090	Construction Area Signs	LS	1	\$25,000	\$25,000
4	120100	Traffic control system	LS	1	\$25,000	\$25,000
5	120300	Temporary Pavement Marker (Refl.)	LS	1	\$20,000	\$20,000
6	130200	Prepare Water Pollution Control Program	LS	1	\$10,000	\$10,000
7	220101	Finishing Roadway	LS	1	\$15,000	\$15,000
8	150712	Remove Painted Pavement Markings	SQFT	1380	\$5	\$6,900
9	152390	Remove Roadside Sign	EA	25	\$200	\$5,000
10	152379	Relocate Fence	LF	1790	\$30	\$53,700
11(F)	190101	Roadway excavation	CY	2,078	\$100	\$207,800
12(F)	250201	Class 2 Aggregate Base	CY	997	\$95	\$94,736
13	390133	Hot Mix Asphalt (Type B)	TON	1250	\$120	\$150,000
14	394090	Place hot mix asphalt (miscellaneous area)	SQYD	30	\$100	\$3,000
15	731504	Minor Concrete (Curb & Gutter)	LF	3200	\$25	\$80,000
16	731521	Minor Concrete (Sidewalk)	SQFT	14438	\$8	\$115,504
17	731623	Minor Concrete (Ramp)	EA	27	\$4,000	\$108,000
18	731516	Minor Concrete (Driveway)	SF	5310	\$15	\$79,650
19	510502	Minor Concrete (Minor Structures)	EA	8	\$5,000	\$40,000
20		Storm Drain Pipe	LF	150	\$100	\$15,000
21	840515	Thermoplastic Pavement Marking (Yellow)	SF	1730	\$10	\$17,300
22	152469	Adjust Utility Cover to Grade	EA	12	\$800	\$9,600
23	568023	Install Roadside Sign	EA	25	\$500	\$12,500
24		Remove Tree	EA	2	\$1,000	\$2,000
25		Minor Concrete (Continous Gutter)	LF	100	\$50	\$5,000
26		Pedestrian RxR Crossing Gates	EA	4	\$100,000	\$400,000
Sub-Total:						\$1,620,690
* Up to 10% Contingency may be included in Engineer's Estimate						
*Contingency:						\$162,069
Construction Total:						\$1,782,759
(F) = Final Pay Item						

(F) = Final Pay Item

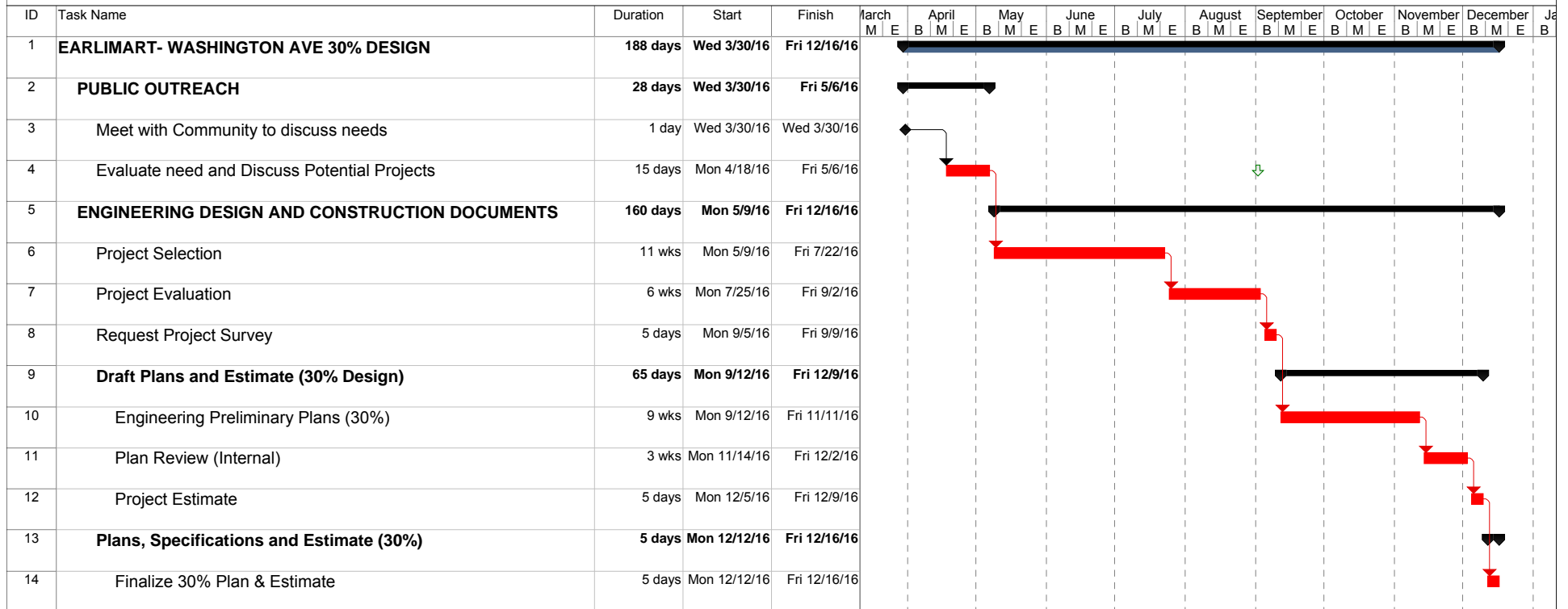
Non-Construction Related Cost						
Item No.	Caltrans Item code	Description	Units	Quantity	Unit Cost	Total
27	-	Environmental Clearance	% of CON	5%	\$1,620,690.11	\$81,034.51
28	-	Preliminary Engineering (PE)	% of CON	10%	\$1,620,690.11	\$162,069.01
29	-	Construction Engineering (CE)	% of CON	15%	\$1,620,690.11	\$243,103.52
30	-	Utility Pole Relocations	EA	9	\$25,000.00	\$225,000.00
31	-	Cooridination with Railroad	LS	1	\$20,000.00	\$20,000.00
Total:						\$731,207.03

Total Construction & Non-Construction Items					\$2,513,966.16
--	--	--	--	--	-----------------------

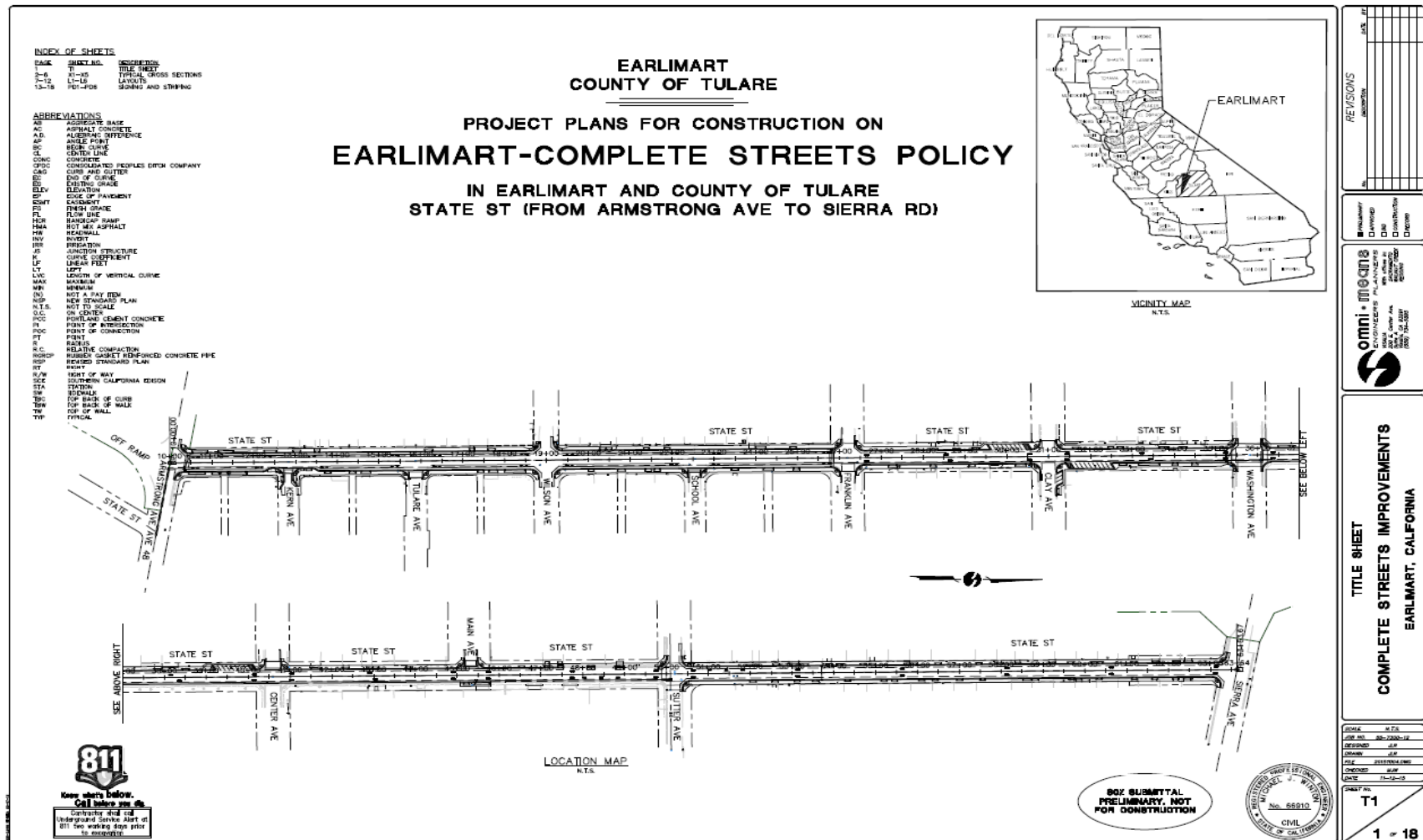
Complete Street Phase III Earlimart - Washington Ave 30% Design

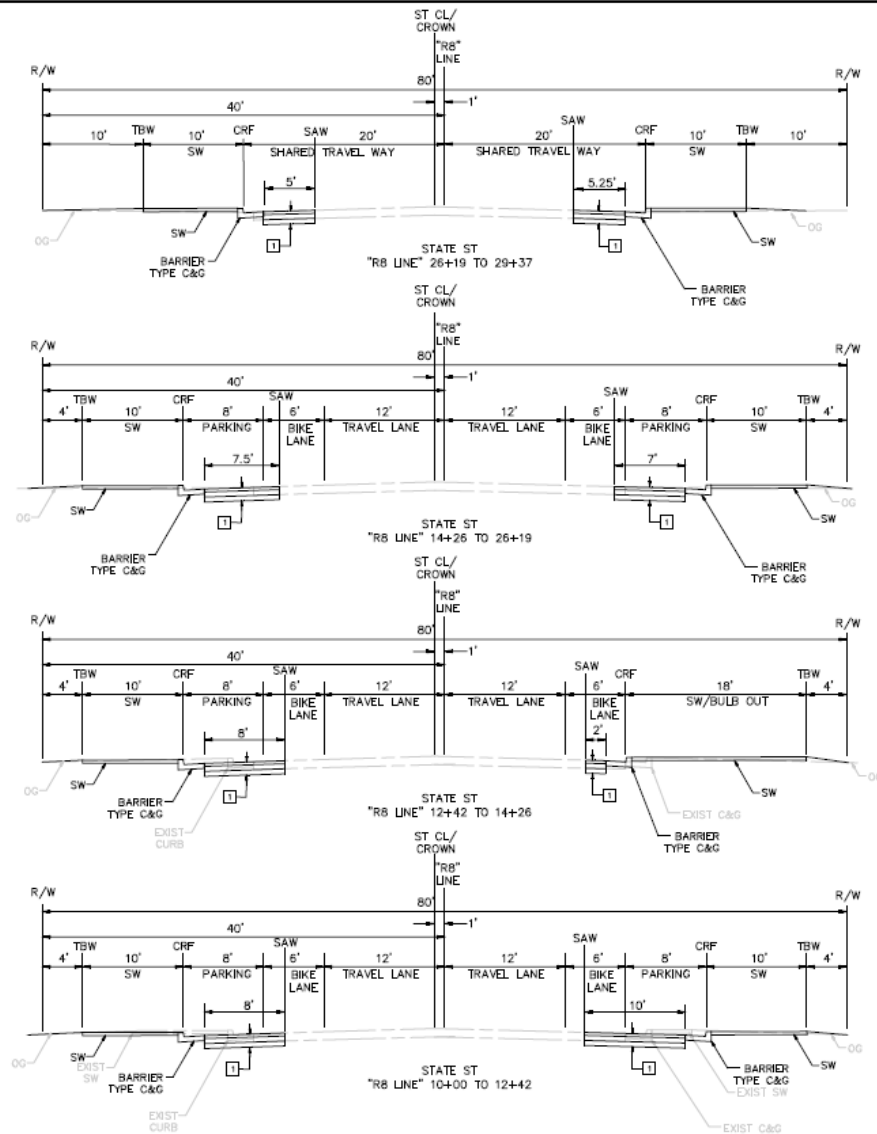
PROJECT SCHEDULE

8/22/2016



Appendix F - Earlimart 30% Submittal Plan Set





TYPICAL STRUCTURAL SECTIONS

1

30-YEAR DESIGN
 11-0.5, R=20
 0.25' HMA (TYPE B)
 0.80' AB (CLASS 2)
 0.50' COMPACTED NATIVE (95% R.C.)

807 SUBMITTAL
 PRELIMINARY, NOT
 FOR CONSTRUCTION



REVISIONS	DATE	BY	CHKD

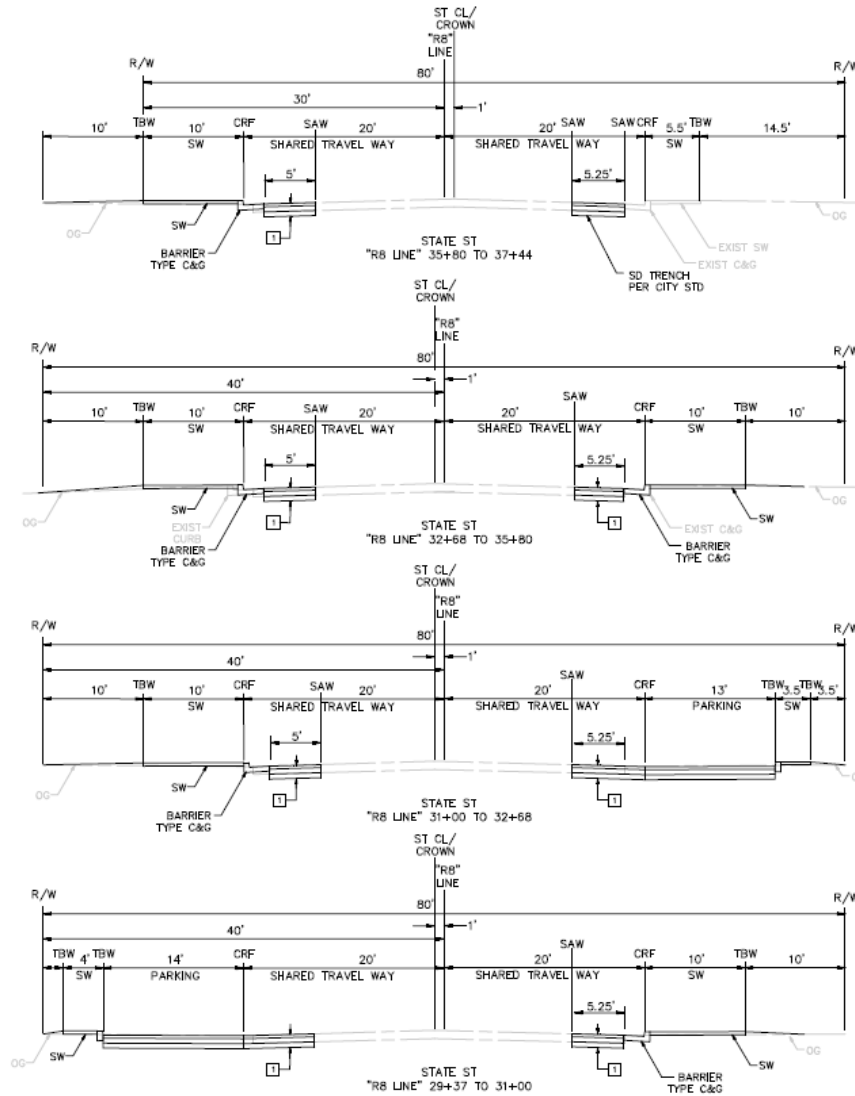
DESIGNED	CHKD	IN CHARGE	DATE



TYPICAL SECTIONS
COMPLETE STREETS IMPROVEMENTS
EARLHART, CALIFORNIA

SCALE	1" = 2'
DATE	02-10-2019
DESIGNED	
DRAWN	
IN CHARGE	
CHECKED	
DATE	02-10-2019

SHEET NO.	X1
2 OF 18	



TYPICAL STRUCTURAL SECTIONS:

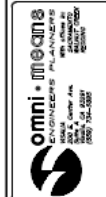
1 20-YEAR DESIGN
 TI=0.5, R=20
 0.25' HMA (TYPE B)
 0.80' AS (CLASS 2)
 0.50' COMPACTED NATIVE (95% R.C.)

80% SUBMITTAL
 PRELIMINARY, NOT
 FOR CONSTRUCTION



REVISIONS	
NO.	DESCRIPTION

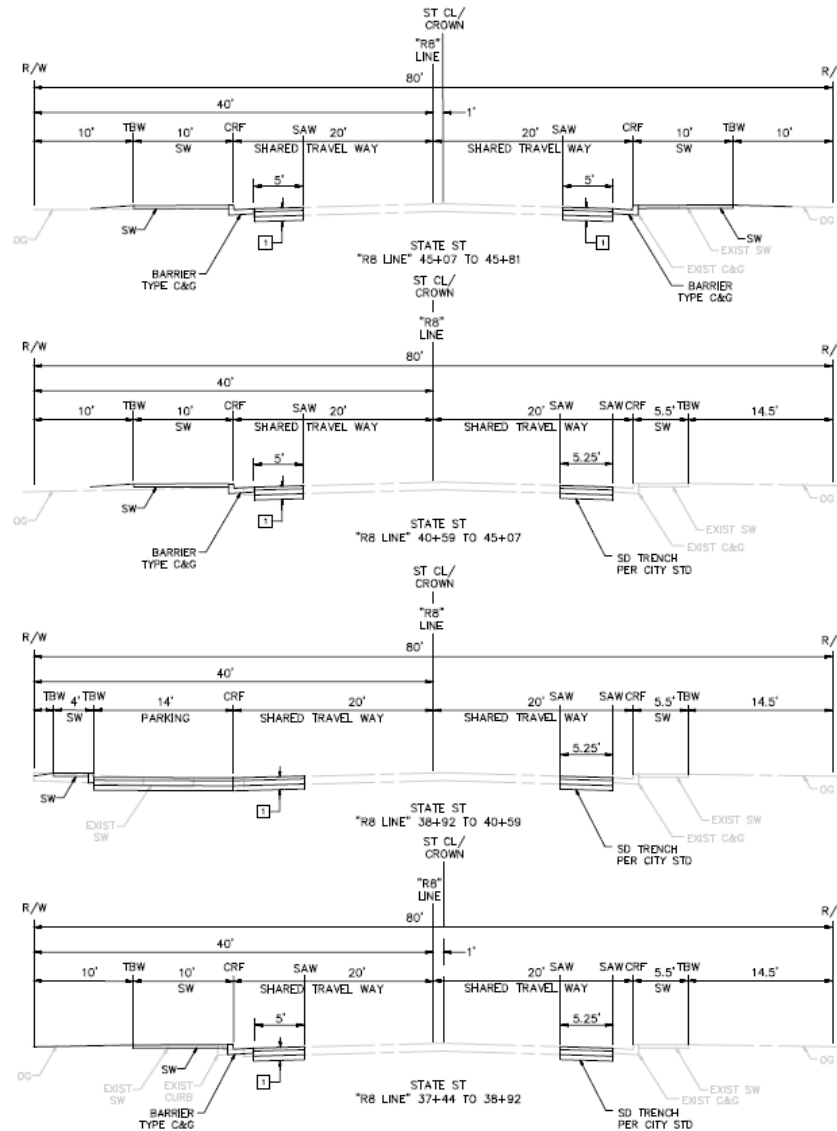
Prepared	Checked
Drawn	Reviewed



TYPICAL SECTIONS
 COMPLETE STREETS IMPROVEMENTS
 EARLMART, CALIFORNIA

SCALE	1" = 10'
JOB NO.	20-2000-10
DESIGNED	FJD
DRAWN	FJD
FILE	20100001.DWG
CHECKED	AWW
DATE	10-10-18

SHEET NO. **X2**
3 of 18



TYPICAL STRUCTURAL SECTIONS:
 30-YEAR DESIGN
 TR=5.5, R=20
 0.25' RMA (TYPE B)
 0.80' AB (CLASS 2)
 2.50' COMPACTED NATIVE (95% R.C.)

807 SUBMITTAL
 PRELIMINARY, NOT
 FOR CONSTRUCTION



REVISIONS	DATE	BY	CHK

DESIGNED BY: [Signature]
 CHECKED BY: [Signature]
 DATE: 11-12-18



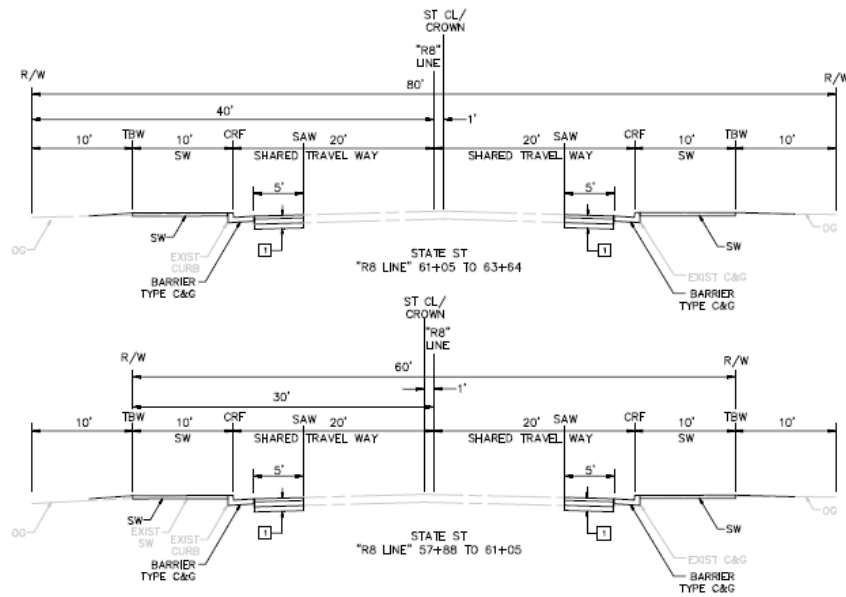
TYPICAL SECTIONS
 COMPLETE STREETS IMPROVEMENTS
 EARLMART, CALIFORNIA

SCALE	1" = 1'
DATE	11-12-18
DESIGNED	MD
CHECKED	MD
DATE	11-12-18

SHEET NO. **X3**
 4 of 18



X4
5 of 18



TYPICAL STRUCTURAL SECTIONS:

- 20-YEAR DESIGN
- T=0.5, S=20
- 0.25" HMA (TYPE B)
- 0.80" AB (CLASS 2)
- 0.50' COMPACTED NATIVE (95% R.C.)

80% SUBMITTAL
PRELIMINARY, NOT
FOR CONSTRUCTION



REVISIONS	
NO.	DESCRIPTION

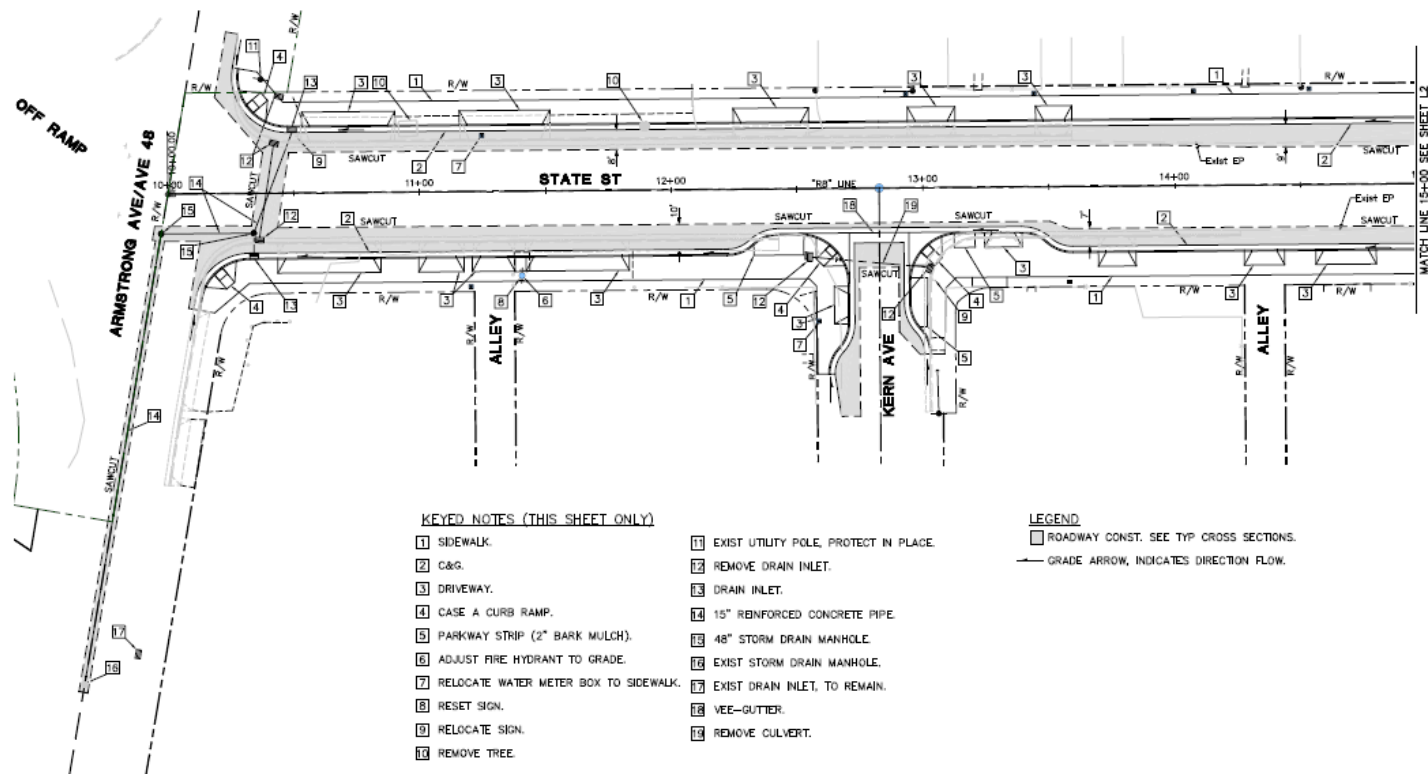
DESIGNED	CHECKED
DRAWN	CONSTRUCTED



TYPICAL SECTIONS
COMPLETE STREETS IMPROVEMENTS
EARLMART, CALIFORNIA

SCALE	1" = 5'
DATE	02-20-2019
DESIGNED	C&G
DRAWN	C&G
CHECKED	2/20/2019/MS
DATE	02-20-2019

SHEET NO.
X5
6 OF 18



KEYED NOTES (THIS SHEET ONLY)

- | | |
|---|--|
| 1 SIDEWALK. | 11 EXIST UTILITY POLE, PROTECT IN PLACE. |
| 2 C&G. | 12 REMOVE DRAIN INLET. |
| 3 DRIVEWAY. | 13 DRAIN INLET. |
| 4 CASE A CURB RAMP. | 14 15" REINFORCED CONCRETE PIPE. |
| 5 PARKWAY STRIP (2" BARK MULCH). | 15 48" STORM DRAIN MANHOLE. |
| 6 ADJUST FIRE HYDRANT TO GRADE. | 16 EXIST STORM DRAIN MANHOLE. |
| 7 RELOCATE WATER METER BOX TO SIDEWALK. | 17 EXIST DRAIN INLET, TO REMAIN. |
| 8 RESET SIGN. | 18 VEE-GUTTER. |
| 9 RELOCATE SIGN. | 19 REMOVE CULVERT. |
| 10 REMOVE TREE. | |

LEGEND

- ROADWAY CONST. SEE TYP CROSS SECTIONS.
 GRADE ARROW, INDICATES DIRECTION FLOW.

80% SUBMITTAL
PRELIMINARY, NOT
FOR CONSTRUCTION



REVISIONS

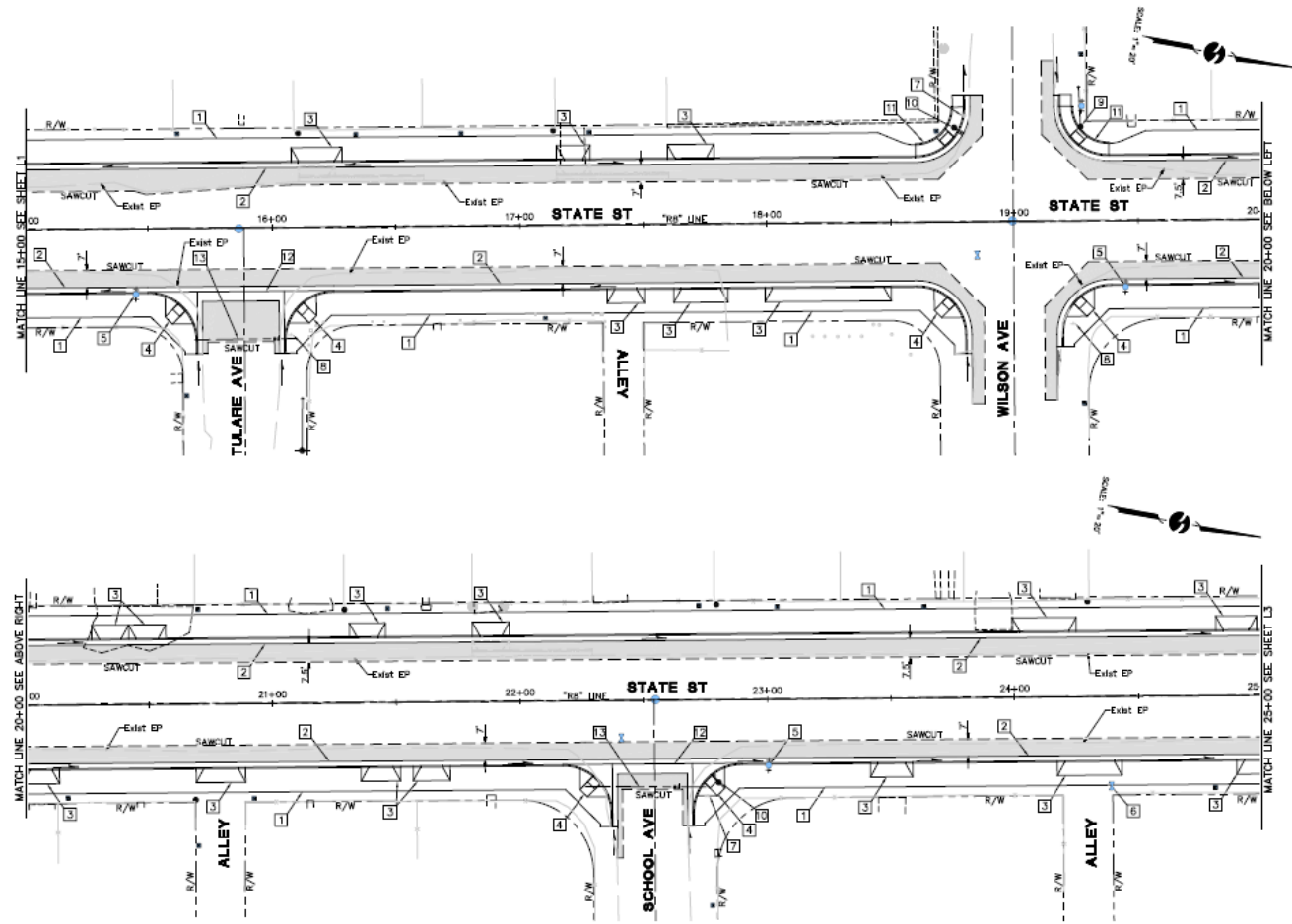
NO.	DATE	DESCRIPTION

omni ENGINEERS PLANNERS
 1000 N. GARDEN AVE.
 SUITE 200
 ANAHEIM, CA 92810
 (714) 771-0000
 FAX (714) 771-0001

LAYOUT
COMPLETE STREETS IMPROVEMENTS
EARLMART, CALIFORNIA

SCALE	1"=50'
JOB NO.	80-0000-10
DESIGNED	JAB
DRAWN	JAB
CHECKED	JAB
DATE	10-10-00

L1
7 of 18



KEYED NOTES (THIS SHEET ONLY)

- | | |
|--------------------------------------|---|
| 1 SIDEWALK. | 8 RESET SIGN. |
| 2 C&G. | 9 EXIST UTILITY POLE, PROTECT IN PLACE. |
| 3 DRIVEWAY. | 10 RELOCATE UTILITY POLE. |
| 4 CASE A CURB RAMP. | 11 CASE C CURB RAMP. |
| 5 RELOCATE FIRE HYDRANT. | 12 VEE-GUTTER. |
| 6 ADJUST WATER VALVE COVER TO GRADE. | 13 REMOVE CULVERT. |
| 7 RELOCATE SIGN. | |

LEGEND

- ROADWAY CONST. SEE TYP CROSS SECTIONS.
- GRADE ARROW, INDICATES DIRECTION FLOW.

**30% SUBMITTAL
PRELIMINARY, NOT
FOR CONSTRUCTION**



REVISIONS

NO.	DATE	DESCRIPTION

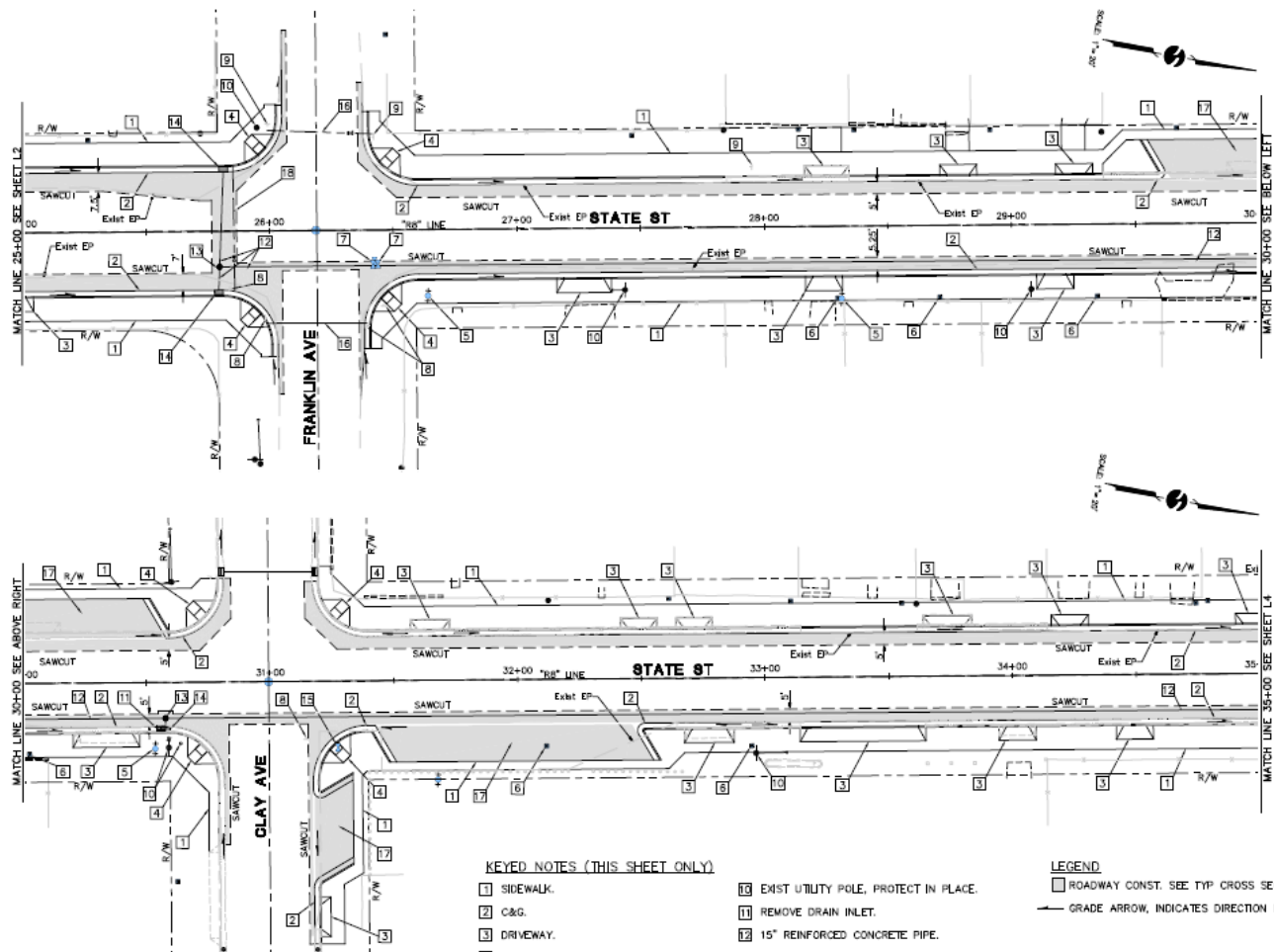
Prepared	Checked
Drawn	Reviewed

omni-mechanics
ENGINEERS PLANNERS
ARCHITECTS
2025 S. GATE AVENUE
SUITE 100
POMONA, CA 91768
(951) 791-1000

**LAYOUT
COMPLETE STREETS IMPROVEMENTS
EARLMART, CALIFORNIA**

DATE	1/15/15
JOB NO.	20-1280-15
DESIGNED	J.W.
DRAWN	J.W.
CHECKED	J.W.
DATE	1/15/15

SHEET NO.
L2
8 OF 18



KEYNOTES (THIS SHEET ONLY)

- | | |
|--------------------------------------|--|
| 1 SIDEWALK. | 16 EXIST UTILITY POLE, PROTECT IN PLACE. |
| 2 C&G. | 17 REMOVE DRAIN INLET. |
| 3 DRIVEWAY. | 18 15" REINFORCED CONCRETE PIPE. |
| 4 CASE A CURB RAMP. | 19 48" STORM DRAIN MANHOLE. |
| 5 ADJUST FIRE HYDRANT TO GRADE. | 20 DRAIN INLET. |
| 6 ADJUST WATER METER BOX TO GRADE. | 21 RELOCATE WATER VALVE COVER. |
| 7 ADJUST WATER VALVE COVER TO GRADE. | 22 ABANDON STORM DRAIN. |
| 8 RELOCATE SIGN. | 23 PARKING. |
| 9 RESET SIGN. | 24 REMOVE CULVERT. |

LEGEND

- ROADWAY CONST. SEE TYP CROSS SECTIONS.
 GRADE ARROW, INDICATES DIRECTION FLOW.

**SOX SUBMITTAL:
 PRELIMINARY, NOT
 FOR CONSTRUCTION**



REVISIONS

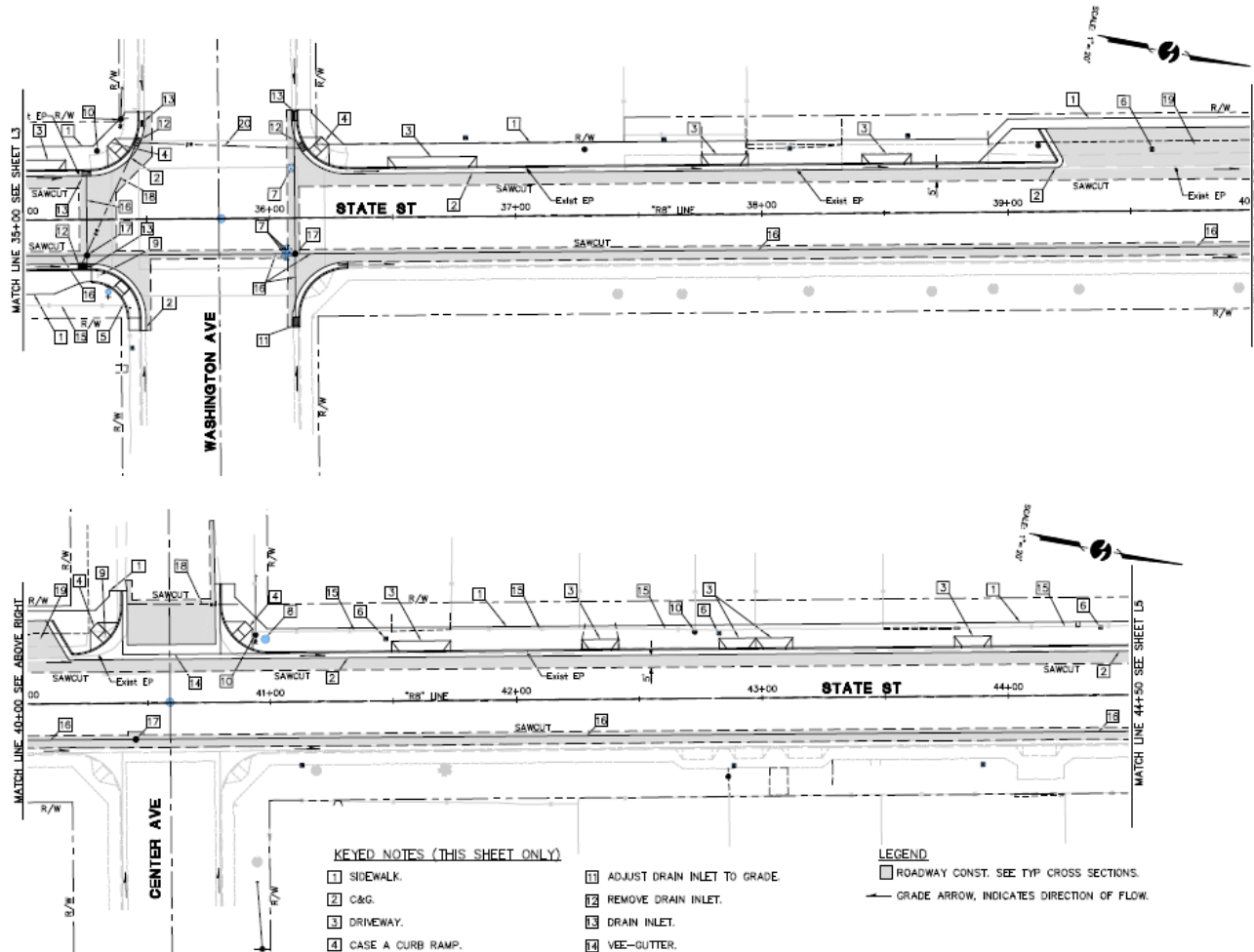
NO.	DATE	DESCRIPTION

omni
 ENGINEERING
 1000 S. GATEWAY AVENUE, SUITE 200
 SAN ANTONIO, TEXAS 78204
 (214) 520-1000

**LAYOUT
 COMPLETE STREETS IMPROVEMENTS
 EARLMART, CALIFORNIA**

DATE	11-10-18
DESIGNED	WJW
DRAWN	WJW
CHECKED	WJW
DATE	11-10-18

L3
 9 of 18



KEYED NOTES (THIS SHEET ONLY)

- | | |
|--|----------------------------------|
| 1 SIDEWALK. | 11 ADJUST DRAIN INLET TO GRADE. |
| 2 C&G. | 12 REMOVE DRAIN INLET. |
| 3 DRIVEWAY. | 13 DRAIN INLET. |
| 4 CASE A CURB RAMP. | 14 VEE-GUTTER. |
| 5 CASE C CURB RAMP. | 15 RELOCATE FENCE (CHAIN LINK). |
| 6 ADJUST WATER METER BOX TO GRADE. | 16 15" REINFORCED CONCRETE PIPE. |
| 7 ADJUST WATER VALVE COVER TO GRADE. | 17 48" STORM DRAIN MANHOLE. |
| 8 ADJUST MANHOLE TO GRADE. | 18 REMOVE CULVERT. |
| 9 RELOCATE SIGN. | 19 PARKING. |
| 10 EXIST UTILITY POLE. PROTECT IN PLACE. | 20 ABANDON STORM DRAIN. |

LEGEND

- ROADWAY CONST. SEE TYP CROSS SECTIONS.
 GRADE ARROW, INDICATES DIRECTION OF FLOW.

**802 SUBMITTAL
PRELIMINARY, NOT
FOR CONSTRUCTION**



REVISIONS

NO.	DATE	DESCRIPTION

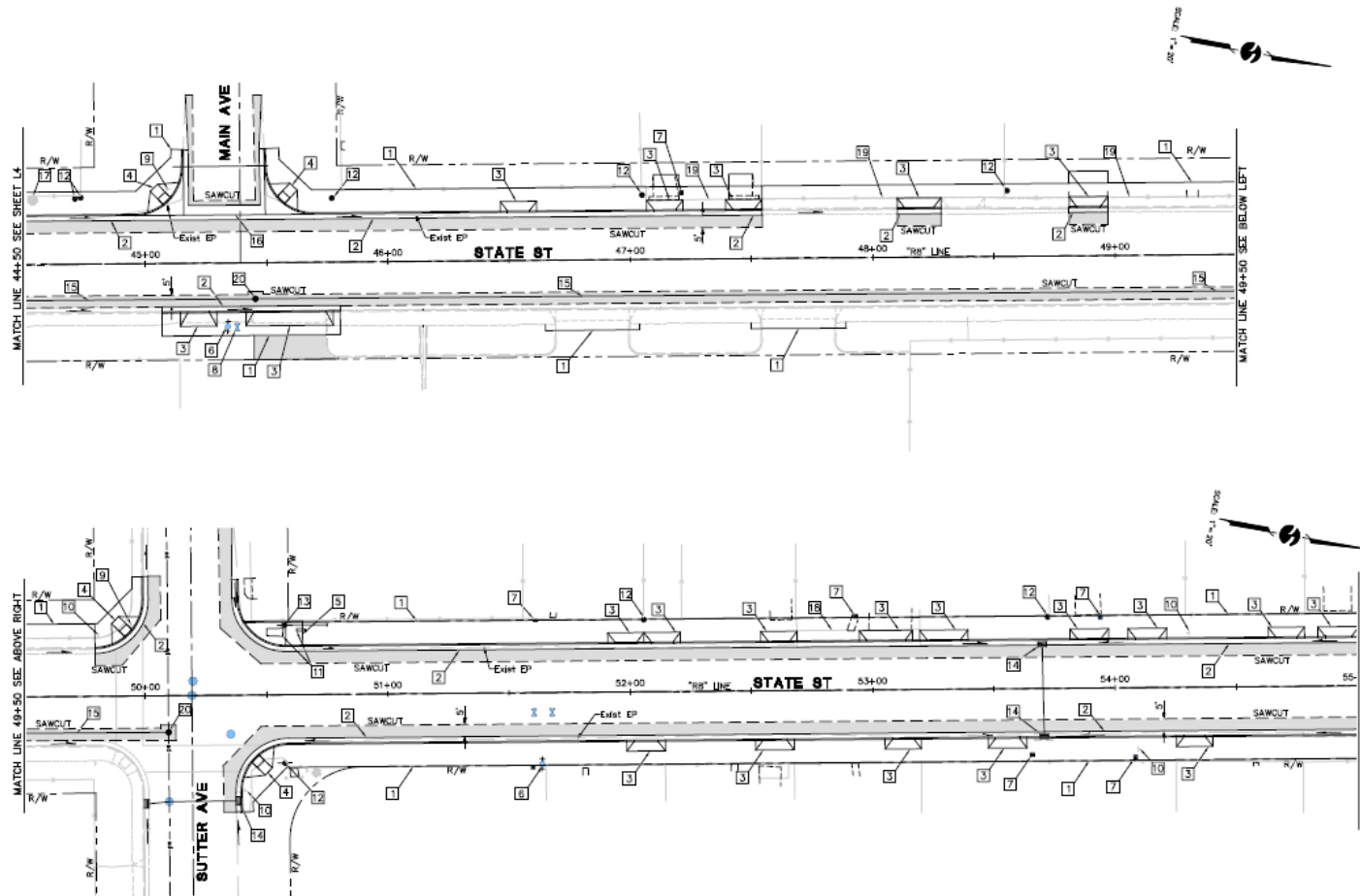
☒ PREPARED
☐ CHECKED
☐ DESIGNED
☐ IN CHARGE



**LAYOUT
COMPLETE STREETS IMPROVEMENTS
EARLMART, CALIFORNIA**

SCALE	1"=50'
JOB NO.	802-2000-L4
DESIGNED	2/01
DRAWN	KJP
DATE	02/01/01
CHECKED	MAJ
DATE	02/01/01

L4



KEYED NOTES (THIS SHEET ONLY)

- | | | |
|------------------------------------|--|-----------------------------------|
| 1 SIDEWALK. | 8 ADJUST WATER VALVE COVER TO GRADE. | 15 15" REINFORCED CONCRETE PIPE. |
| 2 C&G. | 9 RELOCATE SIGN. | 16 VEE-GUTTER. |
| 3 DRIVEWAY. | 10 RESET SIGN. | 17 REMOVE TREE. |
| 4 CASE A CURB RAMP. | 11 RELOCATE CATV PIEDISTAL. | 18 RELOCATE FENCE (CHAIN LINK). |
| 5 CASE CM CURB RAMP. | 12 EXIST UTILITY POLE, PROTECT IN PLACE. | 19 RELOCATE FENCE (WROUGHT IRON). |
| 6 ADJUST FIRE HYDRANT TO GRADE. | 13 RELOCATE UTILITY POLE. | 20 48" STORM DRAIN MANHOLE. |
| 7 ADJUST WATER METER BOX TO GRADE. | 14 ADJUST DRAIN INLET TO GRADE. | |

LEGEND

- ROADWAY CONST. SEE TYP CROSS SECTIONS.
 GRADE ARROW, INDICATES DIRECTION OF FLOW.

**DO NOT SUBMITTAL
PRELIMINARY, NOT
FOR CONSTRUCTION**



REVISIONS	
NO.	DESCRIPTION

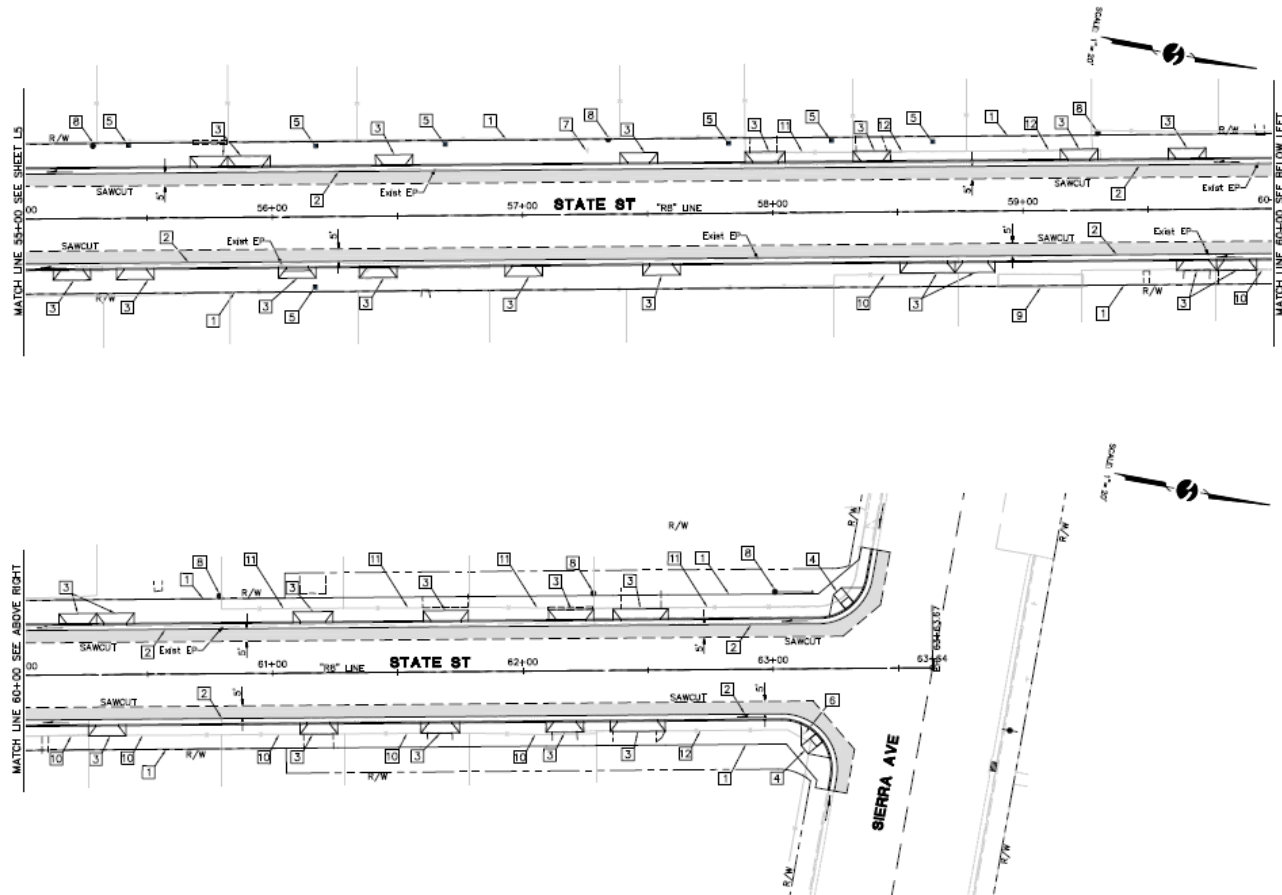
Prepared	Checked
Designed	Reviewed
Drawn	Approved
Instructed	



**LAYOUT
COMPLETE STREETS IMPROVEMENTS
EARMART, CALIFORNIA**

SCALE	1"=50'
DATE	08-20-2014
DESIGNED	EAD
DRAWN	EAD
CHECKED	AWW
DATE	11-13-13

L6
11 of 18



KEYED NOTES (THIS SHEET ONLY)

- | | |
|------------------------------------|---|
| 1 SIDEWALK. | 8 EXIST UTILITY POLE, PROTECT IN PLACE. |
| 2 C&G. | 9 REMOVE SHRUBS. |
| 3 DRIVEWAY. | 10 RELOCATE FENCE (CHAIN LINK). |
| 4 CASE A CURB RAMP. | 11 RELOCATE FENCE (WROUGHT IRON). |
| 5 ADJUST WATER METER BOX TO GRADE. | 12 RELOCATE FENCE (BRICK AND WROUGHT IRON). |
| 6 RELOCATE SIGN. | |
| 7 RESET SIGN. | |

LEGEND

- | | |
|--|---|
| | ROADWAY CONST. SEE TYP CROSS SECTIONS. |
| | GRADE ARROW, INDICATES DIRECTION OF FLOW. |

30% SUBMITTAL
PRELIMINARY, NOT
FOR CONSTRUCTION



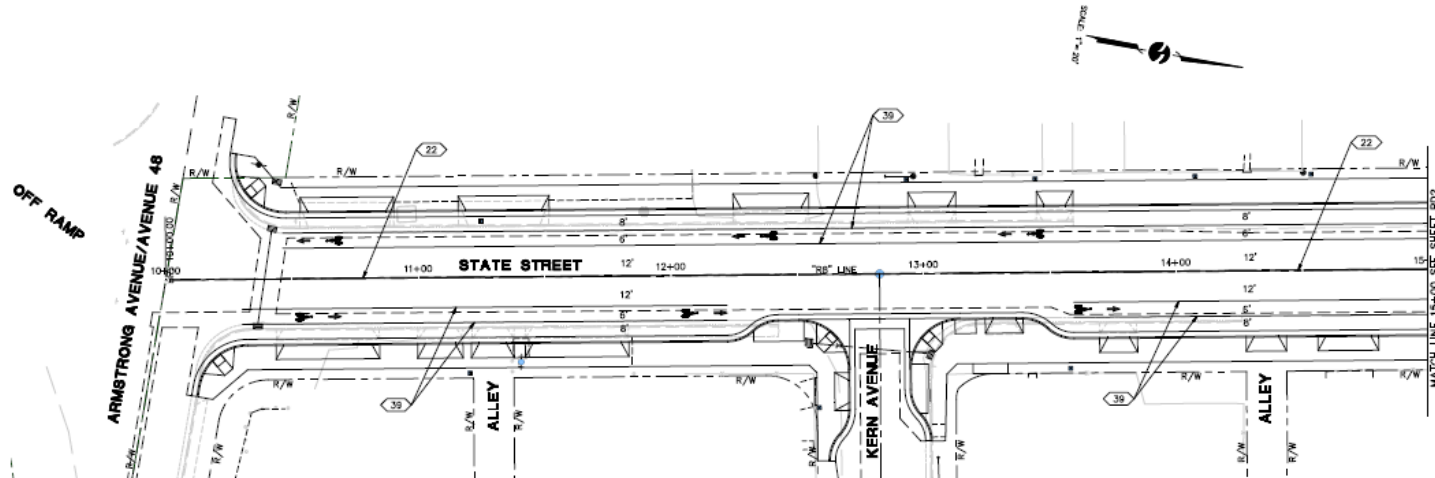
REVISIONS	
NO.	DESCRIPTION

omni • **inc**

ENGINEERS, PLANNERS, ARCHITECTS
1000 S. GATEWAY AVENUE, SUITE 100
SAN ANTONIO, TEXAS 78207
(214) 521-1000

LAYOUT COMPLETE STREETS IMPROVEMENTS EARLMART, CALIFORNIA

SCALE	1"=50'
DATE	08-12-2010
DESIGNED	JAB
DRAWN	JAB
CHECKED	BLM
DATE	10-15-10
SHEET NO.	L6
TOTAL SHEETS	12 OF 18



LEGEND (SHEETS PD1-PD6):

- TRAFFIC STRIPE DETAIL NUMBER
- LIMIT LINE
- EXISTING "STOP" PAVEMENT MARKING
- "STOP" PAVEMENT MARKING PER CALTRANS STD PLAN A24D
- "SLOW" PAVEMENT MARKING PER CALTRANS STD PLAN A24D
- "SCHOOL" PAVEMENT MARKING PER CALTRANS STD PLAN A24D
- "XING" PAVEMENT MARKING PER CALTRANS STD PLAN A24D
- CALIFORNIA SIGN CODE
- SHARED ROADWAY BICYCLE MARKING PER CALTRANS STD PLAN A24C
- BIKE LANE SYMBOL PER CALTRANS STD PLAN A24C.
- BIKE LANE ARROW PER CALTRANS STD PLAN A24A

GENERAL NOTES:

- (FOR SHEETS PD1 THROUGH PD2 ONLY)
1. WORK SHALL BE DONE IN ACCORDANCE WITH THE 2010 EDITION OF THE CALIFORNIA DEPARTMENT OF TRANSPORTATION STANDARD PLANS AND SPECIFICATIONS, THE 2014 EDITION OF THE CALIFORNIA MANUAL OF UNIFORM TRAFFIC CONTROL DEVICES (CA MUTCD) AND SIGN SPECIFICATION SHEETS, THE LATEST EDITIONS OF THE TULARE COUNTY STANDARD PLANS AND SPECIFICATIONS, AND THE SPECIAL PROVISIONS.
 2. THESE PLANS ARE ACCURATE FOR STRIPING ONLY.
 3. ALL CROSSWALKS SHALL BE 10 FEET WIDE, OUTSIDE EDGE TO OUTSIDE EDGE.

CONSTRUCTION NOTES:

1. INSTALL 8" WHITE DIAGONAL STRIPES ON 45 DEGREE SPACED 2' ON CENTER.
2. INSTALL 4" WHITE STRIPE.

90% SUBMITTAL
PRELIMINARY, NOT
FOR CONSTRUCTION



REVISIONS	
NO.	DESCRIPTION

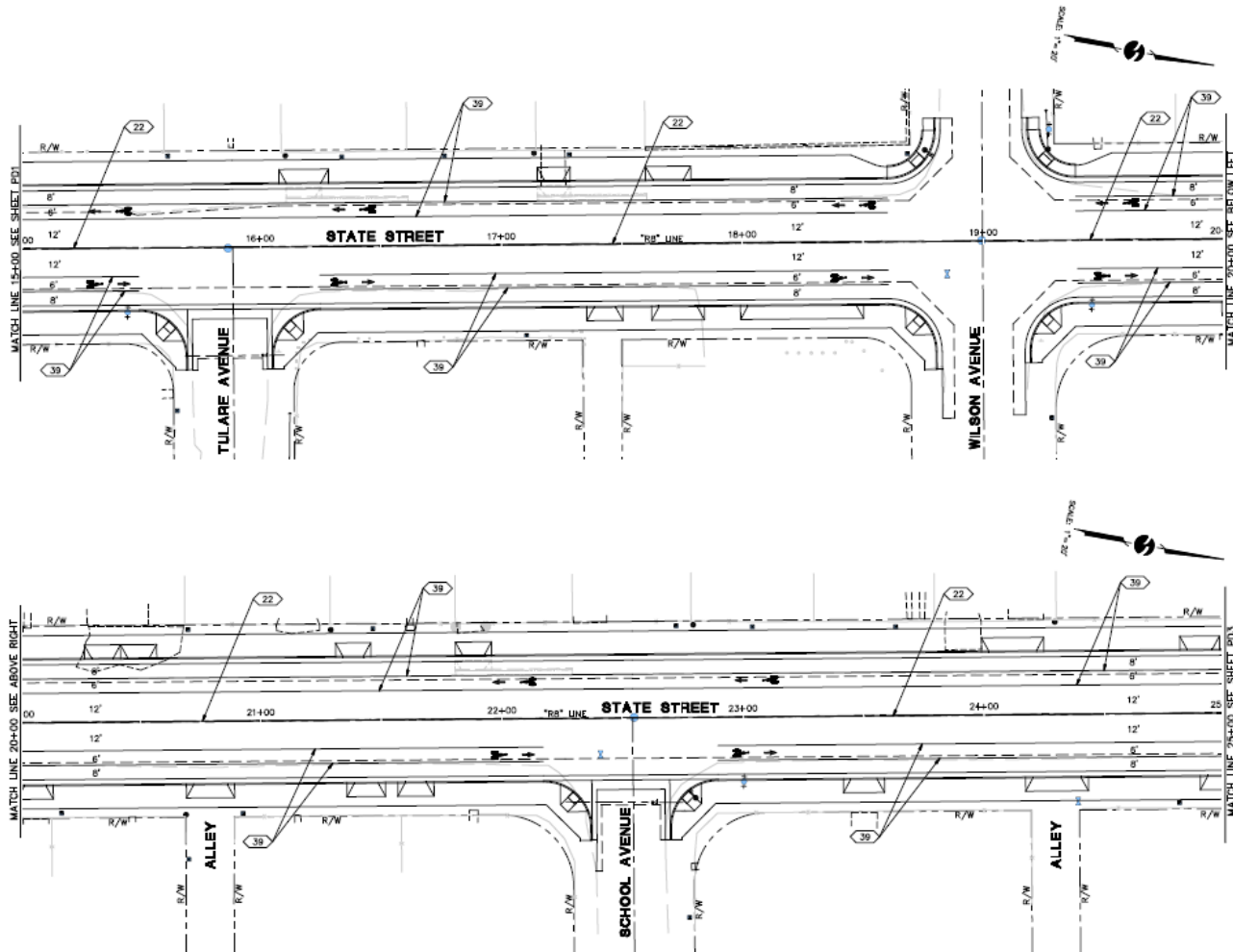
DESIGNED BY	
CHECKED BY	
DATE	



STRIPING PLAN COMPLETE STREETS IMPROVEMENTS EARLMART, CALIFORNIA

SCALE	1"=50'
JOB NO.	20-1000-10
DESIGNED	EJO
DRAWN	EJO
CHECKED	20100504/0401
DATE	04-04-10

SHEET NO.
PD1



807 SUBMITTAL
PRELIMINARY, NOT
FOR CONSTRUCTION



NO.	DATE	REVISIONS
1		
2		
3		
4		
5		
6		
7		
8		
9		
10		

<input type="checkbox"/> PRELIMINARY	<input type="checkbox"/> REVISED
<input type="checkbox"/> CORRECTED	<input type="checkbox"/> CONSTRUCTION
<input type="checkbox"/> OTHER	<input type="checkbox"/> OTHER



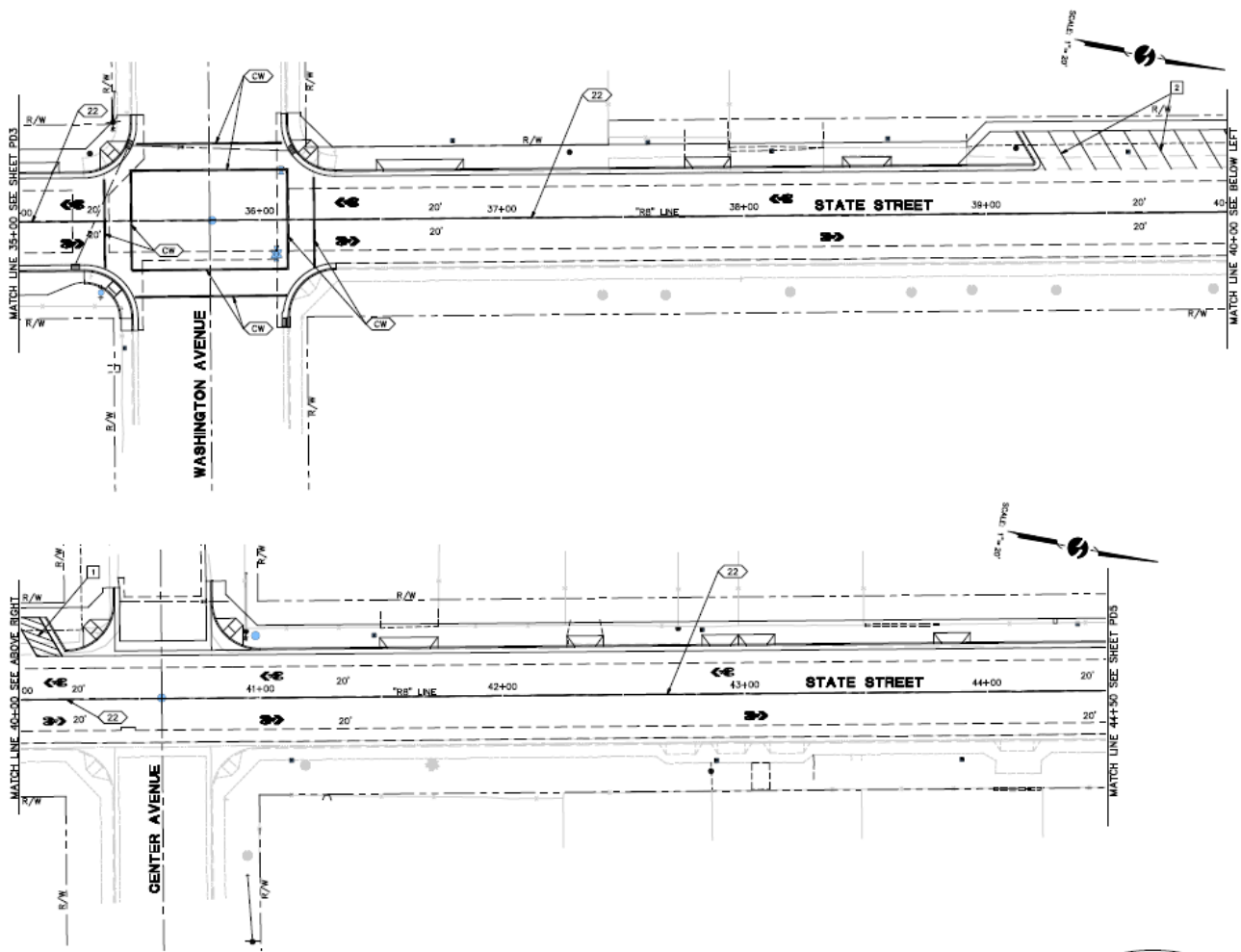
STRIPING PLAN COMPLETE STREETS IMPROVEMENTS EARMART, CALIFORNIA

SCALE	1"=50'
DATE	02-12-19
DESIGNED	E.J.D.
DRAWN	E.J.D.
CHECKED	E.J.D.
DATE	11-12-19

PD2
14 of 18



PD3
15 of 18



90% SUBMITTAL
PRELIMINARY, NOT
FOR CONSTRUCTION



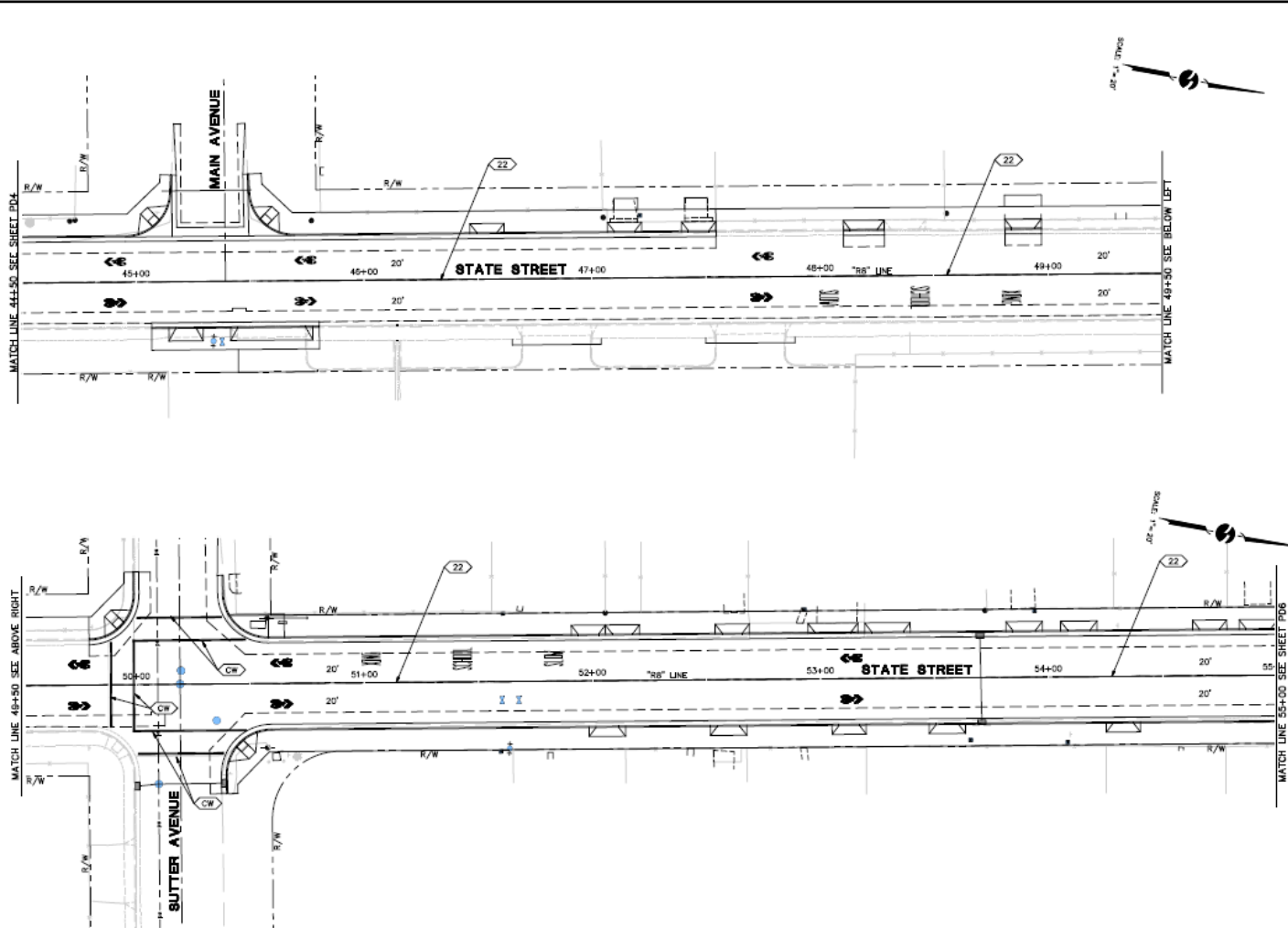
REVISIONS

NO.	DATE	DESCRIPTION

omni • inc.
ENGINEERS PLANNERS
ARCHITECTS
2025 N. GARDEN AVE.
SUITE 200
ANAHEIM, CA 92805
(714) 771-0000

STRIPING PLAN
COMPLETE STREETS IMPROVEMENTS
EARMART, CALIFORNIA

DESIGN: J. WITTON
DATE: 11-25-23
SHEET NO. **PD4**
16 OF 18



90% SUBMITTAL
PRELIMINARY, NOT
FOR CONSTRUCTION



NO.	DATE	REVISIONS

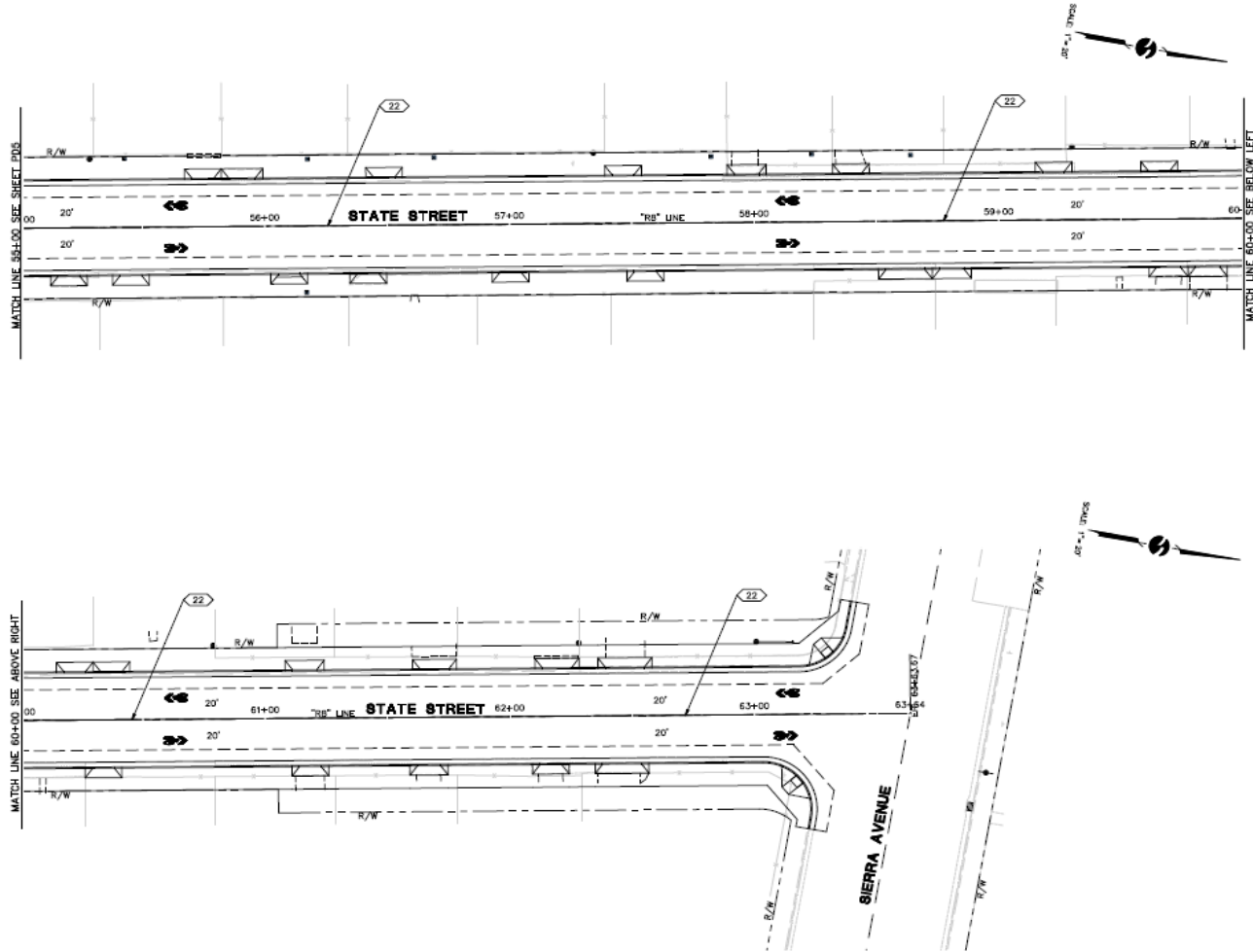
DESIGNED	BY
CHECKED	BY
APPROVED	BY
DATE	

omni • inc.
ENGINEERS PLANNERS
ARCHITECTS
1000 S. MAIN AVE.
SUITE 200
SAN JOSE, CA 95128
(408) 281-1000

STRIPING PLAN
COMPLETE STREETS IMPROVEMENTS
EARLMART, CALIFORNIA

SCALE	1"=50'
DATE	04-12-2010
DESIGNED	J.W.
DRAWN	J.W.
CHECKED	J.W.
DATE	04-12-2010

SHEET NO.
PD5
17 of 18



90% SUBMITTAL
PRELIMINARY, NOT
FOR CONSTRUCTION



REVISIONS	
NO.	DESCRIPTION
1	ISSUED

DESIGNED	BY
CHECKED	BY
APPROVED	BY

omni • meqns
 CONSULTING ENGINEERS
 1000 S. GATEWAY AVENUE
 SUITE 200
 SAN ANTONIO, TEXAS 78207
 (214) 591-1000

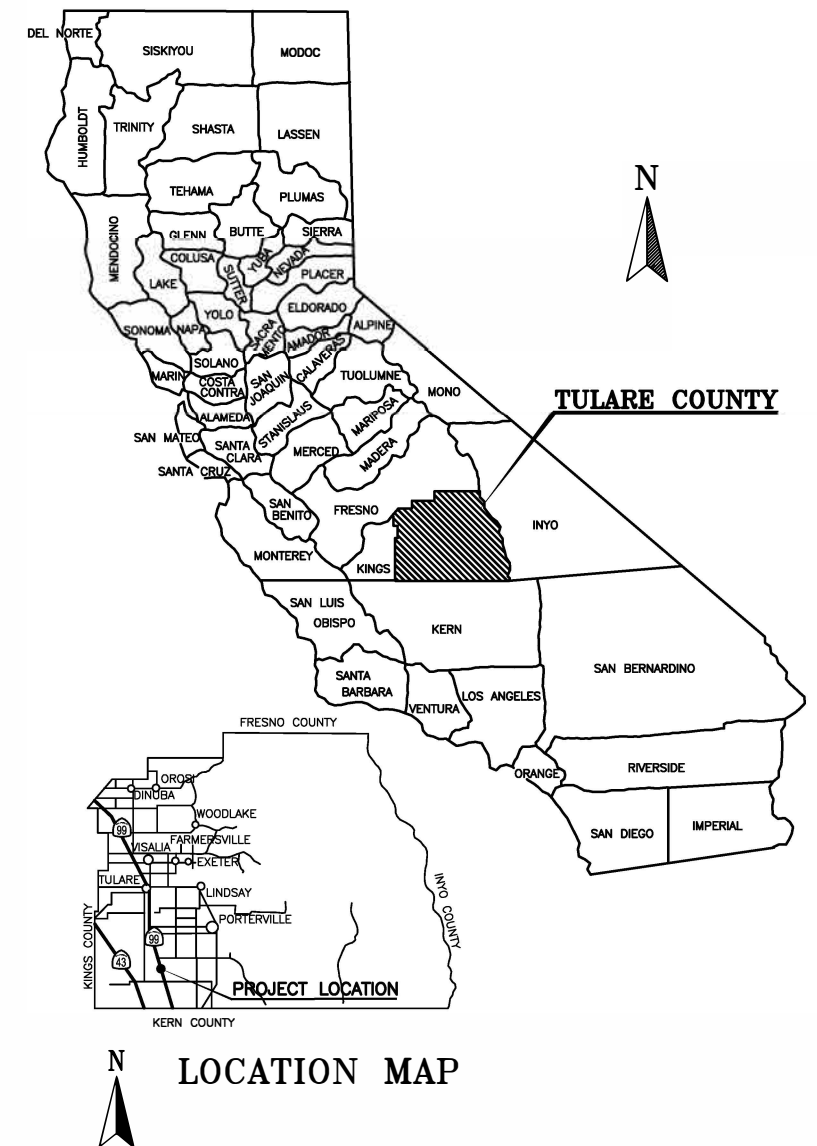
STRIPING PLAN
COMPLETE STREETS IMPROVEMENTS
EARLMART, CALIFORNIA

SCALE	1"=50'
DATE	10-12-19
DESIGNED	J.M.
CHECKED	J.M.
APPROVED	J.M.

PD6
 18 OF 18

STATE OF CALIFORNIA
COUNTY OF TULARE

PROJECT PLANS FOR CONSTRUCTION OF
COMPLETE STREETS PHASE III
EARLIMART
IN COUNTY OF TULARE
WASHINGTON AVENUE – ALILA SCHOOL TO ELM STREET

[illegible]

RESOURCE MANAGEMENT AGENCY
5961 SOUTH MOONEY BLVD.
VISALIA, CA 93277
(559) 624-7000
WWW.TULARECOUNTY.CA.GOV/RMA



TITLE SHEET
COMPLETE STREETS PHASE III
EARLIMART
TULARE COUNTY

SCALE	N.T.S.
VISION	DESIGN
DRAWING NO.	16013-1
DESIGNED BY	PAO
DRAWN BY	JDF
CHECKED BY	PAO
FILE NO.	16013-1T001.DWG
DATE	11-15-2016
SHEET No.	

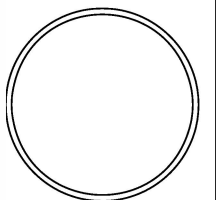
T1
1 of 5

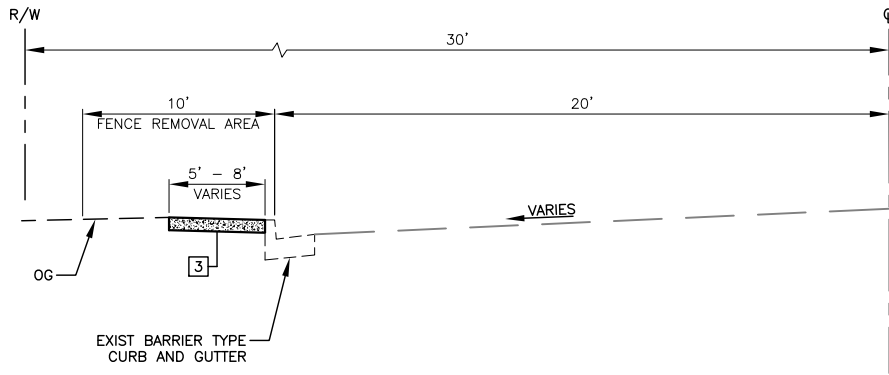


Know what's **below**.
Call before you dig.

Contractor shall call
Underground Service Alert at
811 two working days prior
to excavation

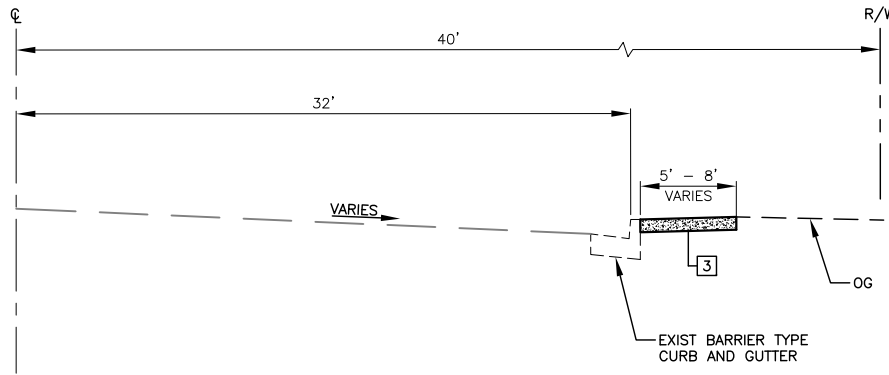
**30% SUBMITTAL
PRELIMINARY, NOT
FOR CONSTRUCTION**





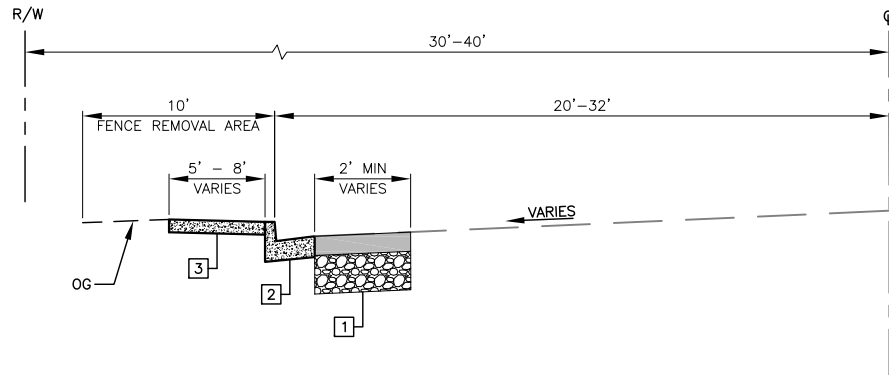
WASHINGTON AVE

STA 23+36.35 TO STA 23+71.01
STA 24+00.01 TO STA 24+34.86
STA 24+87.56 TO STA 25+62.50
STA 26+14.75 TO STA 26+89.72
STA 27+41.71 TO STA 28+16.89
STA 28+69.33 TO STA 29+04.08
STA 29+80.08 TO STA 30+70.06



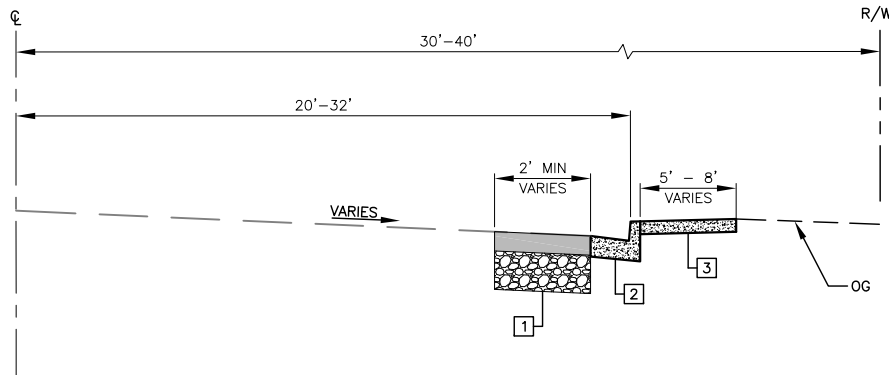
WASHINGTON AVE

STA 45+15.80 TO STA 45+49.57
STA 46+69.99 TO STA 47+26.92
STA 47+41.92 TO STA 47+76.15
STA 47+91.15 TO STA 48+35.97



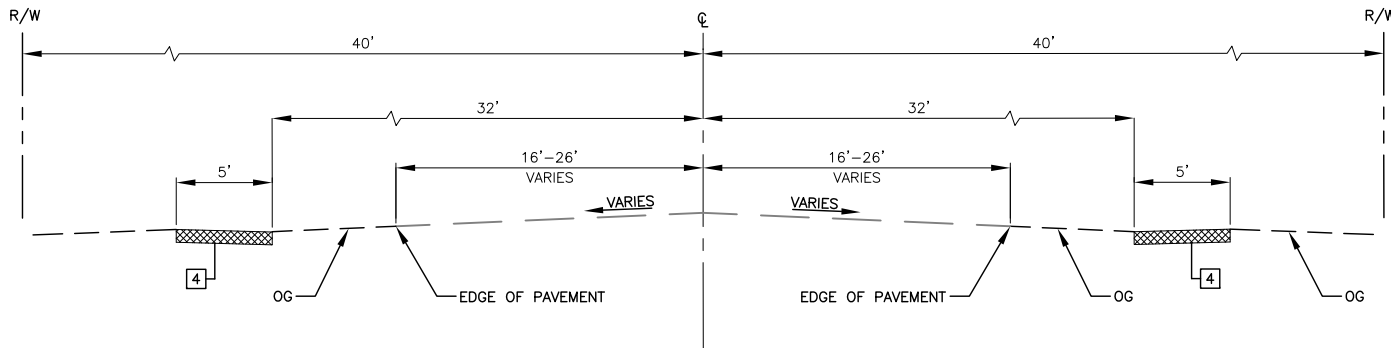
WASHINGTON AVE

STA 17+60.25 TO STA 22+77.87
STA 23+71.01 TO STA 24+00.01
STA 24+34.86 TO STA 24+87.56
STA 25+62.50 TO STA 26+14.75
STA 26+89.72 TO STA 27+41.71
STA 28+16.89 TO STA 28+69.33
STA 30+70.06 TO STA 31+65.12
STA 37+23.49 TO STA 40+25.77
STA 59+69.15 TO STA 59+88.15



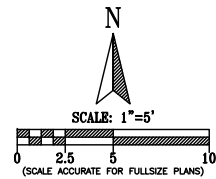
WASHINGTON AVE

STA 17+62.42 TO STA 17+81.42
STA 22+97.44 TO STA 25+56.50
STA 25+70.22 TO STA 26+04.22
STA 27+01.78 TO STA 27+33.78
STA 37+23.47 TO STA 40+25.73
STA 43+17.30 TO STA 43+90.35
STA 45+49.57 TO STA 46+69.99
STA 47+26.92 TO STA 47+41.92
STA 47+76.15 TO STA 47+91.15
STA 49+15.97 TO STA 52+99.11



WASHINGTON AVE

STA 32+24.32 TO STA 34+32.70
STA 34+37.68 TO STA 36+55.59



NOTES:

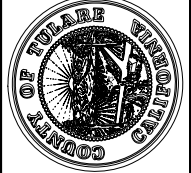
- FOR EXACT LOCATIONS OF CURB AND GUTTER, CURB, AND SIDEWALK, SEE PLAN SHEETS.
- ALL MAILBOXES, FENCES, TREES, SIGNS, STRUCTURES, OR OBSTRUCTIONS IN THE PROPOSED SIDEWALK AREA, EITHER EXISTING OR PROPOSED MUST CONFORM TO CURRENT ADA REQUIREMENTS FOR CLEARANCE AND OTHER STATUTES FOR WHEELCHAIR ACCESSIBILITY.
- ALL EXISTING STRUCTURES, OBJECTS, OR FENCES IN THE CONSTRUCTION AREA OR R/W SHALL BE RELOCATED OR REMOVED UPON CONSULTING THE RESIDENT ENGINEER (RE) OR PROPERTY OWNER. POINT OF REMOVAL OF EXISTING CONCRETE STRUCTURES SHALL BE A CLEAN CUT OR SEPARATION BY SAW-CUT AND DETERMINED BY THE RE.
- KNOWN UNDERGROUND UTILITIES ARE SHOWN AS APPROXIMATE IN LOCATIONS. IT IS THE CONTRACTOR'S RESPONSIBILITY TO LOCATE AND VERIFY ALL UNDERGROUND UTILITIES IN THE CONSTRUCTION AREA BY EITHER CONTACTING USA OR THE UTILITY OWNER.
- BACK OF WALK GRADING SHALL MATCH EXISTING GROUND AT 10% MAXIMUM SLOPE WITH NATIVE MATERIAL, DG, ASPHALT, CONCRETE OR PRE-EXISTING MATERIAL SUCH THAT A SMOOTH TRANSITION IS PROVIDED. PAYMENT FOR BACK OF WALK GRADING IS INCLUDED IN THE PAYMENT FOR ROADWAY EXCAVATION.
- PAVING OR PAVE-OUT FROM PROPOSED TO EXISTING ROAD SHALL NOT EXCEED 15% FOR TRANSITION SLOPE OR PER ACCEPTANCE BY THE RE. ALL PAVING TO MATCH EXISTING SHALL HAVE A CLEAN CUT EDGE OR SAW-CUT AND SPRAYED WITH ASPHALT BINDER.

TYPICAL STRUCTURAL SECTIONS

- 20-YEAR DESIGN
0.5' AC (TYPE B)
1' AB (CLASS II)
- BARRIER TYPE CURB AND GUTTER,
- CONCRETE SIDEWALK, 4" THICK TYPICAL
- 0.2' AC PATHWAY

REVISIONS		DATE	BY
No.	DESCRIPTION		

COUNTY OF TULARE
RESOURCE MANAGEMENT AGENCY
5961 SOUTH MOONEY BLVD.
VISALIA, CA 93277
(559)624-7000
WWW.TULARECOUNTY.CA.GOV/RMA



TYPICAL CROSS SECTIONS
COMPLETE STREETS PHASE III
EARLMART
TULARE COUNTY

SCALE	1"= 5'
DIVISION	DESIGN
JOB NO.	16013-1
DESIGNED	PAO
DRAWN	JDF
CHECKED	PAO
FILE	16013-1X001.DWG
DATE	11-15-2016
SHEET No.	

X1

2 OF 5

**30% SUBMITTAL
PRELIMINARY, NOT
FOR CONSTRUCTION**

CONSTRUCTION NOTES (SHEETS L1 & L2):

- 1 MINOR CONCRETE (CURB & GUTTER)
- 2 MINOR CONCRETE (SIDEWALK) WIDTH PER PLAN
- 3 MINOR CONCRETE (RAMP) CASE A
- 4 MINOR CONCRETE (RAMP) CASE C
- 5 MINOR CONCRETE (RAMP) CASE CM
- 6 MINOR CONCRETE (CONTINUOUS GUTTER CURB RETURN)
- 7 MINOR CONCRETE (DRIVEWAY)
- 8 MINOR CONCRETE STRUCTURE (MANHOLE)
- 9 MINOR CONCRETE STRUCTURE (DROP INLET)
- 10 STORM DRAIN PIPE

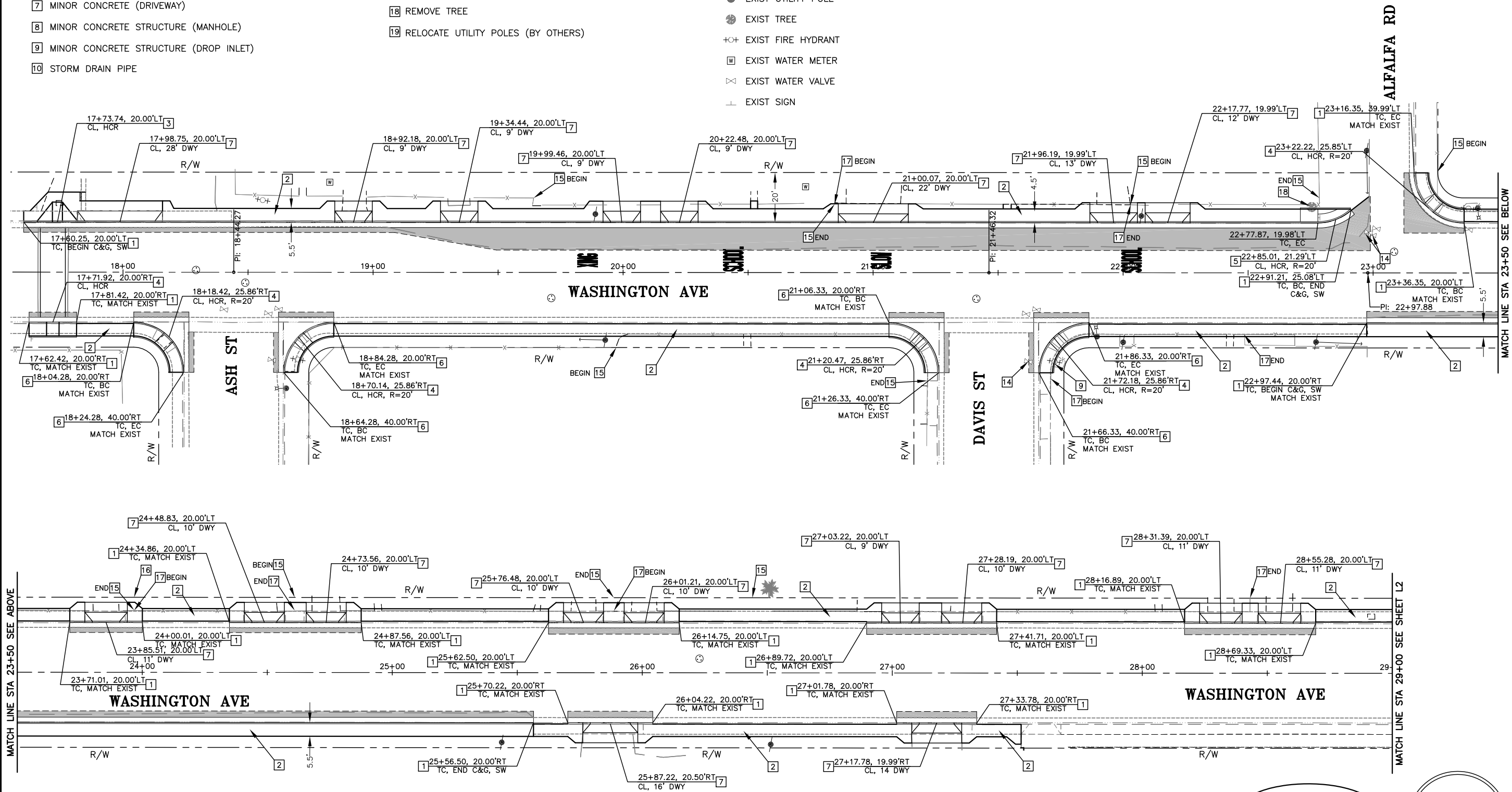
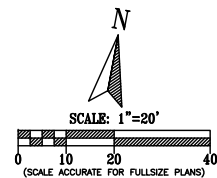
- 11 MISCELLANEOUS AC RAMP
- 12 AC WALKWAY
- 13 PED RAILROAD CROSSING
- 14 ADJUST UTILITY TO GRADE
- 15 RELOCATE CHAIN LINK FENCE
- 16 RELOCATE WOOD FENCE
- 17 RELOCATE WROUGHT IRON FENCE
- 18 REMOVE TREE
- 19 RELOCATE UTILITY POLES (BY OTHERS)

LEGEND

- PROPOSED STRUCTURAL SECTION
- EXIST FENCE
- EXIST MANHOLE
- PROP MANHOLE
- EXIST DI
- PROP DI
- EXIST UTILITY POLE
- EXIST TREE
- EXIST FIRE HYDRANT
- EXIST WATER METER
- EXIST WATER VALVE
- EXIST SIGN
- EXIST MAILBOX
- MONUMENT

GENERAL NOTES:

- GRIND AREAS SHOWN ARE APPROXIMATE, EXACT GRIND & REPLACE AREAS SHALL BE MARKED BY ENGINEER.
- SAWCUT BOUNDARIES ARE APPROXIMATED FOR THE CONSTRUCTION LIMITS AND SHALL BE DEFINED IN THE FIELD BY THE ENGINEER.



30% SUBMITTAL
PRELIMINARY, NOT
FOR CONSTRUCTION

REVISIONS	
DATE	BY

COUNTY OF TULARE
RESOURCE MANAGEMENT AGENCY

5961 SOUTH MOONEY BLVD.
VISALIA, CA 93277
(559)624-7000
WWW.TULARECOUNTY.CA.GOV/RMA

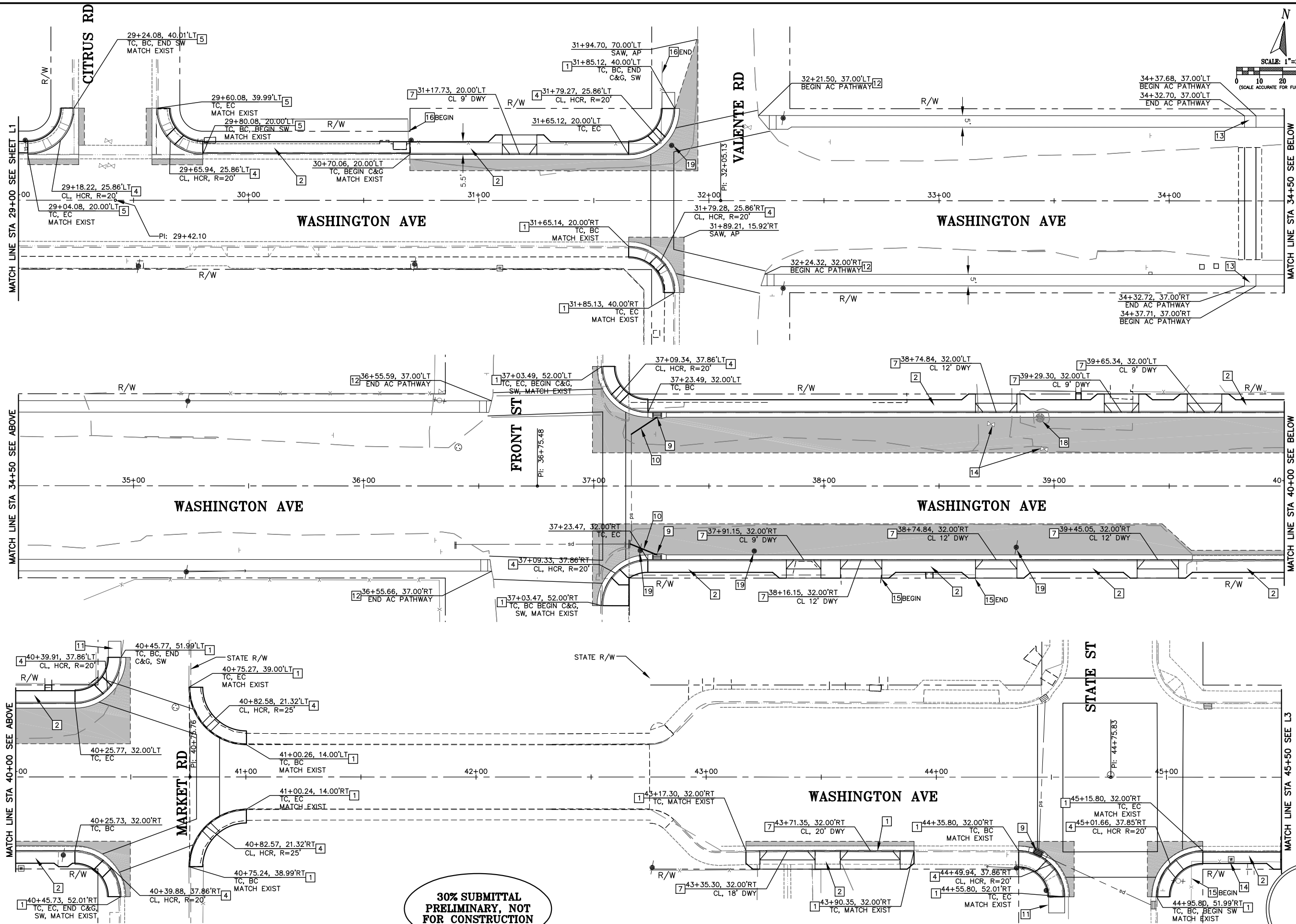
LAYOUT SHEET

COMPLETE STREETS PHASE III

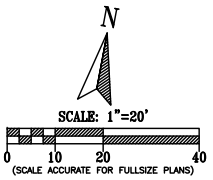
EARLMART

TULARE COUNTY

SCALE	1"=20'
DIVISION	DESIGN
JOB NO.	16013-1
DESIGNED	PAO
DRAWN	JOF
CHECKED	PAO
FILE	16013-11001.DWG
DATE	11-15-2016
SHEET No.	



30% SUBMITTAL
PRELIMINARY, NOT
FOR CONSTRUCTION



REVISIONS	
DATE	BY

COUNTY OF TULARE
RESOURCE MANAGEMENT AGENCY
5961 SOUTH MOONEY BLVD.
VISALIA, CA 93277
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WWW.TULARECOUNTY.CA.GOV/RMA

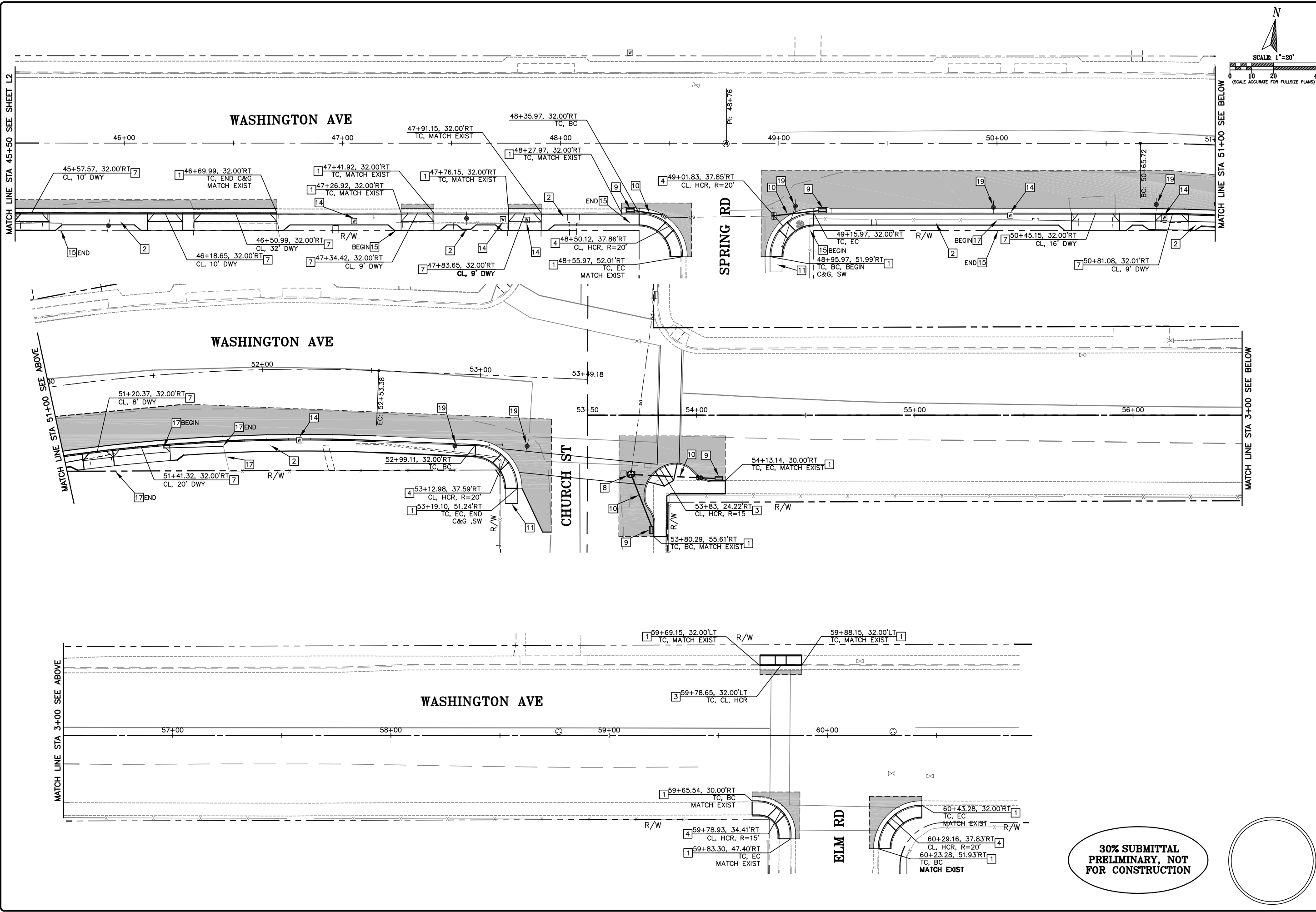


LAYOUT SHEET
COMPLETE STREETS PHASE III
EARLMART
TULARE COUNTY

SCALE	1"=20'
DIVISION	DESIGN
JOB NO.	16013-1
DESIGNED	PAO
DRAWN	JOF
CHECKED	PAO
FILE	16013-11001.DWG
DATE	11-15-2016
SHEET No.	

L2
4 OF 5

\\PROJECTS\16013-1\16013-11001.DWG 12/8/2016 11:42 AM



DATE

BY

REVISIONS

DESCRIPTION

COUNTY OF TULARE

RESOURCE MANAGEMENT AGENCY

5961 SOUTH MOONEY BLVD.

VISALIA, CA 93277

(559)624-7000

WWW.TULARECOUNTY.CA.GOV/RMA

LAYOUT SHEET

COMPLETE STREETS PHASE III

EARLMART

TULARE COUNTY

SCALE 1"=20'

DIVISION DESIGN

JOB NO. 16013-1

DESIGNED PAO

DRAWN JDF

CHECKED PAO

FILE 16013-11001.DWG

DATE 11-15-2016

SHEET No.

30% SUBMITTAL

PRELIMINARY, NOT

FOR CONSTRUCTION

L3

5 OF 5

Appendix G – Complete Streets Outreach



Board Members: Chair – Gloria Borunda
Treasurer – Mike Fernandez
Vice Chair – Andres Valdez
Member – Vicky Mendoza
Secretary – Blanca Esparza

AGENDA
Thursday February 5, 2015
Earlimart Memorial Building
6:00 PM

A. Call to Order

1. Treasurer's Report: Mike Fernandez
2. Council Business: Project updates/ New Projects
3. Donations

B. Guest Speakers:(15 Minutes per Speaker)

1. Supervisor District 2 – Pete Vander Poel
2. Earlimart School District – Supt. Sandra Rivera
3. Fire Dept.
4. Sheriff Dept. Update – CBO Franco
5. Rural Foundation – Jose Villalobos/Domingo Trevino
6. CHP Update –
7. CSET
8. Earlimart Family Resource Center –
9. United Health Centers
10. Resource Management Agency for Tulare County— Kyra Fierros

C. Community Concerns: Open to the Public

D. Adjournment:

Next Meeting Date & Time - March 5, 2015 6:00 PM



**Earlimart Community Plan Update Workshop
Tulare County Resource Management Agency**

Thursday, March 5, 2015

Earlimart Memorial Building

6:00p.m.

1. Introduction-Who we are (Eric Coyne and Dave Bryant)
2. Earlimart Park Update
3. Housing Element Update/Survey
4. Complete Streets Policy
5. ATP Cycle II (Eric Coyne)
6. Public Input- Questions/ Comments
7. Conclusion
8. Thank you for attending!

Notes from Earlimart Town Council in conjunction with the Tulare County Resource Management Agency

Community Plan/Complete Streets Meeting/ATP Program

Earlimart, CA

Thursday, March 5, 2015

Earlimart Memorial Building

712 E. Washington

Earlimart, CA 93219

County Planning Division Dave Bryant and Eric Coyne attended the Earlimart Town Council meeting in Earlimart, CA on March 5, 2015. County staff presented a 30 minute presentation. The County is currently updating Earlimart's Community Plan for adoption. At this meeting County staff informed the Community of the status of the community plan and received input for the Complete Streets Policy, needed to be integrated in the Community Plan, per the California Complete Streets Act of 2008. Staff asked the community for input regarding their top picks for roadway improvement.

Eric Coyne, County Staff, introduced the Active Transportation Program to the town council. In the discussion the community was informed the County was seeking funds for the Community of Earlimart. Additionally, we asked for community input.



The Earlimart Town Council affirmed their interest in recommending that State Street should be included as their top priority Complete Streets project. The limits of the project would include Avenue 56 to Avenue 48. This recommendation was also supported at the previous meeting of the Town Council meeting in February 2015. This project is also supported by the local school district, County Fire and County Sheriff. Based on these interests, State Street has been added to the Phase 2 round of complete street projects RFP going to the Board on March 24, 2015. The Town Council expressed interest in their top 5 complete streets candidate projects as follows: 1) State Street, 2) Washington Street, 3) Church Street, 4) School Street, and 5) Lane Avenue. The Town Council indicated that they would be supportive of any of the top 3 streets as complete streets, however State Street is their top priority project. In addition the Town Council indicated they had they would like to assist the county in developing recommendations for future Road improvement projects.

A Housing conditions survey was distributed.

The Town Council expressed concerns relating historically to studies being prepared as opposed to actual construction work and that this has led to a lack of participation on the part of the community at Town Council meetings. The Town Council was encouraged by the new Park/Health Center project and economic development projects that are occurring in the community. They were also encouraged that it appeared that funding from the state for road and pedestrian related projects is increasing in addition to Measure R Projects. Based on these developments, they indicated that there may be an opportunity to increase public participation at future Town Council meetings related to the Community Plan Update. They requested that a translator be present at these future meetings. It was shared with the community that by working with the school district, fliers or notices could go out with the students to advertise Community Plan Update meetings. Staff mentioned that a similar approach in Cutler and Orosi, led to 78 members of the public attending their last Community Plan update meeting. Rule 20 A under grounding was also discussed. The community expressed an interest in developing a recreation center in the location of the sand pits west of Highway 99.

County staff in attendance: Dave Bryant and Eric Coyne.

The meeting was held in English.

The meeting started at 6:00p.m. and ended at 7:45 p.m.



EARLIMART COMMUNITY PLAN UPDATE

APPENDICES

Appendix A: Planning Commission Resolutions

Appendix B: Board of Supervisors Resolution

BEFORE THE PLANNING COMMISSION

COUNTY OF TULARE, STATE OF CALIFORNIA

IN THE MATTER OF THE FINAL ENVIRONMENTAL)
IMPACT REPORT, FINDINGS OF FACT, STATEMENT)
OF OVERRIDING CONSIDERATIONS AND)
MITIGATION MONITORING AND REPORTING) RESOLUTION NO. 9347
PROGRAM FOR THE EARLIMART COMMUNITY)
PLAN 2017 UPDATE (GPA17-005) INCLUSIVE OF)
THE EDEN PRODUCE COMPANY PROJECT (GPA 14-)
005), DELANO JOINT UNION HIGH SCHOOL)
DISTRICT PROJECT (GPA 16-003), MIXED-USE)
COMBINING ZONE (PZC17-033), BY-RIGHT ZONING)
(PZC 17-034), AND THE EARLIMART ZONING)
DISTRICT ORDINANCE MAP (PZC17-035).)

Resolution of the Tulare County Planning Commission recommending that the Board of Supervisors certify the Final Program Environmental Impact Report ("EIR") (SCH # 2015081005), under California Environmental Quality Act (CEQA) Sections 15080 through 15097 of the state CEQA Guidelines generally, for General Plan Amendment No. GPA17-005 which is inclusive of the Earlimart Community Plan 2017 Update, Eden Produce Company Project (GPA 14-005), Delano Joint Union High School Project (GPA 16-003), amendments to Section 18.9 (PZC17-033), Section 16 (PZC 17-034) and the Zoning District Ordinance Map (PZC 17-034) of Ordinance No. 352, the Zoning Ordinance, for the Community of Earlimart. The General Plan Amendment is required to (i) update the Earlimart Community Plan, (ii) approve a Zoning Ordinance amendment to add Earlimart to the Mixed-Use Combining zoning district, (iii) and the rezoning of properties within the Earlimart Urban Development Boundary and removal of the Site Review and Planned Development and (iv) approve an amendment to Section 16 of the Zoning Code to allow additional by-right uses, located in the Community of Earlimart The unincorporated community of Earlimart is located within Sections 33 & 34, Township 23 South, Range 25 East and Sections 3 & 4, Township 24 South, Range 25 East MDB&M.

WHEREAS, on January 28, 2014 the Tulare County Board of Supervisors approved the Tulare County Resource Management Agency (RMA) Economic Development and Planning Branch proposal to update the Earlimart Community Plan;

WHEREAS, this resolution of the Tulare County Planning Commission recommends that the Board of Supervisors certify the EIR, the Mitigation Monitoring and Reporting Program ("MMRP"), and CEQA Findings of Fact ("FOF") for the Earlimart Community Plan Update (GPA 17-005) Eden Produce Company (GPA14-005), Delano Joint Union High School District (GPA 16-003), Mixed Use Combining Zone (PZC 17-033), By-Right Uses (PZC 17-034), Earlimart Zoning District Ordinance Map (PZC 17-035), and, as being in compliance with the California Environmental Quality Act ("CEQA") and the State CEQA Guidelines;

WHEREAS, the Project Final Environmental Impact Report (Exhibit "A") is attached and incorporated by reference herein;

WHEREAS, the Environmental Assessment Officer has reviewed and authorized the processing of the EIR for the Earlimart Community Plan 2017 Update (GPA 17-005), Eden Produce Company (GPA14-005), Delano Joint High School District (GPA 16-003), Mixed Use Combining Zone (PZC 17-033), Additional By-Right Uses (PZC17-034) and Earlimart Zoning District (PZC 17-035);

WHEREAS, a Notice of Preparation (NOP) was prepared for the Project and circulated on August 4, 2015, to responsible agencies, interested groups and individuals for a 30-day review period ending September 2, 2015;

WHEREAS, a public scoping meeting was held on August 13, 2015, at the Tulare County Resource Management Agency Conference Room "L", and there was no attendance from any agencies or the public in general;

WHEREAS, an Environmental Impact Report (EIR) for General Plan Amendment No. GPA 17-005, Mixed Use Overlay PZC 17-033, Community Plan Rezoning No. PZC 17-035, and Additional By-Right Uses PZC 15-034, was prepared by Tulare County Staff and approved for public review by the Tulare County Environmental Assessment Officer; and submitted for 45-day public review, comment, and response on July 21, 2017;

WHEREAS, the Draft EIR was released on July 21, 2017, which included an evaluation of existing resources and potential Project impacts, and the proposed mitigation measures to reduce any impacts to a less than significant level; and

WHEREAS, on July 21, 2017, under CEQA Guidelines Section 15087, the Notice of Availability for this EIR was sent for publishing to the Visalia Times-Delta (a newspaper of general circulation in Tulare County), posting of the Notice of Completion for this EIR at the State Clearinghouse with the requisite copies of the Draft EIR to be mailed to affected public agencies, and notification to interested parties, for public review, with a review period that started on July 21, 2017 and closed on September 5, 2017;

WHEREAS, this EIR and MMRP are for the Earlimart Community Plan Update 17-005, inclusive of, the Earlimart Community Plan 2017 Update, Eden Produce Company General Plan Amendment No. GPA 14-005, Mixed Use Overlay District PZC 17-033, "By Right Zoning" PZC 17-034, Earlimart Zoning District PZC 17-035, and Delano Joint Union High School District GPA 16-003.

WHEREAS, the Earlimart Community Plan 2017 Update will become consistent with the General Plan 2030 Update, and will include the following primary goals and objectives;

- (1) Land Use and Environmental Planning – Promote development within planning areas next to the Regional Highway 99 Corridor in order to implement the following General Plan goals:
 - a. Update the affected Urban Development Boundary to include the Northern Earlimart Rezone areas;
 - b. Ensure that the text and mapping of the Community Plan Designations and Zoning Reclassifications address various development matters such as encouraging Agricultural Adaptive Reuse activities, recognizing Non-Conforming Use activities, and facilitating Ministerial Permit approvals;
 - c. Encourage infill development within Urban Development Boundaries by providing opportunities for developers to respond to market demands specific to rural communities such as Earlimart;
 - d. Reduce development pressure on agriculturally-designated lands within the Valley Floor, thereby encouraging agricultural production to flourish;
 - e. Reduce vehicle miles travelled throughout the County, thereby benefitting air quality and reducing greenhouse gas emissions; and
 - f. Help to improve the circulation, transit and railroad transportation system within this community, including, but not limited to, laying the groundwork for the construction of key projects such as Safe Routes to Schools, Complete Streets, and Bike Lanes/Pedestrian Paths.
- (2) Improvements for a “disadvantaged community” – It is expected that the community planning areas will be improved for the following reasons:
 - a. With quicker project processing resulting from an updated Community Plan, increased employment opportunities would be provided by the private sector as proposed project developments can be approved as quickly, affordably, efficiently, and effectively as possible;
 - b. Increased housing grant awards are more likely to occur based on an updated Community Plans that is consistent with the policies of the General Plan 2030 Update (August 2012) and Housing Element 2015 Update (November 2015); and
 - c. With an updated Community Plan, enhanced infrastructure grant awards are more likely, thereby providing access to funding to install or upgrade road, water supply and distribution, wastewater treatment, and storm water drainage facilities.
- (3) Strengthening Relationship with Tulare County Association of Governments (TCAG) – An important benefit of this deliberate Community Plan process will be the opportunity for Tulare County Resource Management Agency (RMA) to strengthen the County’s relationship with the TCAG as adoption of this and other community plans will help to facilitate the funding and implementation of several key transportation programs such as Safe Routes to Schools, Complete Streets, and Bike/Pedestrian Projects. By pursuing these transportation programs through a heightened collaborative process, the likelihood of getting actual projects on the ground will be realized faster than historically achieved. In doing so, these communities and others can become safer and healthier by providing a more efficient transportation network.

WHEREAS, a Draft EIR for the Project (SCH # 2015081005) has been prepared pursuant to CEQA Statute and Guidelines, and is incorporated into the FEIR with i) written Responses to Comments made during the CEQA review period, ii) the MMRP, iii) changes and revisions thereto, iv) Findings of Fact; and v) Statement of Overriding Consideration:

WHEREAS, the EIR was duly circulated for a 45-day public review period commencing on July 21, 2017, and ending September 5, 2017;

WHEREAS, in accordance with state and local ordinance, RMA Staff has given due notice of the public hearing for the EIR that would be held for the purpose of receiving comment on the environmental documents for GPA 17-033, GPA 16-003, GPA 14-005, PZC 17-033, PZC 17-034, and PZ 17-035;

WHEREAS, on September 15, 2017, a Notice of Public Hearing and Completion of Final Environmental Report was duly sent to public agencies, pertinent departments, and interested parties and published in the Visalia Times-Delta for a Planning Commission meeting set for September 27, 2017;

WHEREAS, written comments were submitted by agencies during the comment period, and after due consideration thereof, written responses were prepared for said comments by Staff;

WHEREAS, comments were received by the California Department of Fish and Wildlife, the California Department of Transportation, and the San Joaquin Valley Unified Air Pollution Control District, and whereas a copy of the written responses dated September 15, 2017, was duly sent to the commenting public agencies, at least 10 days before the action taken; and

WHEREAS, a public hearing was conducted on September 27, 2017 at the regular Planning Commission Meeting in the Board of Supervisors Chambers, where there was opportunity for public testimony to be received on the project;

WHEREAS, on September 27, 2017, staff presented evidence to the Commission;

WHEREAS, this resolution recommended approval and certification of the EIR and MMRP for the Earlimart Community Update at the meeting on September 27, 2017;

WHEREAS, Staff responded to comments provided prior to, during, and after the 45-day review period commencing on July 21, 2017, and ending on September 5, 2017; and

WHEREAS, at that meeting of the Planning Commission public testimony was received by Abigail Solis, President of the Earlimart School Board, and by Greg Davis, owner of the Eden Produce Company, in favor of the Project, and recorded.

WHEREAS, Staff responded to and clarified all issues, and assured the Commission that all Mitigation Measures, as presented to the Commission, are adequate to address all impacts that the Project will have on the environment; and

WHEREAS, the Commission finds that the Staff of Tulare County RMA has conducted the necessary investigations (including environmental review of this matter); and provided additional information affecting this EIR and MMRP at the public hearings noticed, convened and conducted for this matter; and based on substantial evidence, submitted this EIR for the consideration of, and action by, the Commission;

WHEREAS, through a good faith and substantial analysis of environmental impacts from this Project in the EIR, staff found, and the Commission agrees, that all impacts are less than significant with the exception of Agricultural, Biological, Cultural, and Hydrology and Water Quality resources, which will be less than significant with mitigation and Air Quality, which could be significant and unavoidable after implementation of all feasible mitigation measure; and therefore, the EIR and MMRP were the appropriate level of environmental review under CEQA for this Project;

WHEREAS, this resolution hereby certifies and incorporates the following documents by reference including the Mitigation Monitoring and Reporting Program (MMRP) (Exhibit "B"), Findings of Fact (FOF) (Exhibit "C"), Statement of Overriding Consideration (SOC) (Exhibit "D") and Response to Comments (Exhibit "E"), pursuant to the FEIR that has been prepared for the Earlimart Community Plan 2017 Update (GPA 17-005), including General Plan Amendment No. GPA 14-005 for the Eden Produce Company Project, GPA 16-003 for the Delano Joint High School District Project, Zone Ordinance Amendment No. PZC 17-033 for the Mixed Use Combining Zone, Zone Ordinance Amendment No. PZC 17-034 for Additional By-Right Uses and Zoning Ordinance Amendment No. PZC 17-035 for the Earlimart Zoning District Ordinance Map.

NOW, THEREFORE, BE IT RESOLVED as follows:

1. This Planning Commission hereby recommends certification, based on their findings and on substantial evidence in the record, that the analysis presented in the Environmental Impact Report (EIR) and Mitigation Monitoring and Reporting Program (MMRP) for the Earlimart Community Plan Updated (GPA 17-005) inclusive of GPA 14-005 and GPA 16-003, PZC 17-033, PZC 17-034, and PZC 17-035 has been completed in compliance with the California Environmental Quality Act and the State Guidelines for the Implementation of the California Environmental Quality Act of 1970;

2. The Commission further finds that the public review period for the Draft EIR and ended on September 5, 2017, and that a Public Hearing for the Final EIR was opened and closed after the items discussion, at a regular meeting of the Commission held on September 27, 2017.

3. The Commission is deemed the recommending body with respect to the Project EIR and Community Plan Update, unless duly appealed to the Tulare County Board of Supervisors. In the context of the California Environmental Quality Act ("CEQA"), the County is the "Lead Agency."

4. Therefore, the findings made in regard to this Environmental Impact Report and Mitigation Monitoring and Reporting Program for the Project are recommended by the Planning Commission as the Lead Agency on September 27, 2017 by Resolution No. 9347.

5. Consistent with Public Resource Code section 21081 and Guidelines sections 15091 through 15093 (including Public Resources Code section 21061.1 and Guidelines section 15364 relating to the definition of "feasibility"), the Commission hereby makes various findings relating to the less than significant effects identified in the Project EIR. Based on substantial evidence in the EIR and Pursuant to the discussion in each section of the EIR, and the Public Record of Proceedings, the Commission finds and declares that the mitigation measures included in the MMRP are required to reduce potential impacts related to Agricultural, Biological, Cultural, and Hydrology and Water Quality, but that Project-related impacts would be reduced to less than significant with the adoption of these measures. The Commission also finds that the Project has the potential to have significant impacts related to Air Quality. The EIR evaluated all known mitigation measures and concluded that additional mitigation measures beyond compliance with agency regulation are not feasible, and that the Project may result in a significant and unavoidable impact to the environment for air impacts.

AND, BE IT FURTHER RESOLVED as follows:

A. The Commission finds that the County is required to undertake Mitigation Measures (see Attachment B), that are restrictive and applied only to this Community Plan. Therefore, the public will benefit from the Project as it will implement the General Plan and advance socially desirable community improvements that are environmentally and economically sound.

B. The Secretary of the Planning Commission is hereby directed to file the Notice of Determination upon completion of the appeal period for this project.

C. The Commission confirms that the documents and other materials that constitute the public record of proceedings (which supports the Commission's decision made herein) are available at the Tulare County Resource Management Agency at 5961 South Mooney Boulevard, Visalia, California 93277, (559) 624-7000. The custodian for these documents and other materials is Mr. Hector Guerra, Chief Environmental Planner, Environmental Planning Division, Tulare County Resource Management Agency.

D. The Planning Commission hereby recommends that the Board of Supervisors certify and adopt the Environmental Impact Report, Findings of Fact, and the Mitigation Monitoring and Reporting Program, and certify and adopt the Statement of Overriding

Considerations for GPA 17-005, GPA14-005, GPA16-003, PZC 17-033, PZC 17-034, and PZC 17-035.

The foregoing resolution was adopted upon motion of Commissioner Dias, seconded by Commissioner Millies, at a regular meeting of the Planning Commission on September 27, 2017, by the following roll call vote:

AYES: Dias, Aguilar, Whitlatch, Millies, Gong, Pitigliano
NOES: None
ABSTAIN: None
ABSENT: Elliot

TULARE COUNTY PLANNING COMMISSION



Michael Washam, Secretary

Attachments:

- A – Final Environmental Impact Report
- B – Mitigation Monitoring and Reporting Plan (MMRP)
- C – Findings of Fact (FOF)
- D – Statement of Overriding Consideration (SOC)
- E – Responses to Comments

BEFORE THE BOARD OF SUPERVISORS COUNTY OF TULARE, STATE OF CALIFORNIA

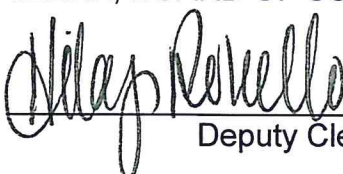
IN THE MATTER OF EARLIMART)	
COMMUNITY PLAN 2017 UPDATE)	Resolution No. <u>2017-0869</u>
(GENERAL PLAN AMENDMENT NO. GPA)	Ordinance No. <u>3509</u>
17-005) AND AMENDMENTS TO THE)	
TULARE COUNTY ZONING ORDINANCE)	
NO. 352)	

UPON MOTION OF SUPERVISOR WORTHLEY, SECONDED BY SUPERVISOR ENNIS, THE FOLLOWING WAS ADOPTED BY THE BOARD OF SUPERVISORS, AT AN OFFICIAL MEETING HELD OCTOBER 17, 2017 BY THE FOLLOWING VOTE:

AYES: SUPERVISORS CROCKER, VANDER POEL, SHUKLIAN, WORTHLEY, AND ENNIS
NOES: NONE
ABSTAIN: NONE
ABSENT: NONE



ATTEST: MICHAEL C. SPATA
COUNTY ADMINISTRATIVE OFFICER/
CLERK, BOARD OF SUPERVISORS

BY: 
Deputy Clerk

* * * * *

That the Board of Supervisors:

1. Held a Public Hearing at 9:30 a.m. or shortly thereafter; and
2. Certified and adopted the Final EIR under the California Environmental Quality Act (CEQA), including Findings of Fact, Statement of Overriding Considerations, and the Mitigation Monitoring and Reporting Program (MMRP) for the Earlimart Community Plan 2017 Update (GPA 17-005) as being in compliance with the California Environmental Quality Act (CEQA) and the State CEQA Guidelines, including CEQA findings, and adopted the Mitigation Monitoring and Reporting Programs; and
3. Adopted the Earlimart Community Plan 2017 Update (GPA 17-005), inclusive of the Eden Produce Company Project (GPA 14-005) and the Delano Joint High School District Project (GPA 16-003), and Amendments to the General Plan Planning Framework Element, Open Space (Environmental Resources Management) Element, Land Use Element, Circulation Element (Part I) and the Community Plans Element (Part II); and

4. Waived the final reading and adopted Amendments to Section 18.9 and Section 16 of Tulare County Ordinance No. 352, the Zoning Ordinance, and Map Amendments to rezone properties consistent with the Earlimart Community Plan 2017 Update as follows:
 - A. Zone Ordinance Amendment (PZC 17-033) to Section 18.9 to adopt a Mixed-Use Combining Zone within the Earlimart Urban Development Boundary (UDB).
 - B. Zone Ordinance Amendment (PZC 17-034) to Section 16 to allow additional by-right uses within the Earlimart UDB.
 - C. Zone Ordinance Amendment (PZC 17-035) as set forth in Zoning District Ordinance Map consistent with the Earlimart Community Plan 2017 Update; and
5. Adopted the findings of approval set forth in Planning Commission Resolution No. 9347 (Final EIR), Planning Commission Resolution No. 9348 (Earlimart Community Plan 2017 Update), Planning Commission Resolution No. 9349 (Section 18.9 Mixed-Use), Planning Commission Resolution No. 9350 (Section 16 By-Right Uses) and Planning Commission Resolution No. 9351 (Zoning District Ordinance Maps); and
6. Authorized the Chairman to sign any and all necessary zoning changes under Ordinance 352, and specifically changes to Sections 18.9 and Section 16 to add the Community of Earlimart to include the Mixed-Use Combining Zone and additional by-right uses; and
7. Directed the Clerk of the Board to publish once in the Visalia Times-Delta newspaper the summary of the ordinance amendments and amended zoning map with the names of the Board of Supervisors voting for and against the amendment and to post a certified copy of the full ordinance amending the Tulare County Zoning Ordinance with the names of the Board of Supervisors voting for and against the amendment, within fifteen (15) days as required by Section 25124 et. seq; and
8. Directed the Environmental Assessment Officer, or designee, of the Tulare Resource Management Agency to file the Notice of Determination with the Tulare County Clerk; and
9. Approved more detailed findings contained in Exhibit "A" incorporated by reference herein; and
10. Authorized the Director of the Resource Management Agency, or designee, to make appropriate technical revisions to the General Plan Amendment, Community Plans, and associated project documents

EXHIBIT "A"

A. Earlimart 2017 Update (General Plan Amendment 17-005)

WHEREAS, on September 27, 2017 the Planning Commission held a public hearing on the Earlimart Community Plan 2017 Update, which included an application for the Earlimart Community Plan 2017 Update as a collaborative effort funded in part by Community Development Block Grant (14-CDBG-9892), the Eden Produce Company, the Delano Joint High School District and the County of Tulare, and includes PZC 17-033 Mixed-Use Combining Zone (Section 18.9), PZC 17-034 By-Right Uses (Section 16), PZC 17-035 Zoning District Ordinance Map (Rezoning Plan), collectively referred to as the Earlimart Community Plan 2017 Update (GPA 17-005); and

WHEREAS, upon conclusion of the public hearing, the Planning Commission recommended the Board of Supervisors certify and adopt the Final Environmental Impact Report, including the Findings of Fact (FOF), Statement of Overriding Considerations (SOC), and Mitigation Monitoring and Reporting Program (MMRP), as being in compliance with the California Environmental Quality Act (CEQA) and the State CEQA Guidelines, including CEQA findings, and adopt the Mitigation Monitoring and Reporting Program and approve the Earlimart Community Plan 2017 Update; and

WHEREAS, the Board of Supervisors held a public hearing on October 17, 2017 to consider the proposed Earlimart Community Plan 2017 Update; and

WHEREAS, the Board of Supervisors finds the public hearing for the Earlimart Community Plan 2017 Update was properly noticed in accordance with state law and the Tulare County Ordinance Code.

WHEREAS, the Board of Supervisors finds the Earlimart Community Plan 2017 Update is compatible, integrated and internally consistent with the existing policies of and with the Tulare County General Plan.

WHEREAS, the Board of Supervisors accepts and incorporates by reference herein, the findings of facts and recommendations adopted by the Planning Commission as follows:

1. Planning Commission Resolution No. 9347, in the matter of the Certification and Adoption of the Certification and Adoption of the FEIR under California Environmental Quality Act (CEQA), including the Findings of Fact, Statement of Overriding Considerations, and the Mitigation Monitoring and Reporting Program (MMRP) for the Earlimart Community Plan 2017 Update (PC Package Attachment 1 – CEQA Resolution).
2. Planning Commission Resolution No. 9348, to adopt the Earlimart Community Plan 2017 Update (GPA 17-005), inclusive of the Eden Produce Company Project (GPA 14-005) and the Delano Joint High School District Project (GPA 16-003), and Amendments to the General Plan Planning Framework Element, Open Space (Environmental Resources Management)

Element, Land Use Element, Circulation Element (Part I) and the Community Plans Element (Part II) (PC Package Attachment 2 –GPA Resolution).

3. Planning Commission Resolution No. 9349, in the matter of an Amendment to Section 18.9 of Ordinance No. 352, the Zoning Ordinance, and establish the Mixed Use Combining Zone within the Community of Earlimart Urban Development Boundary (PC Package Attachment 3 – Section 18.9 Resolution).
4. Planning Commission Resolution No. 9350, in the matter of an Amendment to Section 16 of Ordinance No. 352, the Zoning Ordinance, to allow additional by-right uses within the Community of Earlimart Urban Development Boundary (PC Package Attachment 4 – Section 16 Resolution).
5. Planning Commission Resolution No. 9351, in the matter of an Amendment to Ordinance No. 352, the Zoning Ordinance, as set forth in the Zoning District Ordinance Map and Earlimart Rezoning Plan, consistent with the Earlimart Community Plan 2017 Update. (PC Package Attachment 5 – Rezoning Plan Resolution).

NOW, THEREFORE, BE IT RESOLVED as follows:

- A. The Board of Supervisors certifies that: (1) the Final EIR has been completed in compliance with CEQA; (2) the Final EIR was presented to the Board of Supervisors, and that the Board of Supervisors reviewed and considered the information contained in the Final EIR prior to approving the project; and, (3) the Final EIR reflects Tulare County's independent judgment and analysis.
- B. The Board of Supervisors adopts the Findings of Fact, Statement of Overriding Considerations and the Mitigation Monitoring and Reporting Program (MMRP) for the Earlimart Community Plan 2017 Update included in PC Package Attachment 1 of the Board Report for this agenda item, dated October 17, 2017.

NOW BE IT FURTHER RESOLVED as follows:

- C. The Board of Supervisors adopts the entirety of all of the above findings and actions as one General Plan Amendment No. GPA 17-005.
- D. The Board of Supervisors hereby waives the final reading of Change of Ordinance 352 and adopts amendments to the Tulare County Zoning Ordinance 352 pertaining to Section 18.9 - Mixed-Use Combining Zone, to Section 16 - Variances and Special Uses, and Zoning District Map Changes.
- E. The Board of Supervisors authorizes the Chairman to sign any and all necessary zoning changes under Ordinance 352, and specifically changes to Sections 18.9 and Section 16 to add the communities of Earlimart Community Plan 2017 Update to include Mixed Use Combining Zone and additional By-Right uses.
- F. The Board of Supervisors directs the Clerk of the Board to publish once in the Visalia Times-Delta newspaper the summary of the ordinance amendments and

amended zoning map with the names of the Board of Supervisors voting for and against the amendment and to post a certified copy of the full ordinance amending the Tulare County Zoning Ordinance with the names of the Board of Supervisors voting for and against the amendment, within fifteen (15) days as required by Section 25124 et. seq.

G. The Board of Supervisors directs the Environmental Assessment Officer, or designee, of the Tulare Resource Management Agency to file Notices of Determination with the Tulare County Clerk.

H. The Board of Supervisors authorizes the Director of the Resource Management Agency, or designee, to make appropriate technical revisions to the General Plan Amendment, Earlimart Community Plan, and associated project documents.

RMA

HAR
10/17/2017